

12 - STRUCTURAL




INDEX OF DRAWINGS

| DRAWING NUMBER | DRAWING TITLE | DRAWING NUMBER | DRAWING TITLE |
|----------------|--------------------------------------|----------------|---|
| STI-001 | DRAWING INDEX | FST-300 | CANOPY SECTIONS & DETAILS 1 |
| FST-001 | STRUCTURAL NOTES 1 | FST-301 | CANOPY SECTIONS & DETAILS 2 |
| FST-002 | STRUCTURAL NOTES 2 | FST-302 | CANOPY SECTIONS & DETAILS 3 |
| FST-003 | STRUCTURAL NOTES 3 | FST-303 | CANOPY SECTIONS & DETAILS 4 |
| FST-100 | FOUNDATION PLAN 1 | FST-304 | CANOPY SECTIONS & DETAILS 5 |
| FST-101 | FOUNDATION PLAN 2 | FST-305 | CANOPY SECTIONS & DETAILS 6 |
| FST-102 | FOUNDATION PLAN 3 | FST-306 | CANOPY SECTIONS & DETAILS 7 |
| FST-103 | ENLARGED PLATFORM FOUNDATION PLAN 1 | FST-307 | CANOPY SECTIONS & DETAILS 8 |
| FST-104 | ENLARGED PLATFORM FOUNDATION PLAN 2 | FST-308 | PLATFORM MISC. STEEL DETAILS 1 |
| FST-105 | SUGGESTED SEQUENCE OF CONSTRUCTION 1 | FST-309 | PLATFORM MISC. STEEL DETAILS 2 |
| FST-106 | SUGGESTED SEQUENCE OF CONSTRUCTION 2 | FST-400 | TOWER SUPERSTRUCTURE SECTIONS & DETAILS 1 |
| FST-107 | ENLARGED WEST TOWER FOUNDATION PLAN | FST-401 | TOWER SUPERSTRUCTURE SECTIONS & DETAILS 2 |
| FST-108 | ENLARGED EAST TOWER FOUNDATION PLAN | FST-402 | TOWER SUPERSTRUCTURE SECTIONS & DETAILS 3 |
| FST-109 | CANOPY FRAMING PLAN 1 | FST-500 | PEDESTRIAN BRIDGE FRAMING DETAILS 1 |
| FST-110 | CANOPY FRAMING PLAN 2 | FST-501 | PEDESTRIAN BRIDGE FRAMING DETAILS 2 |
| FST-111 | CANOPY FRAMING PLAN 3 | FST-502 | PEDESTRIAN BRIDGE FRAMING DETAILS 3 |
| FST-112 | TOWER FLOOR FRAMING PLAN | FST-600 | ERECTION SEQUENCE 1 |
| FST-113 | WEST & EAST TOWER FRAME ELEVATION | FST-601 | ERECTION SEQUENCE 2 |
| FST-114 | TOWER ROOF FRAMING PLAN | | |
| FST-115 | TOWER COLUMN SCHEDULE | | |
| FST-200 | PLATFORM SECTIONS & DETAILS 1 | | |
| FST-201 | PLATFORM SECTIONS & DETAILS 2 | | |
| FST-202 | PLATFORM SECTIONS & DETAILS 3 | | |
| FST-203 | PLATFORM SECTIONS & DETAILS 4 | | |
| FST-204 | PLATFORM SECTIONS & DETAILS 5 | | |
| FST-205 | PLATFORM SECTIONS & DETAILS 6 | | |
| FST-206 | PLATFORM SECTIONS & DETAILS 7 | | |
| FST-207 | PLATFORM SECTIONS & DETAILS 8 | | |
| FST-208 | PLATFORM SECTIONS & DETAILS 9 | | |
| FST-209 | PLATFORM SECTIONS & DETAILS 10 | | |
| FST-210 | PLATFORM SECTIONS & DETAILS 11 | | |
| FST-211 | PLATFORM SECTIONS & DETAILS 12 | | |
| FST-212 | WEST PLATFORM PROFILE - NORTH HALF | | |
| FST-213 | WEST PLATFORM PROFILE - SOUTH HALF | | |
| FST-214 | EAST PLATFORM PROFILE - SOUTH HALF | | |
| FST-215 | EAST PLATFORM PROFILE - NORTH HALF | | |
| FST-216 | TOWER FOUNDATION DETAILS 1 | | |
| FST-217 | TOWER FOUNDATION DETAILS 2 | | |
| FST-218 | TOWER FOUNDATION DETAILS 3 | | |
| FST-219 | TOWER FOUNDATION DETAILS 4 | | |
| FST-220 | TOWER FOUNDATION DETAILS 5 | | |

DESIGNED BY:

TranSystems

TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

| | | | | | | | | | | | |
|------|------|----------------------|-----------|---|--|--|---|--|---|------------------------|--------------------------------|
| - | - | - | - | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | DESIGNER/DRAFTER: C DONOHUE CHECKED BY: H BUI |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION |  |  530 PRESTON AVENUE MERIDEN, CT 06450 | NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | TOWN: BERLIN | PROJECT NO. 170-3155 |
| - | - | - | - | | | | | | | | DRAWING NO. STI-001 |
| - | - | - | - | | | | | | | | SHEET NO. 01.12.001 |
| - | - | - | - | | | | | | | | |
| - | - | - | - | | | | | | | | |
| - | - | - | - | | | | | | | | |
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. | Plotted Date: 2/26/2014 | | Filename: ...\\FA_CGR_CPS_0170-2296_007_04_STI_001.dgn | | | | | |

PRECAST CONCRETE (CONTINUED)

PC-6 ALL OPENINGS (SHOP AND FIELD CUT) MUST BE SHOWN ON FINAL APPROVED SHOP DRAWINGS. FIELD CUT OPENINGS ARE NOT PERMITTED UNLESS SPECIFICALLY SHOWN ON THE SHOP DRAWINGS AND ACCOUNTED FOR IN THE DESIGN BY THE PRECAST MANUFACTURER.

PC-7 REFER TO ARCHITECTURAL DRAWINGS FOR DIMENSIONS AND REVEALS NOT SHOWN.

PC-8 COORDINATE WITH ELECTRICAL DRAWINGS FOR GROUNDING REQUIREMENTS.

PC-9 PRECAST CONCRETE PLANKS AND RAMPS:

- A. PRECAST CONCRETE PLANKS AND RAMPS ARE TO BE DESIGNED FOR SUPERIMPOSED DEAD AND LIVE LOADS INDICATED ON THE DRAWINGS, AND IN ACCORDANCE WITH ALL APPLICABLE BUILDING CODES AND REGULATIONS. ADDITIONALLY, DESIGN THE PANELS FOR ALL STRESSES INDUCED BY THE LIFTING AND ERECTION PROCESS.
- B. PRECAST MANUFACTURER MUST CONSULT ARCHITECTURAL, MECHANICAL AND ELECTRICAL DRAWINGS FOR OPENINGS REQUIRED IN PRECAST PLANKS AND RAMPS IN ADDITION TO THOSE INDICATED ON THE STRUCTURAL DRAWINGS. REINFORCE PLANKS AND RAMPS, OR PROVIDE HEADER, AS REQUIRED.
- C. FOLLOWING ERECTION, DEFLECTION OF PRECAST UNITS ARE NOT TO EXCEED THE SPAN/360 FOR THE SUM OF DEFLECTIONS DUE TO LIVE LOAD AND LONG-TERM SUSTAINED LOADS.
- D. CONCRETE FOR TOPPING SLABS SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH (f_c) OF NOT LESS THAN 5000 PSI AT 28 DAYS.
- E. SURFACE OF PRECAST PLANKS AND RAMPS TO RECEIVE CONCRETE TOPPING MUST BE CLEANED AND PREPARED IN ACCORDANCE WITH THE RECOMMENDATIONS OF ACI 302.1 (LATEST EDITION).
- F. COMPOSITE CONCRETE TOPPING TO BE REINFORCED WITH 6x6-W2.9xW2.9 WELDED WIRE FABRIC (FURNISHED IN SHEETS).
- G. PRECAST PLANKS SHALL HAVE A SMOOTH ALIGNMENT ALONG TRACK-SIDE FACE WHEN TIED TOGETHER. ALLOWABLE TOLERANCE SHALL BE $\frac{1}{8}$ ".

STRUCTURAL STEEL

S-1 PRIOR TO FABRICATION, THE STEEL FABRICATOR IS TO SUBMIT TO THE STRUCTURAL ENGINEER OF RECORD FOR REVIEW THE FOLLOWING:

- DESIGN CALCULATIONS OF STRUCTURAL STEEL CONNECTIONS, PREPARED BY OR UNDER THE SUPERVISION OF A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF WHICH THE PROJECT IS CONSTRUCTED AND BEARING THE SEAL OF THE PROFESSIONAL ENGINEER.
- SHOP DRAWINGS SHOWING ERECTION PLANS, PIECE DRAWINGS, AND CONNECTION DETAILS.

S-2 THE STRUCTURAL STEEL FABRICATOR, AND/OR GENERAL CONTRACTOR, MUST VERIFY ALL EXISTING DIMENSIONS AND CONDITIONS AT THE SITE. ALL DISCREPANCIES FOUND ARE TO BE REPORTED TO THE STRUCTURAL ENGINEER OF RECORD PRIOR TO PREPARATION OF SHOP DRAWINGS. SHOP DRAWINGS ARE TO INCLUDE ALL FIELD MEASUREMENTS AND CONDITIONS.

S-3 STRUCTURAL STEEL FABRICATOR IS TO PROVIDE FOR VERTICAL AND HORIZONTAL FIELD ADJUSTMENT OF ALL SUPPORT ASSEMBLIES.

S-4 CUTS, HOLES, COPING, ETC REQUIRED FOR OTHER TRADES MUST BE SHOWN ON THE SHOP DRAWINGS AND MADE IN THE SHOP. CUTS OR BURNING OF HOLES IN THE FIELD WILL NOT BE PERMITTED.

S-5 STEEL BEAMS ARE TO BE EQUALLY SPACED IN BAYS UNLESS OTHERWISE NOTED.

S-6 FABRICATE AND INSTALL BEAMS WITH NATURAL CAMBER UP UNLESS CAMBER IS NOTED ON THE DRAWINGS.

S-7 ALL STRUCTURAL STEEL FRAMES AND TRUSSES ARE TO BE SECURELY BRACED UNTIL ALL FLOOR SLABS AND ROOF DECKS HAVE BEEN INSTALLED AND BECOME CAPABLE OF STABILIZING THE FRAMES.

8-8 UNLESS ALTERNATE CONNECTIONS ARE APPROVED BY THE ENGINEER, BOLTED CONNECTIONS SHALL BE MADE ACCORDING TO AISC TABLE II OR III - FRAMED BEAM CONNECTIONS. THE MINIMUM DEPTH OF THE CONNECTION MUST BE MORE THAN 67% OF THE BEAM DEPTH EXCEPT THAT BEAMS FRAMING TO COLUMNS SHALL HAVE FULL DEPTH CONNECTIONS USING $\frac{3}{4}$ " MINIMUM CONNECTION ANGLES OR PLATES.

- A. NON-COMPOSITE BEAM CONNECTIONS TO BE DESIGNED FOR ONE HALF THE UNIFORM LOAD CAPACITY OF THE BEAM AS TABULATED BY AISC UNLESS HIGHER LOADS ARE INDICATED ON THE DRAWINGS.
- B. COMPOSITE BEAMS (SHEAR STUDS ON TOP FLANGE) REQUIRE CONNECTION CAPACITIES TO BE INCREASED TO 75% LOAD CAPACITY OF THE BEAM UNLESS HIGHER LOADS ARE INDICATED ON THE DRAWINGS.

| BEAM SIZE | MIN. NO. OF BOLTS |
|-----------|-------------------|
| W8, W10 | 2 |
| W12, W14 | 3 |
| W16, W18 | 4 |
| W21 | 5 |
| W24 | 6 |
| W27 | 7 |
| W30 | 8 |
| W33 | 9 |
| W36 | 10 |

S-9 UNLESS OTHERWISE NOTED, STRUCTURAL STEEL CONNECTIONS TO BE SHOP WELDED AND FIELD BOLTED.

- A. BOLTS: $\frac{3}{4}$ " DIAMETER ASTM A 325 UNO WITH MATCHING WASHERS AND HEAVY HEX NUTS. USE TYPE N (HIGH STRENGTH) FOR FRAMED CONNECTIONS AND TYPE SC AT WIND MOMENT AND BRACING CONNECTIONS.

S-10 EXPANSION BOLTS TO BE STUD TYPE, CARBON STEEL ANCHORS OF THE DIAMETER AND EMBEDMENT INDICATED ON THE DRAWINGS. USE HILTI KWIK BOLT TZ EXPANSION ANCHORS MANUFACTURED BY HILTI FASTENING SYSTEMS, OR APPROVED EQUAL. INSTALL PER MANUFACTURERS RECOMMENDATIONS.

STRUCTURAL STEEL (CONTINUED)

S-11 SUBSTITUTION OF EXPANSION ANCHORS FOR EMBEDDED ANCHORS SHOWN ON THE DRAWINGS WILL NOT BE PERMITTED.

S-12 SHEAR STUD CONNECTORS TO BE $\frac{3}{4}$ " DIAMETER X $3\frac{1}{2}$ " LONG (IN PLACE HEIGHT) AND SECURELY WELDED TO THE BEAMS. NUMBER OF STUDS IS INDICATED BY (XX) AFTER BEAM SIZE ON PLAN. NUMBER OF STUDS INDICATE BY (XX-XX-XX ETC) SHALL BE PLACED IN CORRESPONDING GROUPS BETWEEN INTERSECTING MEMBERS AND SUPPORTING MEMBERS (NO STUDS ON CANTILEVERS).

- A. WHERE DECK IS PERPENDICULAR TO COMPOSITE BEAM (I.E. FILLER BEAM), PLACE A SINGLE STUD IN EVERY OTHER DECK CELL STARTING AT EACH BEAM END AND WORKING TOWARD THE CENTER OF THE BEAM. ANY REMAINING STUDS ARE TO BE PLACED IN EMPTY CELLS STARTING AGAIN AT EACH BEAM END AND WORKING TOWARD THE CENTER. ADDITIONAL STUDS OR "PUDDLE WELDS" MUST BE USED FOR REMAINING EMPTY FLUTES. IF NUMBER OF STUDS IS MORE THAN NUMBER OF FLUTES, PLACE DOUBLE OR TRIPLE ROWS AS NEEDED STARTING FROM EACH BEAM END AND WORKING TOWARD THE CENTER OF THE BEAM.
- B. WHERE DECK IS PARALLEL TO COMPOSITE BEAM (I.E. GIRDER), STUDS ARE TO BE EQUALLY SPACED OVER THE LENGTH OF THE BEAM. STUDS IN GROUPS BETWEEN INTERSECTING MEMBERS ARE TO BE EQUALLY SPACED.
- C. MINIMUM CENTER-TO-CENTER SPACING OF STUDS TO BE 6 DIAMETERS ($4\frac{1}{2}$ ") ALONG LONGITUDINAL AXIS OF COMPOSITE BEAM AND 4 DIAMETERS (3") TRANSVERSE TO THE LONGITUDINAL AXIS OF THE COMPOSITE BEAM. PLACE SINGLE ROW STUDS ON BEAM CENTER LINE. FOR STAGGERED ARRANGEMENT OF STUDS MAINTAIN MINIMUM $3\frac{3}{4}$ " SPACE BETWEEN STUDS, AND $\frac{7}{8}$ " MINIMUM DISTANCE FROM EDGE OF BEAM FLANGE. STUD SPACING IS NOT TO EXCEED 36" ON CENTER.
- D. SHOP DRAWING OF STUD PLACEMENT MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

S-13 PAINT AND PROTECTION:

- A. STRUCTURAL STEEL TO RECEIVE PAINTED FINISH TO BE SHOP CLEANED AND PRIME PAINTED PER THE SPECIFICATIONS. REFERENCE ARCHITECTURAL DRAWINGS FOR FINISH PAINT SYSTEMS.
- B. GALVANIZED FASTENERS AND ACCESSORIES TO BE HOT DIPPED PER ASTM A153.
- C. REPAIR DAMAGE TO GALVANIZED COATINGS USING ASTM A780 ZINC RICH PAINT.
- D. BRIDGE ASSEMBLY TO BE TREATED WITH THERMAL SPRAY METALIZED (TSM) COATING AND TOP COAT PER SPECS. FIELD WELDS WILL REQUIRE TOUCH UP OF TSM FINISH.

S-14 ALL CANOPY AND PEDESTRIAN BRIDGE ELEMENTS CONSIDERED "ARCHITECTURALLY EXPOSED STRUCTURAL STEEL."

MASONRY

M-1 MASONRY WORK MUST BE IN CONFORMANCE WITH THE "BUILDING CODE FOR MASONRY STRUCTURES" (ACI-530-05), AND THE "SPECIFICATIONS FOR MASONRY STRUCTURES" (ACI-530.1-05), OF THE AMERICAN CONCRETE INSTITUTE.

M-2 ALL MORTAR TO CONFORM TO ASTM C270, TYPE N
A. PORTLAND CEMENT: ASTM C150, TYPE I OR II.
B. LIME: ASTM C207.

M-3 GROUT IS TO CONFORM TO ASTM C476 AND HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH (f'_c) OF 3000 PSI.
A. SLUMP: 8 TO 11 INCHES.

M-4 CONCRETE MASONRY SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF MASONRY (F'M) EQUAL TO 1900 PSI AND:
A. HOLLOW BLOCK: ASTM C90, NORMAL WEIGHT.

M-5 EPOXY COATED BAR REINFORCEMENT PER ASTM A615, GRADE 60 AND ASTM A775. GROUT ALL REINFORCED CELLS SOLID WITH GROUT. PROVIDE MINIMUM LAP SPLICES OF 48 BAR DIAMETERS. PROVIDE BAR SPACERS AS REQUIRED TO PROPERLY LOCATE REINFORCING WITHIN CMU CELLS. PLACE BARS @ CENTER OF CELLS UNO.

M-6 PROVIDE ONE-PIECE PREFABRICATED HORIZONTAL JOINT REINFORCING UNITS PER SPEC AT 16" O.C. VERTICALLY TYPICAL AND AT 8" O.C. AT ALL WALL CORNERS AND INTERSECTIONS AND IN THE FIRST TWO COURSES ABOVE AND BELOW MASONRY OPENINGS. PROVIDE LAP AS RECOMMENDED BY THE MANUFACTURER WITH A MINIMUM OF 6". DISCONTINUE HORIZONTAL JOINT REINFORCING AT CONTROL JOINTS.

M-7 FULL BED AND HEAD JOINTS MUST BE USED.

M-8 GROUT CELLS OF CMU SOLID FOR BOND BEAMS AND CELLS WITH VERTICAL REINFORCEMENT.

M-9 CONTROL JOINTS IN MASONRY WALLS TO BE PROVIDED WHERE INDICATED ON THE STRUCTURAL AND ARCHITECTURAL DRAWINGS OR AT A MAXIMUM OF 24'-0" ON CENTER.

M-10 ALL MASONRY WALLS TO BE SECURELY BRACED UNTIL ROOF SYSTEM HAS BEEN INSTALLED AND HAS BECOME CAPABLE OF STABILIZING THE WALLS.

M-11 DOWEL REINFORCED MASONRY WALLS TO WALL OR CURB AS SHOWN ON THE PLANS. LOCATE DOWELS IN CELLS TO CONTAIN WALL REINFORCEMENT. LAP DOWELS AND WALL REINFORCEMENT A MINIMUM OF 48 BAR DIAMETERS UNO.

M-12 DURING CONSTRUCTION, COVER AND PROTECT THE TOPS OF MASONRY WALLS AT THE END OF EACH DAY.

M-13 UNLESS NOTED OTHERWISE, PLACE TYPICAL CMU REINFORCMENT IN CENTER OF FULLY GROUTED CELLS AND SPACE AS FOLLOWS: (1)#5 VERTICAL AT 16" ON CENTER.

M-14 ALL CORNERS TO BE TIED BY MASONRY BOND.

M-15 DIMENSIONS SHOWN ON STRUCTURAL DRAWINGS FOR MASONRY ARE NOMINAL. SEE ARCHITECTURAL DRAWINGS FOR ACTUAL MASONRY DIMENSIONS.

[illegible]

KEYS AND ABBREVIATIONS

| | |
|--------|--|
| AB | ARCHITECT/ENGINEER |
| AB | ANCHOR BOLT |
| ABV | ABOVE |
| ACI | AMERICAN CONCRETE INSTITUTE |
| ADDL | ADDITIONAL |
| AFF | ABOVE FINISHED FLOOR |
| AHU | AIR HANDLING UNIT |
| AISC | AMERICAN INSTITUTE OF STEEL CONSTRUCTION |
| ALT | ALTERNATE |
| APPROX | APPROXIMATE |
| ARCH | ARCHITECT OR ARCHITECTURAL(S) |
| ASTM | AMERICAN SOCIETY FOR TESTING AND MATERIALS |
| APG | AUGER PRESSURE GROUTED PILE |
| B PL | BASE PLATE OR BEARING PLATE |
| B/ | BOTTOM OF |
| BF | BOTH FACES |
| BLDG | BUILDING |
| BLK | BLOCK |
| BM | BEAM |
| BRG | BEARING |
| BS | BOTH SIDES |
| BT | BENT |
| BTWN | BETWEEN |
| CB | CATCH BASIN |
| CF | CUBIC FOOT OR CUBIC FEET |
| CHAM | CHAMFER |
| CIP | CAST IN PLACE |
| CJ | CONTROL JOINT |
| C | CENTERLINE |
| CLR | CLEAR |
| CMU | CONCRETE MASONRY UNIT |
| COL | COLUMN |
| CONC | CONCRETE |
| CONN | CONNECTION |
| CONST | CONSTRUCTION |
| CONT | CONTINUOUS |
| CRSI | CONCRETE REINFORCING STEEL INSTITUTE |
| CT | CANTILEVER |
| C&S | COMMUNICATION & SIGNAL |
| db | BAR DIAMETER |
| DBL | DOUBLE |
| DET | DETAIL |
| DIA | DIAMETER |
| DIAG | DIAGONAL |
| DIM | DIMENSION |
| DL | DEAD LOAD |
| DN | DOWN |
| DWG | DRAWING |
| DWL | DOWEL |
| EA | EACH |
| EF | EACH FACE |
| EL | ELEVATION |
| ELEC | ELECTRICAL |
| EMBED | EMBED(ED)(MENT) |
| ENGR | ENGINEER |
| EOS | EDGE OF SLAB |
| EQ | EQUAL |
| EQUIP | EQUIPMENT |
| EST | ESTIMATE(D) |
| EW | EACH WAY |
| EXC | EXCAVATE OR EXCAVATION |
| EXIST | EXISTING |
| EXP | EXPANSION |
| EXT | EXTERIOR |
| FD | FLOOR DRAIN |
| FDN | FOUNDATION |
| FF | FAR FACE |
| FIN | FINISHED |
| FL | FLOOR |
| FS | FAR SIDE |
| FT | FOOT OR FEET |
| FTG | FOOTING |
| GA | GAGE OR GAUGE |
| GALV | GALVANIZED |
| GC | GENERAL CONTRACTOR |
| HORIZ | HORIZONTAL |
| HP | HIGH POINT |
| HS | HIGH STRENGTH |
| HSS | HOLLOW STRUCTURAL SECTION |
| HT | HEIGHT |
| ID | INSIDE DIAMETER |
| IN | INCH(ES) |
| INCL | INCLUDE(D)(ING), INCLUSIVE |
| INFO | INFORMATION |
| INT | INTERIOR |
| ISO JT | ISOLATION JOINT |
| JST | JOIST |
| JT | JOINT |
| K | KIP(S) |
| KB | KNEE BRACE |
| KCF | KIPS PER CUBIC FEET |
| KLF | KIPS PER LINEAR FOOT |
| KSF | KIPS PER SQUARE FEET |
| KSI | KIPS PER SQUARE INCH |
| L | LENGTH |

| | |
|----------|-----------------------------------|
| B | POUND |
| BD | DEVELOPMENT LENGTH |
| LL | LIVE LOAD |
| LLH | LONG LEG HORIZONTAL |
| LLV | LONG LEG VERTICAL |
| LONG | LONGITUDINAL |
| LP | LOW POINT |
| LWC | LIGHT-WEIGHT CONCRETE |
| MAS | MASONRY |
| MATL | MATERIAL |
| MAX | MAXIMUM |
| MCJ | MASONRY CONTROL JOINT |
| MECH | MECHANICAL(S) |
| M/E/P | MECHANICAL, ELECTRICAL & PLUMBING |
| MFR | MANUFACTURER |
| MID | MIDDLE |
| MIN | MINIMUM |
| MISC | MISCELLANEOUS |
| MO | MASONRY OPENING |
| MP | MASONRY PIER |
| N | NORTH |
| NF | NEAR FACE |
| NIC | NOT IN CONTRACT |
| NO | NUMBER |
| NOM | NOMINAL |
| NS | NEAR SIDE |
| NTS | NOT TO SCALE |
| NWC | NORMAL-WEIGHT CONCRETE |
| OC | ON CENTER |
| OD | OUTSIDE DIAMETER |
| OPNG | OPENING |
| OPP HAND | OPPOSITE HAND |
| P/C | PRECAST CONCRETE |
| PCF | POUNDS PER CUBIC FEET |
| PCI | POUNDS PER CUBIC INCH |
| PERM | PERMANENT |
| PLF | POUNDS PER LINEAR FOOT |
| P/S | PRESTRESSED CONCRETE |
| PSF | POUNDS PER SQUARE FEET |
| PSI | POUNDS PER SQUARE INCH |
| PVMT | PAVEMENT |
| RAD | RADIUS OR RADII |
| RE: | REFER TO |
| REF | REFERENCE |
| REINF | REINFORCEMENT |
| REQD | REQUIRED |
| REV | REVISION OR REVISE |
| ROW | RIGHT-OF-WAY |
| RR | RAILROAD |
| RTU | ROOF TOP UNIT |
| SC | SLIP CRITICAL |
| SCHED | SCHEDULE |
| SDI | STEEL DECK INSTITUTE |
| SECT | SECTION |
| SER | STRUCTURAL ENGINEER OF RECORD |
| SHT | SHEET |
| SIM | SIMILAR |
| SL | SLOPE(D) OR SLOPING |
| SLV | SLEEVE |
| SOG | SLAB ON GRADE |
| SP | SPACE(S) |
| SPA | SPACES OR SPACING |
| SPEC | SPECIFICATIONS |
| SS | STAINLESS STEEL |
| STD | STANDARD |
| STIFF | STIFFENER |
| STL | STEEL |
| STRUCT | STRUCTURAL |
| SYMM | SYMMETRICAL |
| T | THICKNESS |
| T&B | TOP & BOTTOM |
| T&S | TEMPERATURE & SHRINKAGE |
| T/ | TOP OF |
| TPE | TOP OF PIER ELEVATION |
| TSE | TOP OF SHELF ELEVATION |
| TSM | THERMAL SPRAY METALIZING |
| TWE | TOP OF WALL ELEVATION |
| TEMP | TEMPORARY |
| THD | THREAD(ED) |
| THK | THICKNESS |
| TRANS | TRANSVERSE |
| TSF | TONS PER SQUARE FEET |
| TYP | TYPICAL |
| UNO | UNLESS NOTED OTHERWISE |
| VERT | VERTICAL |
| VIF | VERIFY IN FIELD |
| W | WIDTH |
| W/ | WITH |
| W/O | WITHOUT |
| WL | WIND LOAD |
| WP | WORKING POINT |
| WT | WEIGHT |
| WWF | WELDED WIRE FABRIC |
| WWR | WELDED WIRE REINFORCEMENT |

1. THE BASELINE OF THE PROPOSED TRACKS ARE BY OTHERS AND BEING CONSTRUCTED UNDER A SEPARATE STATE PROJECT. THEY ARE SHOWN HERE FOR INFORMATIONAL PURPOSES ONLY AND ARE IN NO WAY WARRANTED TO INDICATE THE AS-BUILT CONDITIONS IN THE FIELD. THE CONTRACTOR SHALL VERIFY THE LOCATION OF THE PROPOSED TRACKS VIA A FIELD SURVEY PRIOR TO START OF PLATFORM CONSTRUCTION AND SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES. AT ALL TIMES DURING THE CONSTRUCTION OF THE PLATFORMS, THE CONTRACTOR SHALL MAINTAIN THE HORIZONTAL AND VERTICAL OFFSETS AS INDICATED IN THE CONTRACT DOCUMENTS.
2. THE BASELINE OF THE PROPOSED GAUNTLET TRACK IS BY OTHERS AND BEING CONSTRUCTED UNDER A SEPARATE STATE PROJECT. IT IS REFERENCED HERE FOR INFORMATIONAL PURPOSES ONLY AND IS IN NO WAY WARRANTED TO INDICATE THE AS-BUILT CONDITIONS IN THE FIELD. THE CONTRACTOR SHALL CONSTRUCT THE PLATFORMS IN ACCORDANCE WITH THE APPLICABLE NOTES FOR TRACK 1 AND TRACK 2 AND AS SHOWN ON THE PLANS.

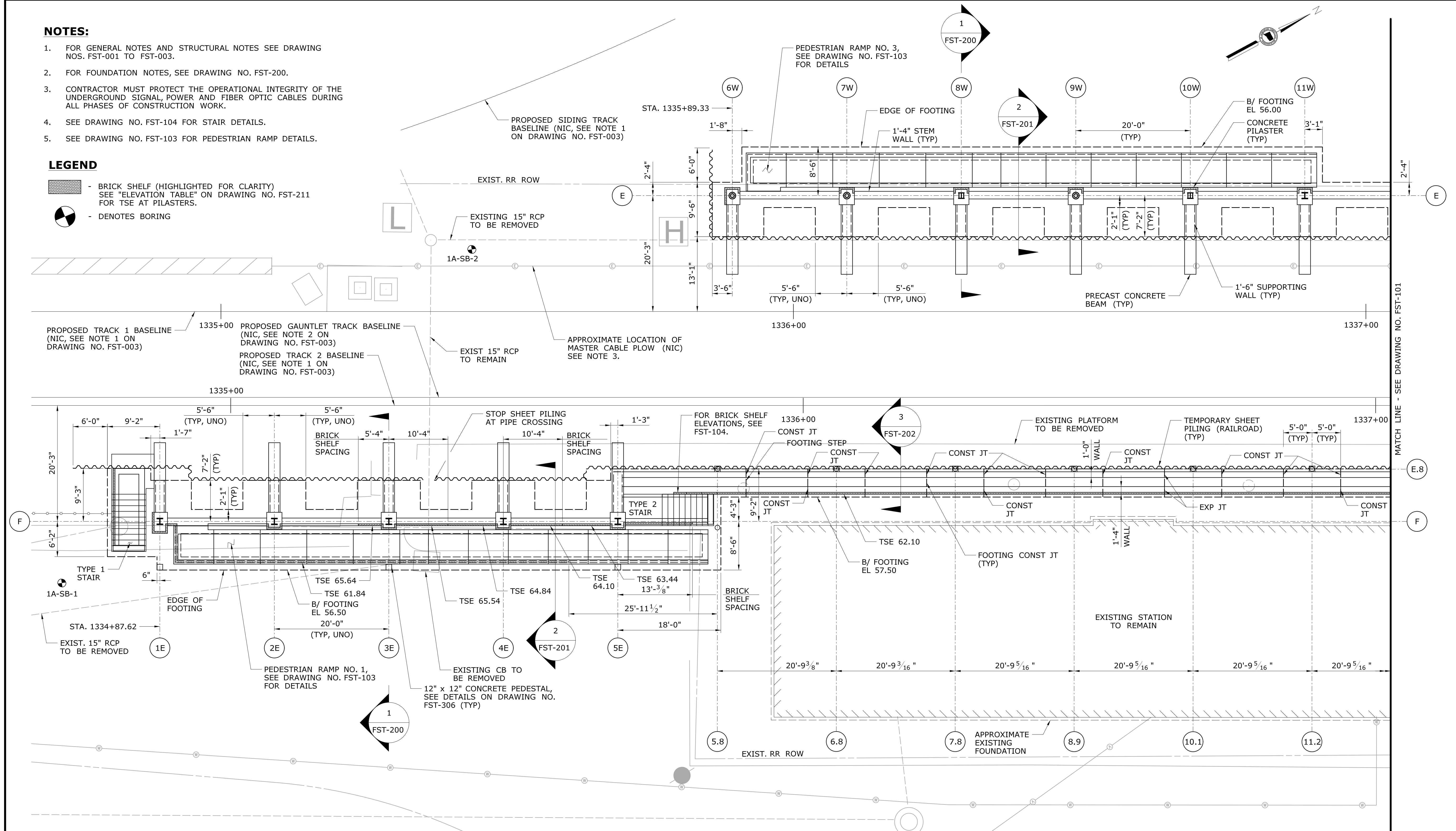
[illegible]

NOTES:

- FOR GENERAL NOTES AND STRUCTURAL NOTES SEE DRAWING NOS. FST-001 TO FST-003.
- FOR FOUNDATION NOTES, SEE DRAWING NO. FST-200.
- CONTRACTOR MUST PROTECT THE OPERATIONAL INTEGRITY OF THE UNDERGROUND SIGNAL, POWER AND FIBER OPTIC CABLES DURING ALL PHASES OF CONSTRUCTION WORK.
- SEE DRAWING NO. FST-104 FOR STAIR DETAILS.
- SEE DRAWING NO. FST-103 FOR PEDESTRIAN RAMP DETAILS.

LEGEND

- BRICK SHELF (HIGHLIGHTED FOR CLARITY) SEE "ELEVATION TABLE" ON DRAWING NO. FST-211 FOR TSE AT PILASTERS.
- DENOTES BORING

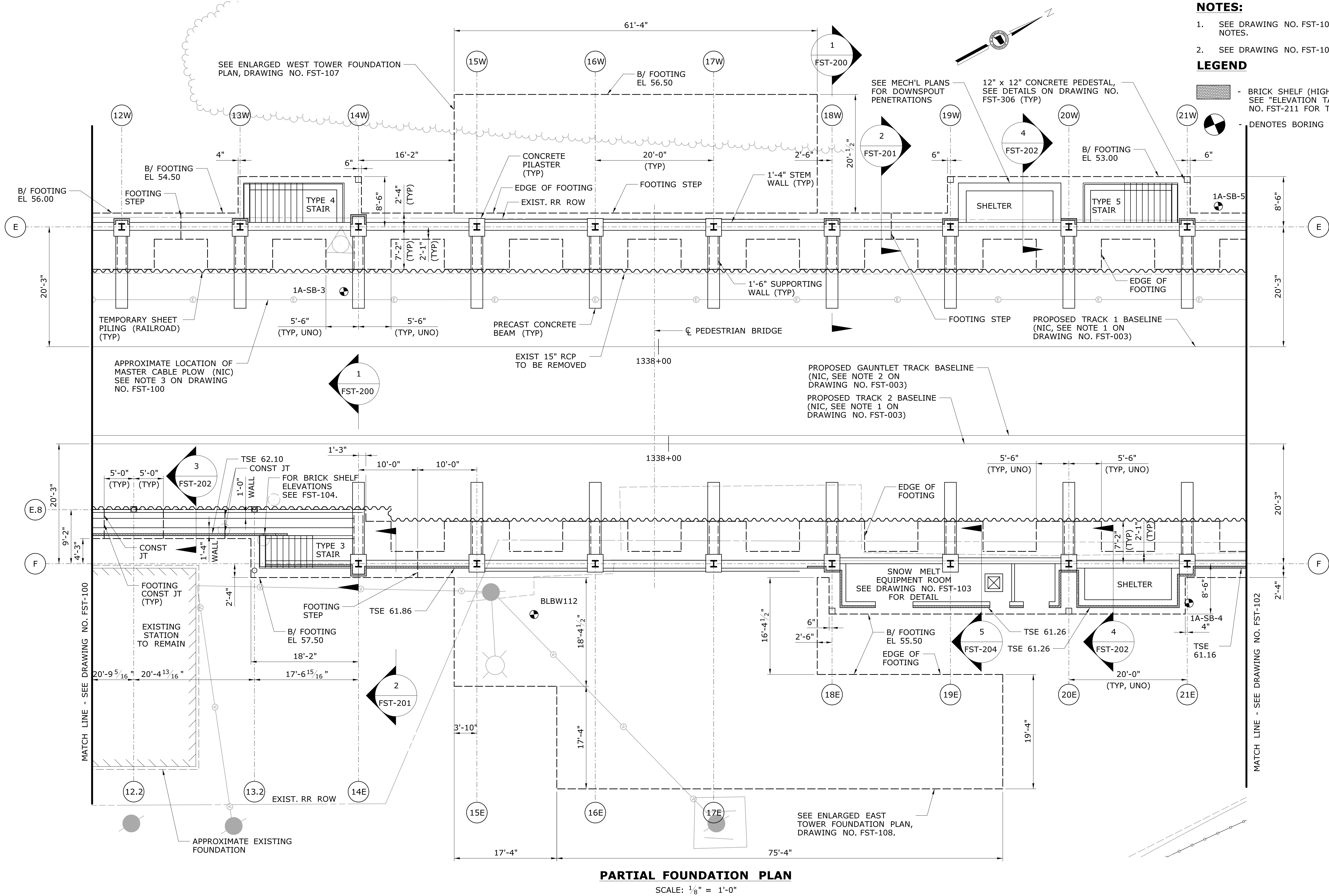


PARTIAL FOUNDATION PLAN

SCALE: 1/8" = 1'-0"

| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|

- NOTES:**
- SEE DRAWING NO. FST-100 FOR FOUNDATION PLAN NOTES.
 - SEE DRAWING NO. FST-104 FOR STAIR DETAILS.
- LEGEND**
- BRICK SHELF (HIGHLIGHTED FOR CLARITY) SEE "ELEVATION TABLE" ON DRAWING NO. FST-211 FOR TSE AT PILASTERS.
 - DENOTES BORING



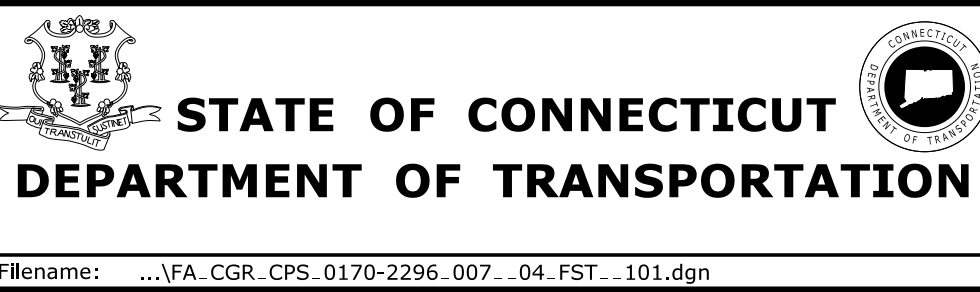
PARTIAL FOUNDATION PLAN
SCALE: 1/8" = 1'-0"

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED



Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_101.dgn

SIGNATURE/
BLOCK:



TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:

**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:

BERLIN

DRAWING TITLE:

FOUNDATION PLAN 2

PROJECT NO.

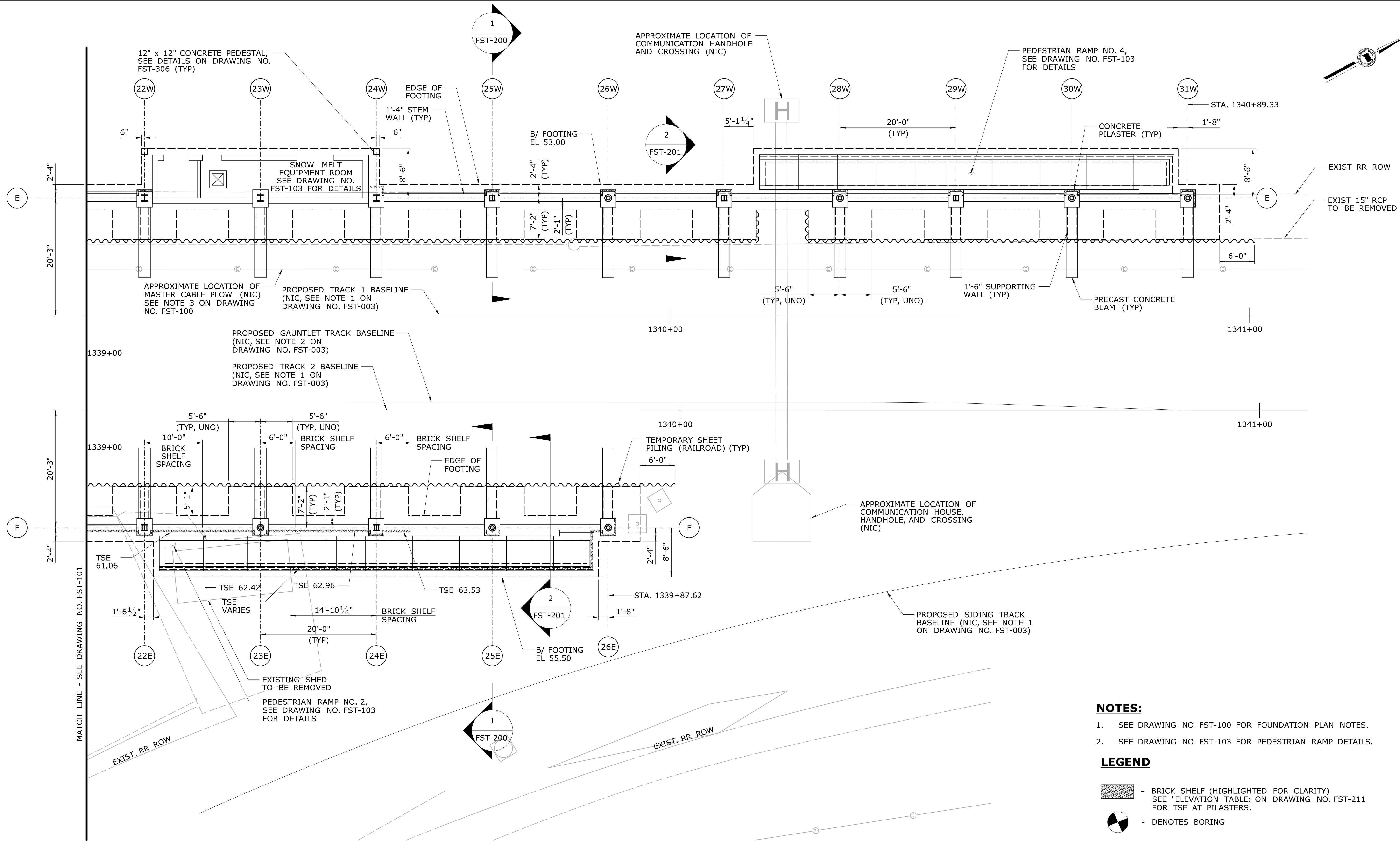
170-3155

DRAWING NO.

FST-101

SHEET NO.

01.12.006



PARTIAL FOUNDATION PLAN
SCALE: 1/8" = 1'-0"

- NOTES:**
- SEE DRAWING NO. FST-100 FOR FOUNDATION PLAN NOTES.
 - SEE DRAWING NO. FST-103 FOR PEDESTRIAN RAMP DETAILS.

- LEGEND**
- BRICK SHELF (HIGHLIGHTED FOR CLARITY) SEE "ELEVATION TABLE: ON DRAWING NO. FST-211 FOR TSE AT PILASTERS.
 - DENOTES BORING

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_102.dgn

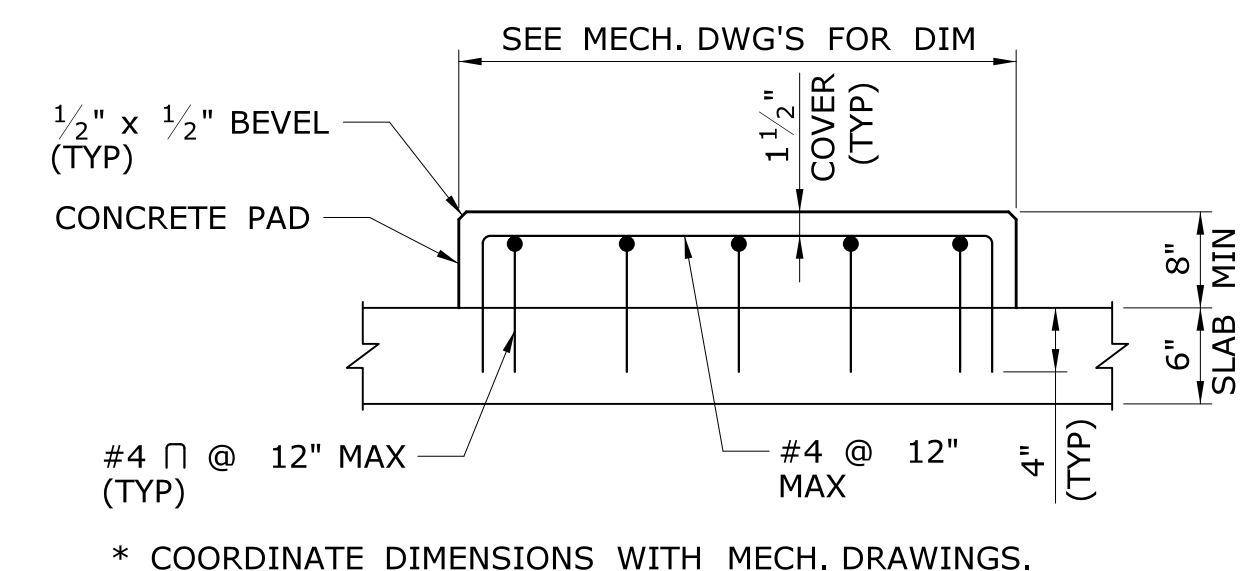
SIGNATURE/
BLOCK:

TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
FOUNDATION PLAN 3

PROJECT NO.
170-3155
DRAWING NO.
FST-102
SHEET NO.
01.12.007




EQUIPMENT PAD DETAIL
N.T.S.



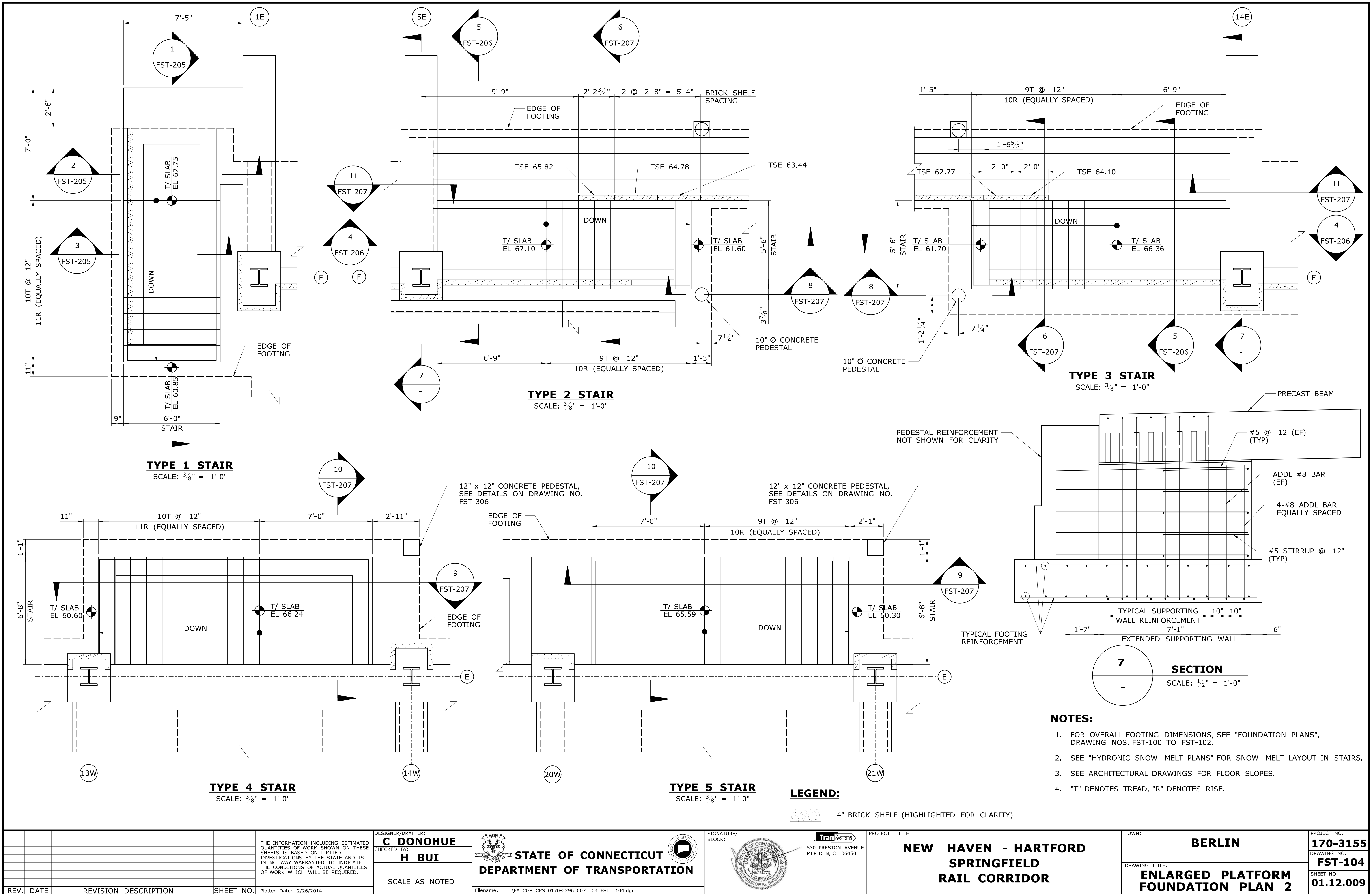
NOTES:

1. FOR OVERALL FOOTING DIMENSIONS, SEE "FOUNDATION PLANS", DRAWING NOS. FST-100 TO FST-102.
2. SEE "HYDRONIC SNOW MELT PLANS" FOR SNOW MELT LAYOUT AT PEDESTRIAN RAMP.
3. SEE ARCHITECTURAL DRAWINGS FOR RAMP AND FLOOR SLOPES AT SNOW MELT EQUIPMENT ROOM AND CTDOT STORAGE ROOM.

LEGEND

 - 4" BRICK SHELF (HIGHLIGHTED FOR CLARITY)

[illegible]



| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

Plotted Date: 2/26/2014

| |
|---------------------------------------|
| DESIGNER/DRAFTER: C DONOHUE |
| CHECKED BY: H BUI |
| SCALE AS NOTED |

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_104.dgn

SIGNATURE/
BLOCK:

TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

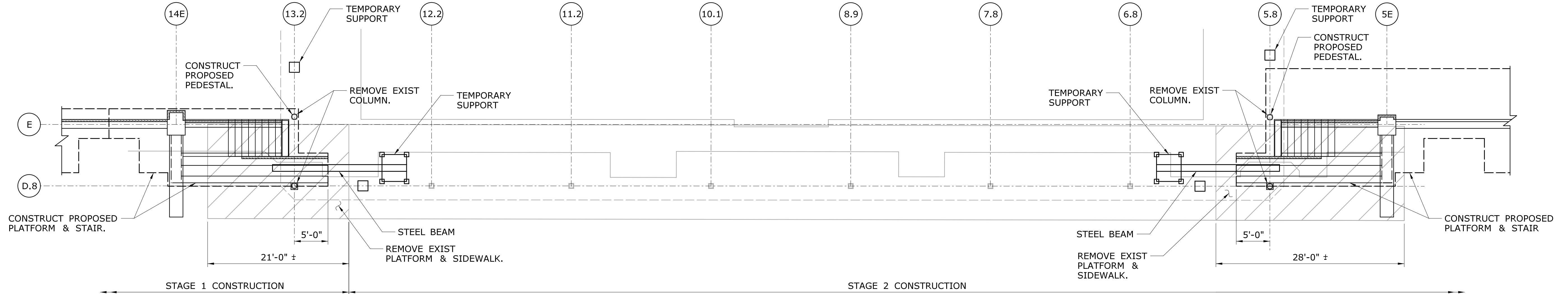
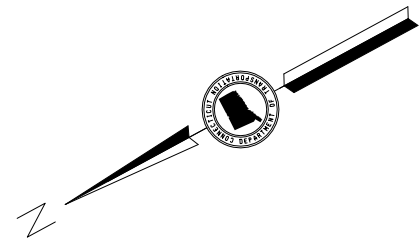
TOWN:
BERLIN

DRAWING TITLE:
**ENLARGED PLATFORM
FOUNDATION PLAN 2**

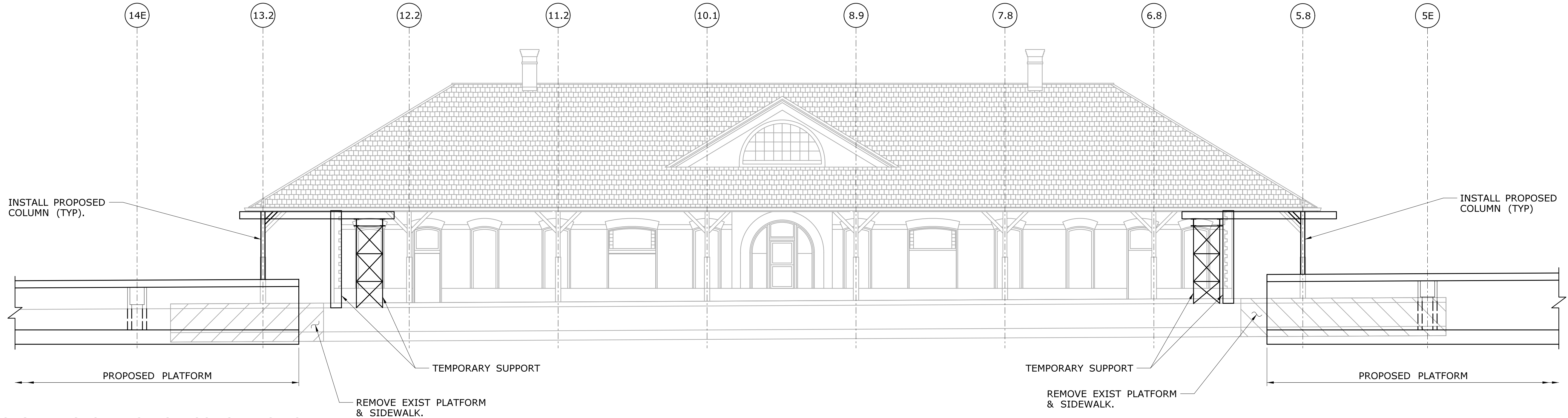
PROJECT NO.
170-3155

DRAWING NO.
FST-104

SHEET NO.
01.12.009



STEP 1 PLAN
SCALE: 1/8" = 1'-0"



STEP 1 ELEVATION
SCALE: 1/8" = 1'-0"

SUGGESTED SEQUENCE OF CONSTRUCTION:

STEP 1 (DURING STAGE 1 CONSTRUCTION)

1. INSTALL TEMPORARY SUPPORTS AROUND COLUMN LINE (13.2) AS SHOWN ON THE PLAN.
2. REMOVE EXISTING PLATFORM, EXISTING SIDEWALK AND EXISTING COLUMNS TO THE LIMITS SHOWN ON THE PLAN.
3. CONSTRUCT PROPOSED PLATFORM, STAIR, AND PEDESTAL TO THE LIMITS SHOWN ON THE PLAN.
4. INSTALL PROPOSED COLUMNS AS SHOWN ON THE PLAN.

STEP 1 (DURING STAGE 2 CONSTRUCTION)

5. REPEAT ALL THE ABOVE AT COLUMN LINE (5.8)

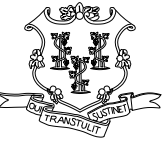
NOTE:
SEE "STAGING PLANS" FOR OVERALL CONSTRUCTION STAGING.

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED


STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_105.dgn

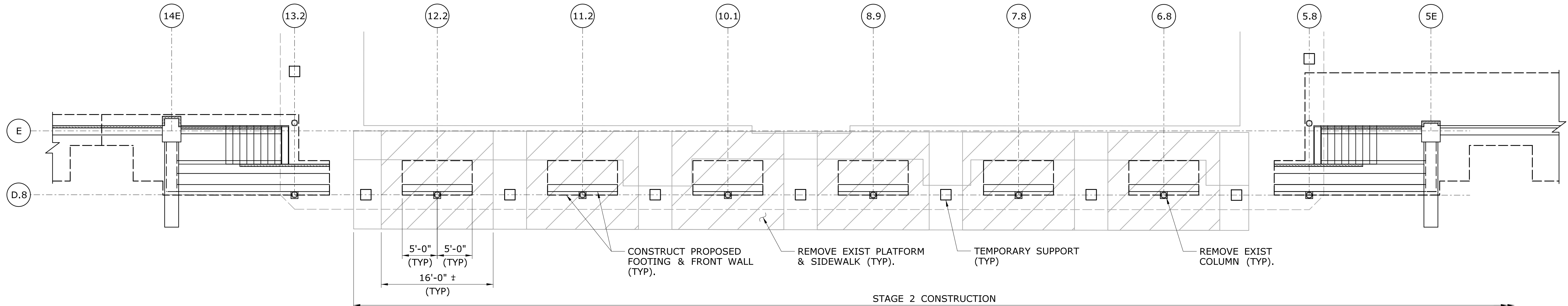
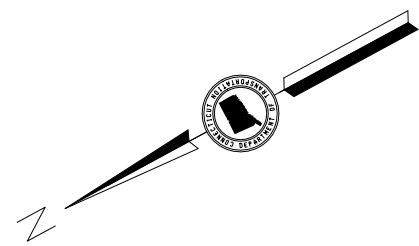
SIGNATURE/
BLOCK:

TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

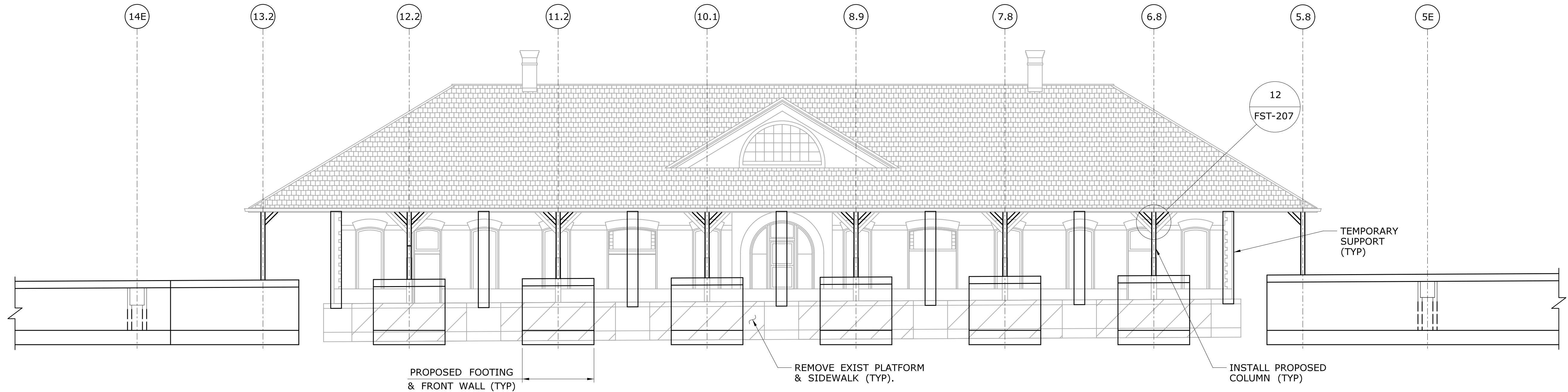
PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**SUGGESTED SEQUENCE
OF CONSTRUCTION 1**

PROJECT NO.
170-3155
DRAWING NO.
FST-105
SHEET NO.
01.12.010



STEP 2 PLAN
SCALE: 1/8" = 1'-0"



STEP 2 ELEVATION
SCALE: 1/8" = 1'-0"

SUGGESTED SEQUENCE OF CONSTRUCTION (CON'T):



STEP 1 (DURING STAGE 2 CONSTRUCTION)

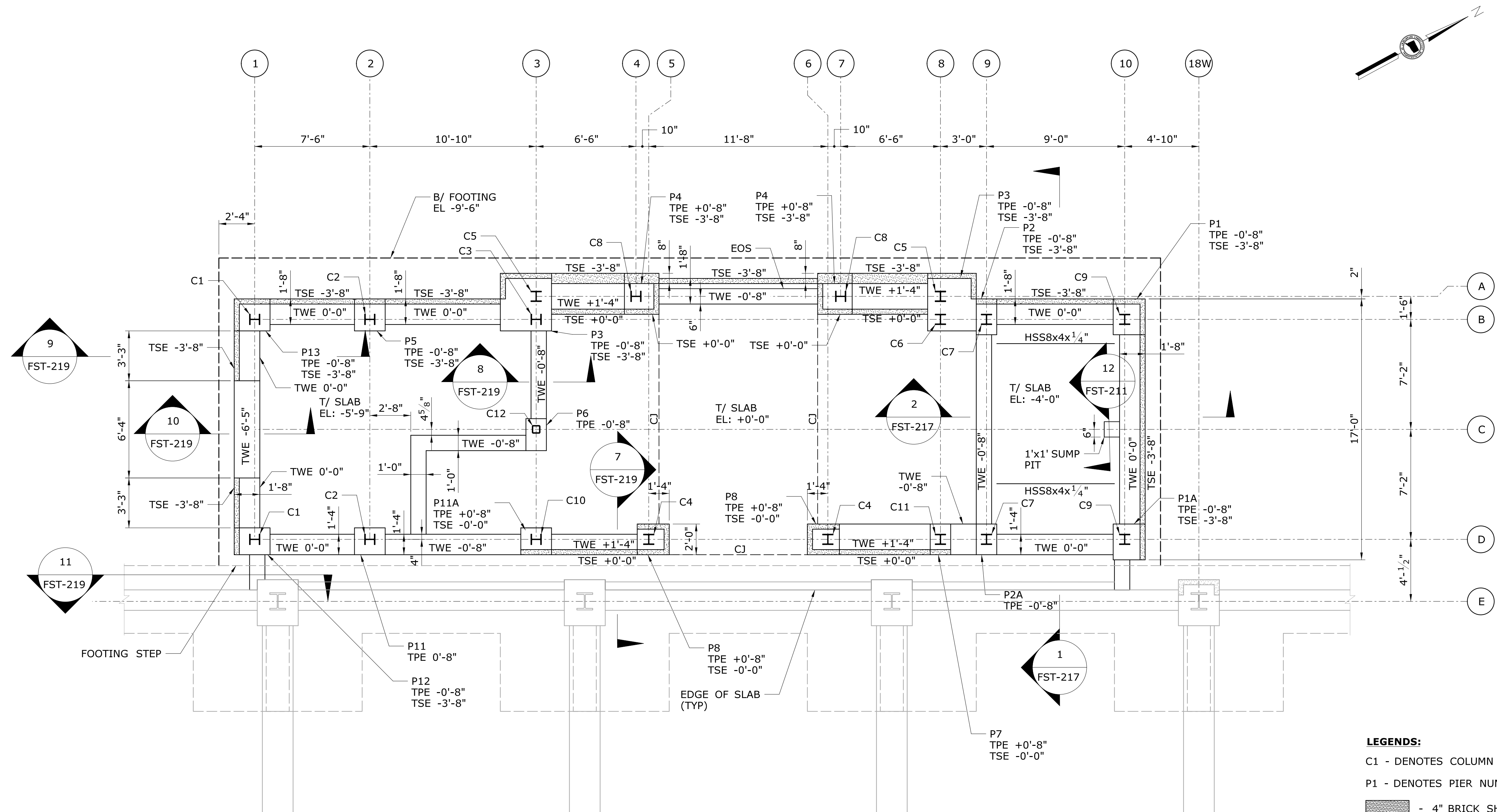
1. INSTALL TEMPORARY SUPPORTS BETWEEN THE REMAIN EXISTING COLUMNS.
2. REMOVE EXISTING PLATFORM, EXISTING SIDEWALK AND EXISTING COLUMNS TO THE LIMITS SHOWN ON THE PLAN.
3. CONSTRUCT PROPOSED FOOTING AND FRONT SUPPORTING WALL TO THE LIMITS SHOWN ON THE PLAN.
4. INSTALL THE REMAINING PROPOSED COLUMNS.

FINAL STEP (DURING STAGE 2 CONSTRUCTION)

1. REMOVE ALL THE TEMPORARY SUPPORTS.
2. REMOVE THE REMAINING EXISTING PLATFORM AND SIDEWALK.
3. CONSTRUCT THE REMAINING FOOTING.
4. CONSTRUCT THE REMAINING FRONT SUPPORT WALL AND THE ENTIRE BACK SUPPORTING WALL.
5. CONSTRUCT SLAB AND THE REMAINING WORK.

NOTE:
SEE "STAGING PLANS" FOR OVERALL CONSTRUCTION STAGING.

| | | | | | | | | | | | | |
|------|------|----------------------|---|--|--|--|---|---|------------------------|---|--|--|
| | | | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | DESIGNER/DRAFTER: C DONOHUE CHECKED BY: H BUI | |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_106.dgn |  TranSystems 530 PRESTON AVENUE MERIDEN, CT 06450 | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | TOWN: BERLIN | DRAWING TITLE: SUGGESTED SEQUENCE OF CONSTRUCTION 2 | PROJECT NO. 170-3155 DRAWING NO. FST-106 SHEET NO. 01.12.011 | |
| | | | | SCALE AS NOTED | | | | | | | | |
| | | | | | | | | | | | | |
| | | | | | | | | | | | | |
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. | Plotted Date: 2/26/2014 | | | | | | | | |



ENLARGED WEST TOWER FOUNDATION PLAN

SCALE: 1/4" = 1'-0"

LEGENDS:

C1 - DENOTES COLUMN NUMBER

P1 - DENOTES PIER NUMBER

4" BRICK SHELF (TYP, UNO)
(HIGHLIGHTED FOR CLARITY)

NOTES:

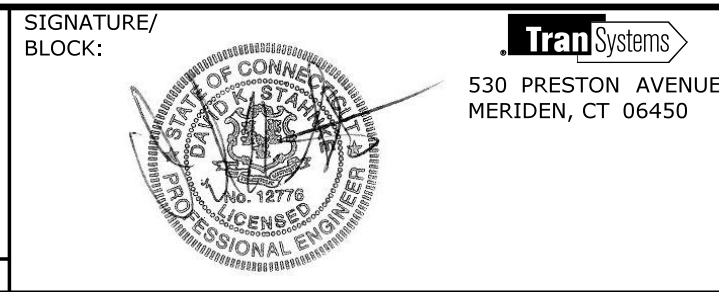
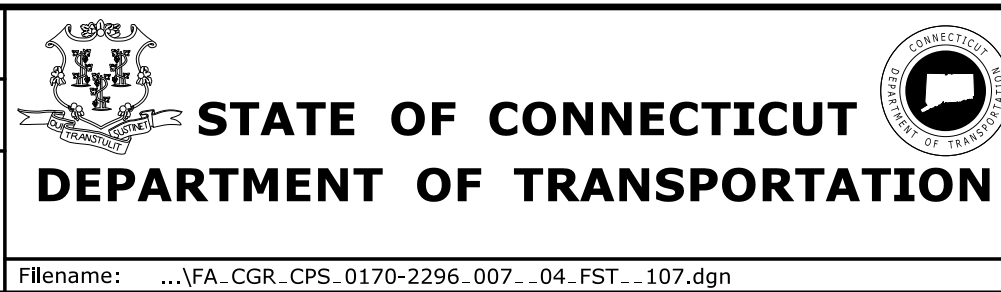
- TOP OF SLAB ELEVATION USED FOR WEST TOWER = 65.98'. THIS IS THE DATUM ELEVATION = 0'-0", ALL ELEVATIONS ARE REFERENCED FROM THIS ELEVATION.
- TOP OF STEEL EL = -0'-8".
- FOR MAT FOOTING PLAN DIMENSIONS, SEE DRAWING NO. FST-101.
- FOR FOUNDATION NOTES, SEE DRAWING NO. FST-200.
- FOR PIER DETAILS, SEE DRAWING NO. FST-220. FOR FINAL PIER ELEVATIONS SEE DRAWING NO. FST-218

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

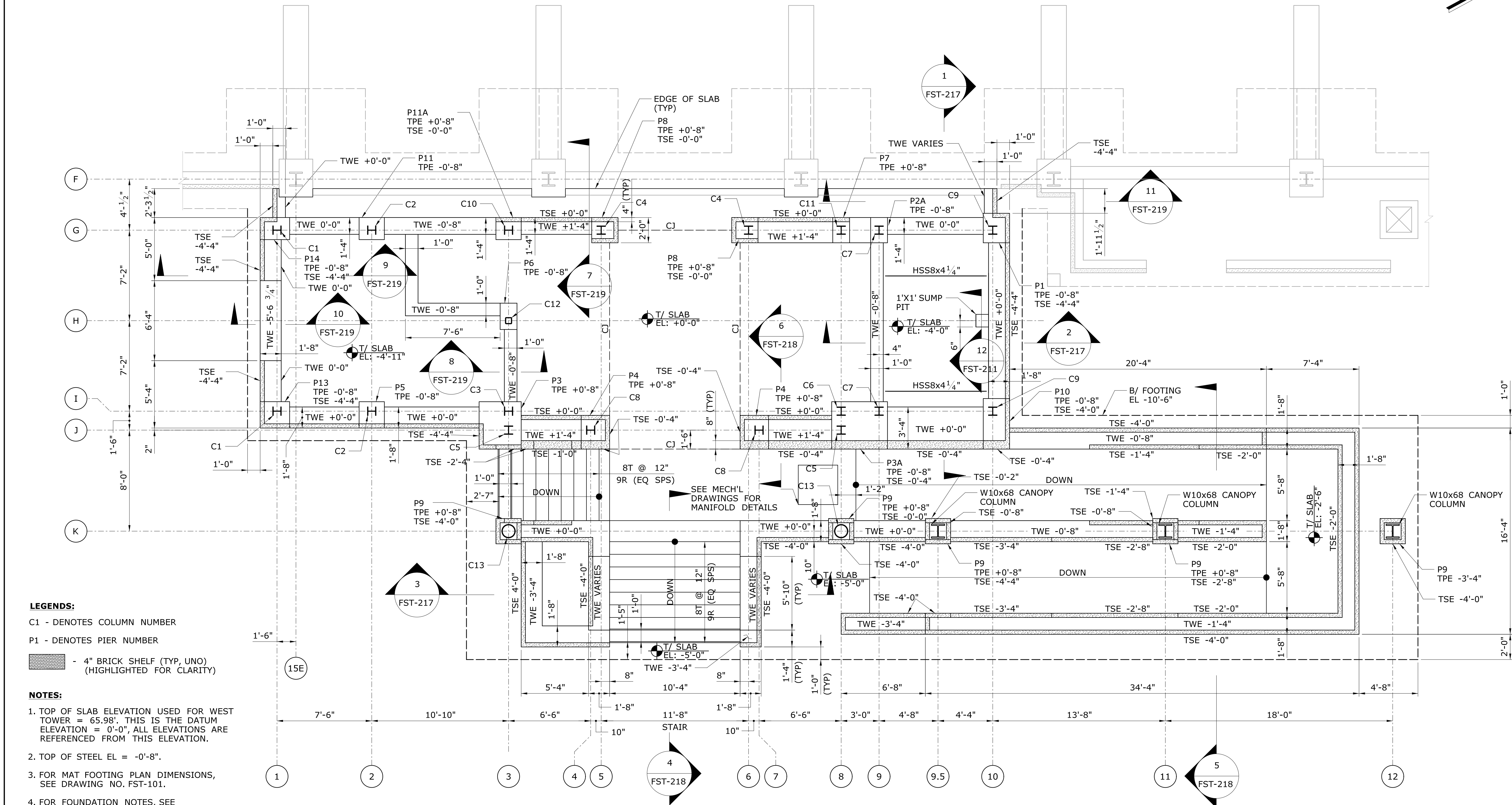
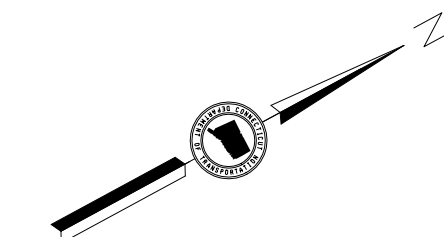
Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
J POPOLI
CHECKED BY:
H BUI
SCALE AS NOTED



PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

| | | | |
|----------------|--|-------------|------------------|
| TOWN: | BERLIN | PROJECT NO. | 170-3155 |
| DRAWING TITLE: | ENLARGED WEST TOWER FOUNDATION PLAN | DRAWING NO. | FST-107 |
| | | SHEET NO. | 01.12.012 |



LEGENDS:
C1 - DENOTES COLUMN NUMBER
P1 - DENOTES PIER NUMBER
- 4" BRICK SHELF (TYP, UNO)
(HIGHLIGHTED FOR CLARITY)

NOTES:
1. TOP OF SLAB ELEVATION USED FOR WEST TOWER = 65.98'. THIS IS THE DATUM ELEVATION = 0'-0", ALL ELEVATIONS ARE REFERENCED FROM THIS ELEVATION.
2. TOP OF STEEL EL = -0'-8".
3. FOR MAT FOOTING PLAN DIMENSIONS, SEE DRAWING NO. FST-101.
4. FOR FOUNDATION NOTES, SEE DRAWING NO. FST-200.
5. FOR PIER DETAILS, SEE DRAWING NO. FST-220. FOR FINAL PIER ELEVATIONS SEE DRAWING NO. FST-218.
6. REFER TO MECHANICAL DRAWINGS FOR SNOWMELT MANIFOLD LOCATIONS.

ENLARGED EAST TOWER FOUNDATION PLAN
SCALE: 1/4" = 1'-0"

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
J POPOLI
CHECKED BY:
H BUI
SCALE AS NOTED

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...FA_CGR_CPS_0170-2296_007_04_FST_108.dgn

SIGNATURE/BLOCK:

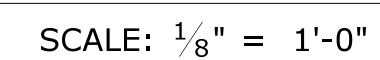
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

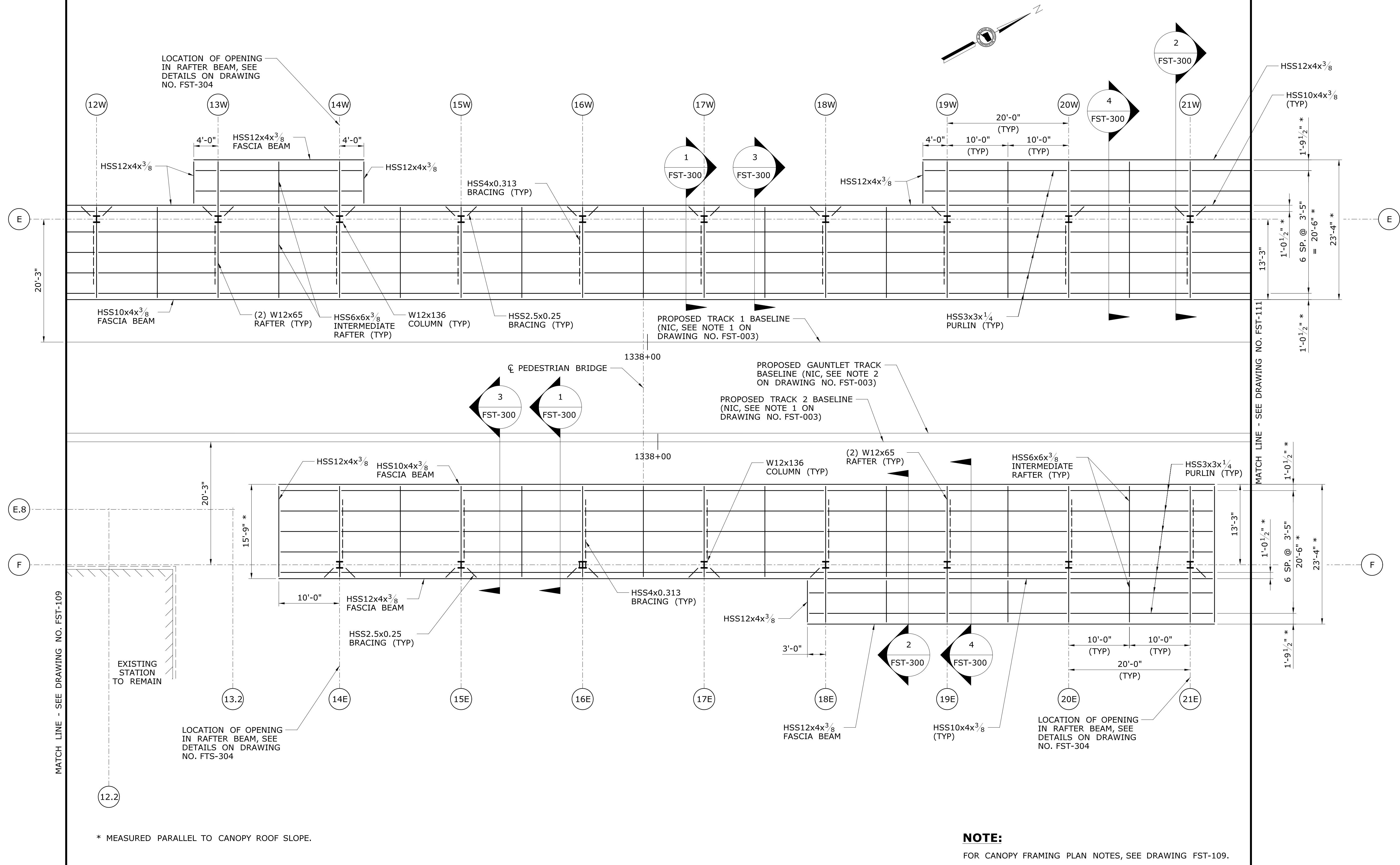
PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**ENLARGED EAST TOWER
FOUNDATION PLAN**




PROJECT NO.
170-3155
DRAWING NO.
FST-108
SHEET NO.
01.12.013

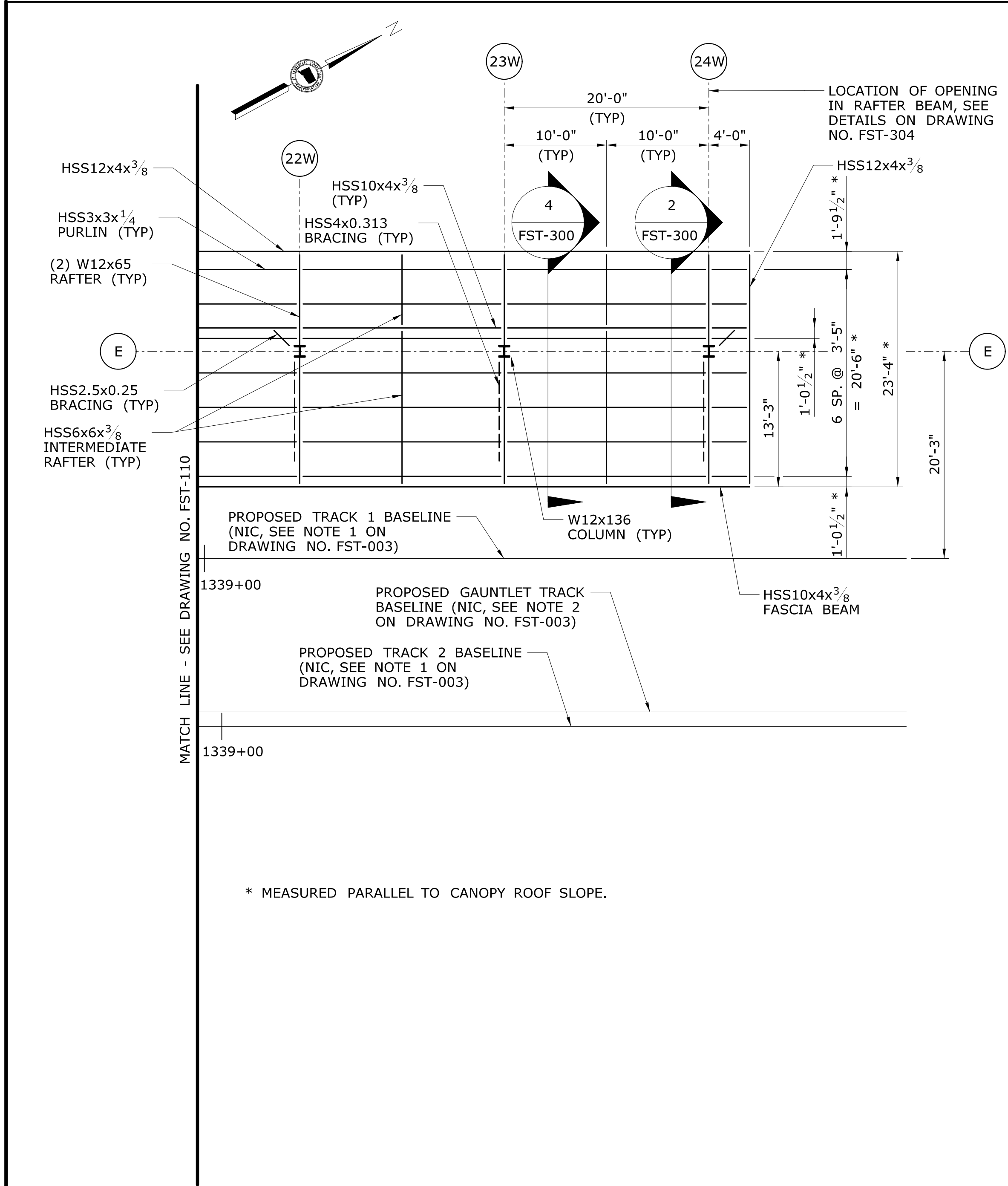
1. FOR GENERAL NOTES AND STRUCTURAL NOTES, SEE DRAWINGS NOS. FST-001 THRU FST-003.
2. ALL DIMENSIONS ARE HORIZONTAL, UNO.
3. FOR CANOPY ROOF DETAILS, SEE ARCHITECTURAL DRAWINGS.
4. FOR CANOPY DETAILS SEE DRAWINGS NO. FST-300 THRU FST-305.
5. SECTION REFERENCES FOR DETAILS ARE TYPICAL FOR ALL CANOPIES, UNO.

[illegible]

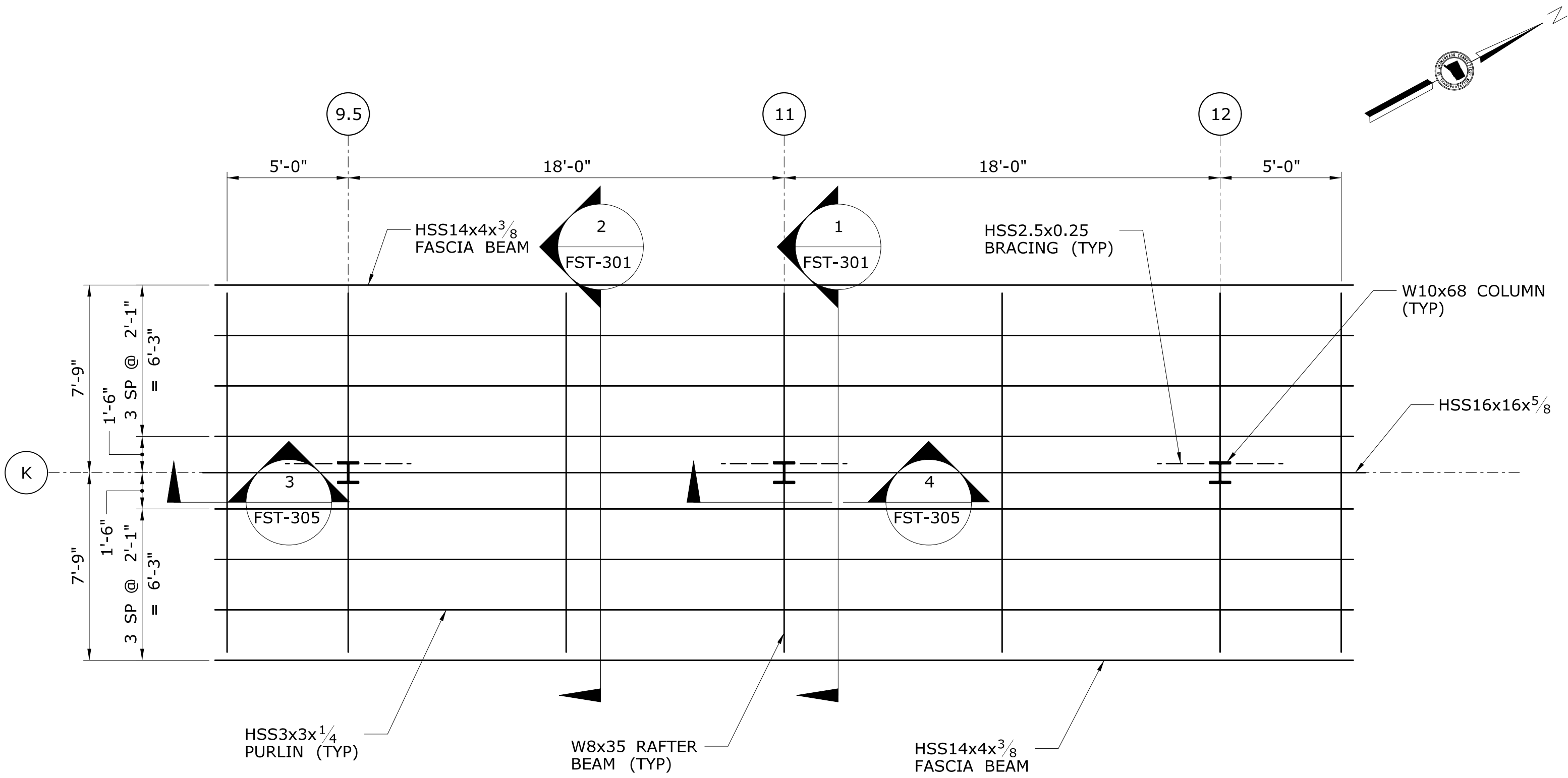


PARTIAL CANOPY FRAMING PLAN
SCALE: 1/8" = 1'-0"

| | | | | | | | | | | | | | | | | |
|------|------|----------------------|---|--|---|---|---|----------------------------|--------------------------------|--|--|--|--|--|--|--|
| | | | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | DESIGNER/DRAFTER: C DONOHUE CHECKED BY: H BUI SCALE AS NOTED |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_110.dgn |  SIGNATURE/ BLOCK:  530 PRESTON AVENUE MERIDEN, CT 06450 | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | TOWN: BERLIN | PROJECT NO. 170-3155 | | | | | | | |
| | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | |
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. | Plotted Date: 2/26/2014 | DRAWING TITLE: CANOPY FRAMING PLAN 2 | | | | | DRAWING NO. FST-110 SHEET NO. 01.12.015 | | | | | | |



PARTIAL CANOPY FRAMING PLAN
SCALE: 1/8" = 1'-0"



ENTRANCE CANOPY FRAMING PLAN
SCALE: 1/4" = 1'-0"

* MEASURED PARALLEL TO CANOPY ROOF SLOPE.

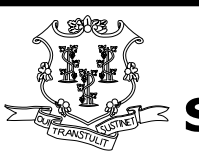
NOTE:
FOR CANOPY FRAMING PLAN NOTES, SEE DRAWING NO. FST-109.


| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

**STATE OF CONNECTICUT**
DEPARTMENT OF TRANSPORTATION



Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_111.dgn

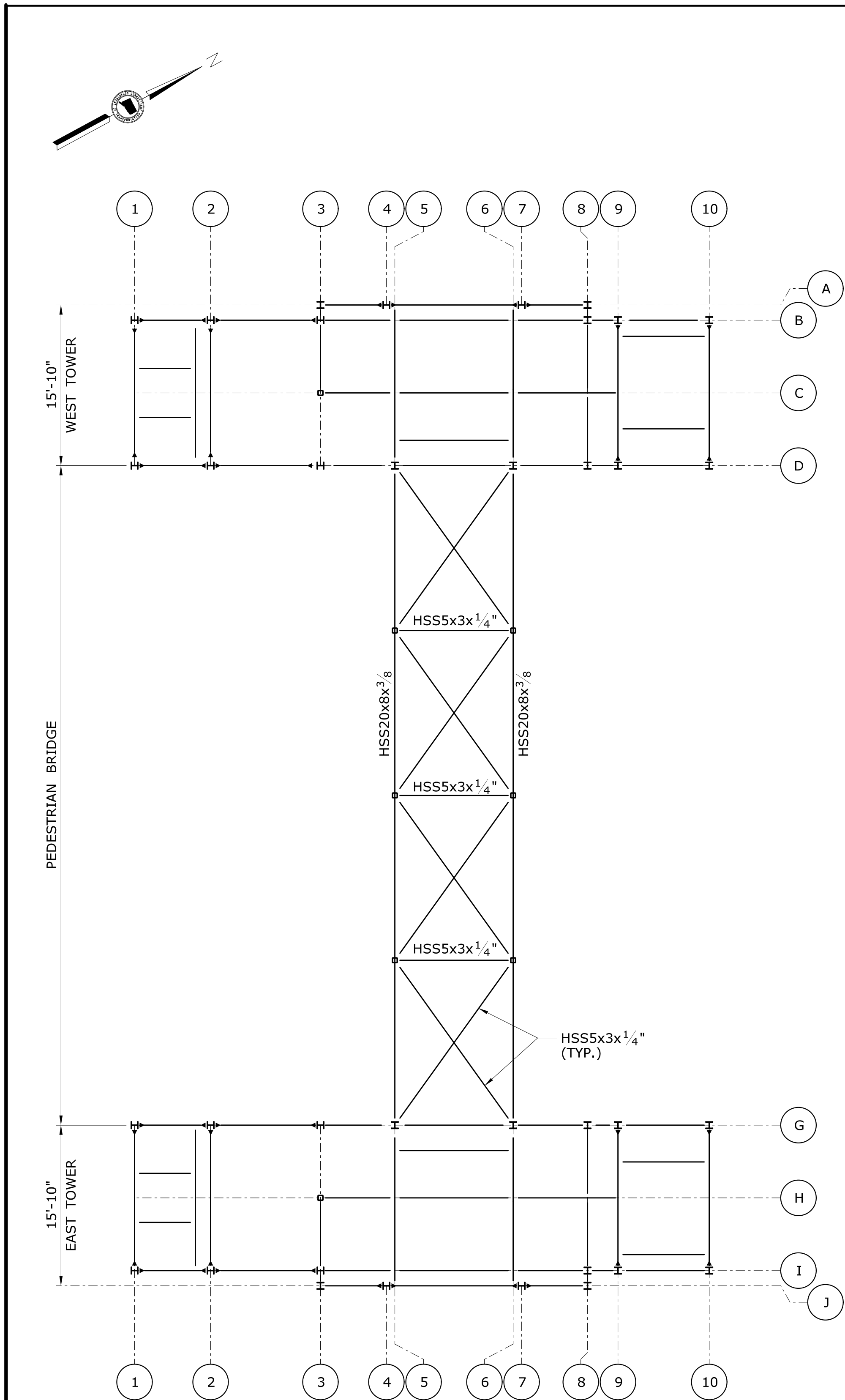
SIGNATURE/
BLOCK:


**TranSystems**
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

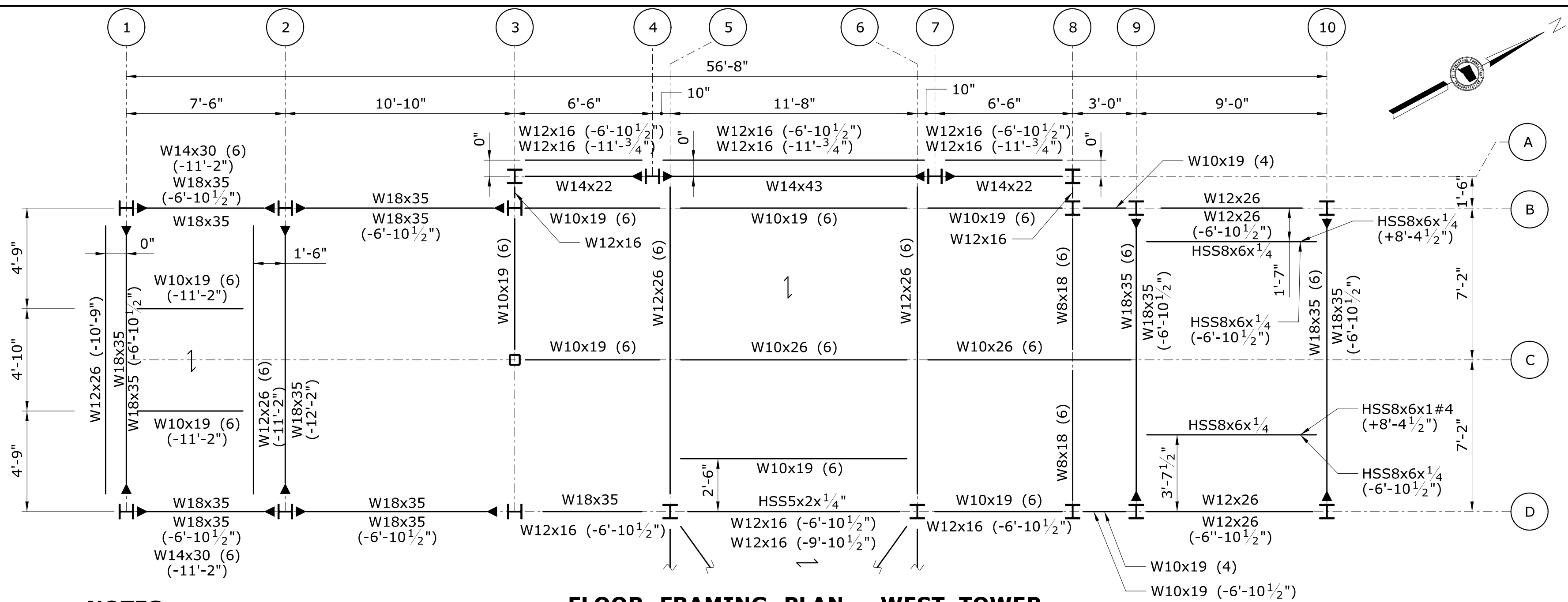
TOWN:
BERLIN
DRAWING TITLE:
**CANOPY FRAMING
PLAN 3**

PROJECT NO.
170-3155
DRAWING NO.
FST-111
SHEET NO.
01.12.016



PEDESTRIAN OVERPASS FLOOR FRAMING PLAN

SCALE: 1/8" = 1'-0"



FLOOR FRAMING PLAN - WEST TOWER

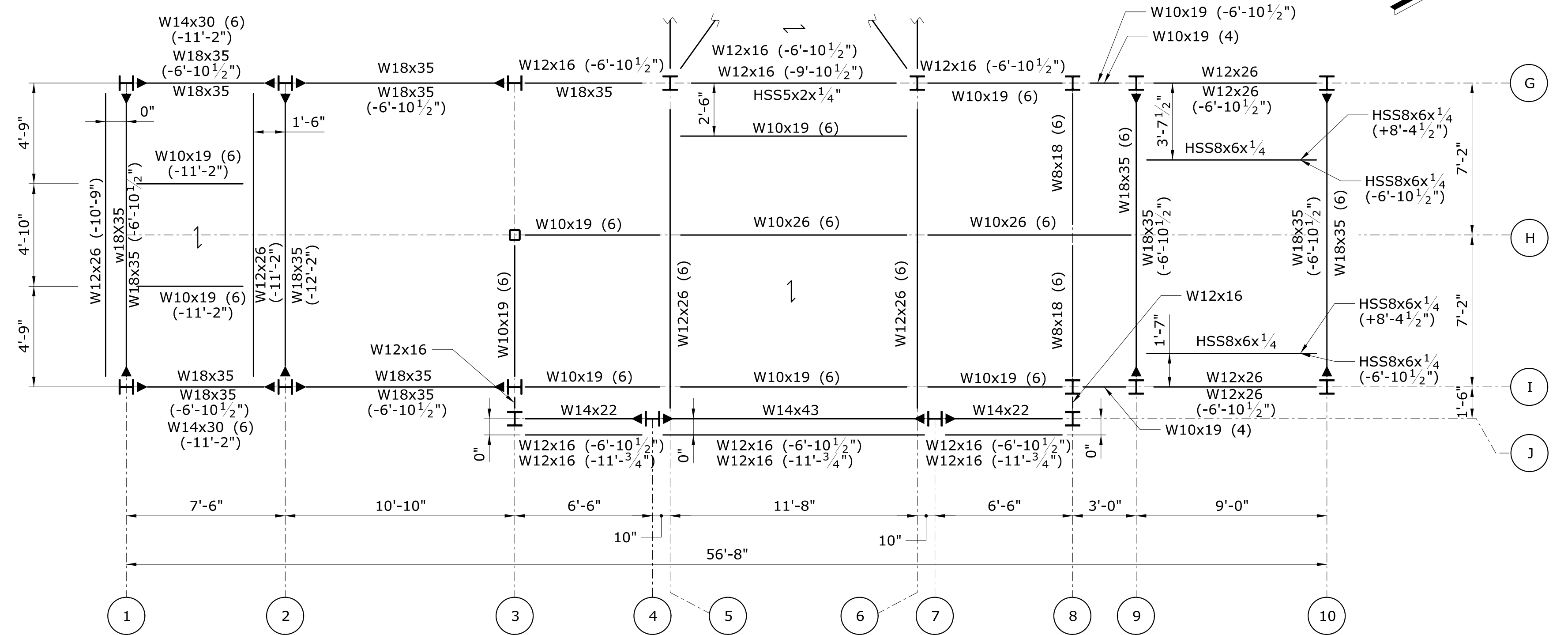
SCALE: 1/4" = 1'-0"

NOTES:

- FOR GENERAL NOTES AND STRUCTURAL NOTES, SEE DRAWING NOS. FST-001 TO FST-003.
- COORDINATE ALL DIMENSIONS AND DETAILS WITH THE ARCHITECTURAL DRAWINGS.
- FOR PLATFORM PLANS, SEE DRAWING NOS. FST-103 TO FST-105.
- FOR TOWER ROOF FRAMING PLANS, SEE DRAWING NOS. FST-112 TO FST-113.
- FOR PEDESTRIAN BRIDGE FRAMING DETAILS, SEE DRAWING NOS. FST-500 TO FST-502.
- SEE DRAWING NO. FST-115 FOR COLUMN SCHEDULE.
- ALL ELEVATIONS ARE BASED ON AN ASSUMED VERTICAL DATUM AT T/SLAB EL (0'-0") ON THE PLATFORM LEVEL. T/STEEL EL = (21'-1") FROM DATUM ELEVATION UNLESS NOTED (±X'-XX") FROM THIS BASE T/STEEL ELEVATION.
- T/SLAB EL AT PEDESTRIAN OVERPASS LEVEL = (21'-6") FROM DATUM ELEVATION.
- 2" NW CONCRETE OVER 3"x18GA COMPOSITE METAL DECK (TOTAL THICKNESS =5") REINFORCE WITH 6x6-W2.1xW2.1 WWF. DECK SHALL SPAN 3 SPANS WHERE ABLE.
- FOR REQUIRED MOMENT CONNECTION CAPACITIES TABLE, SEE DRAWING NO. FST-113.

LEGEND:

- COLUMN
- ▲ MOMENT CONNECTION
- COLUMN
- ↓ DECK SPAN DIRECTION



FLOOR FRAMING PLAN - EAST TOWER

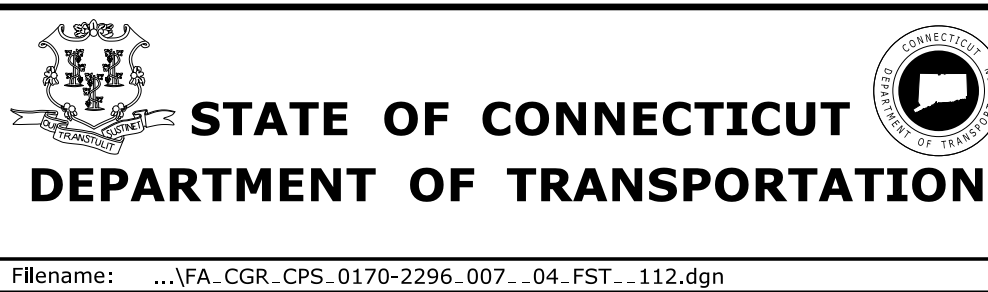
SCALE: 1/4" = 1'-0"

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
J POPOLI
CHECKED BY:
H BUI
SCALE AS NOTED



SIGNATURE/
BLOCK:



TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:

**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:

BERLIN

DRAWING TITLE:

**TOWER FLOOR
FRAMING PLAN**

PROJECT NO.

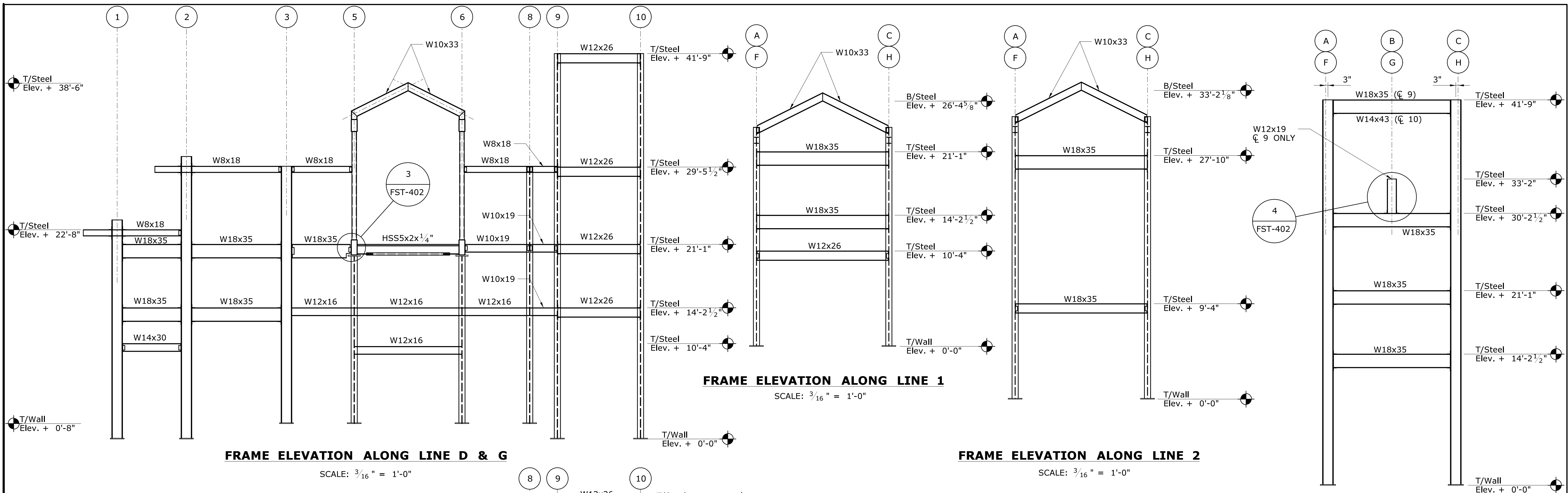
170-3155

DRAWING NO.

FST-112

SHEET NO.

01.12.017

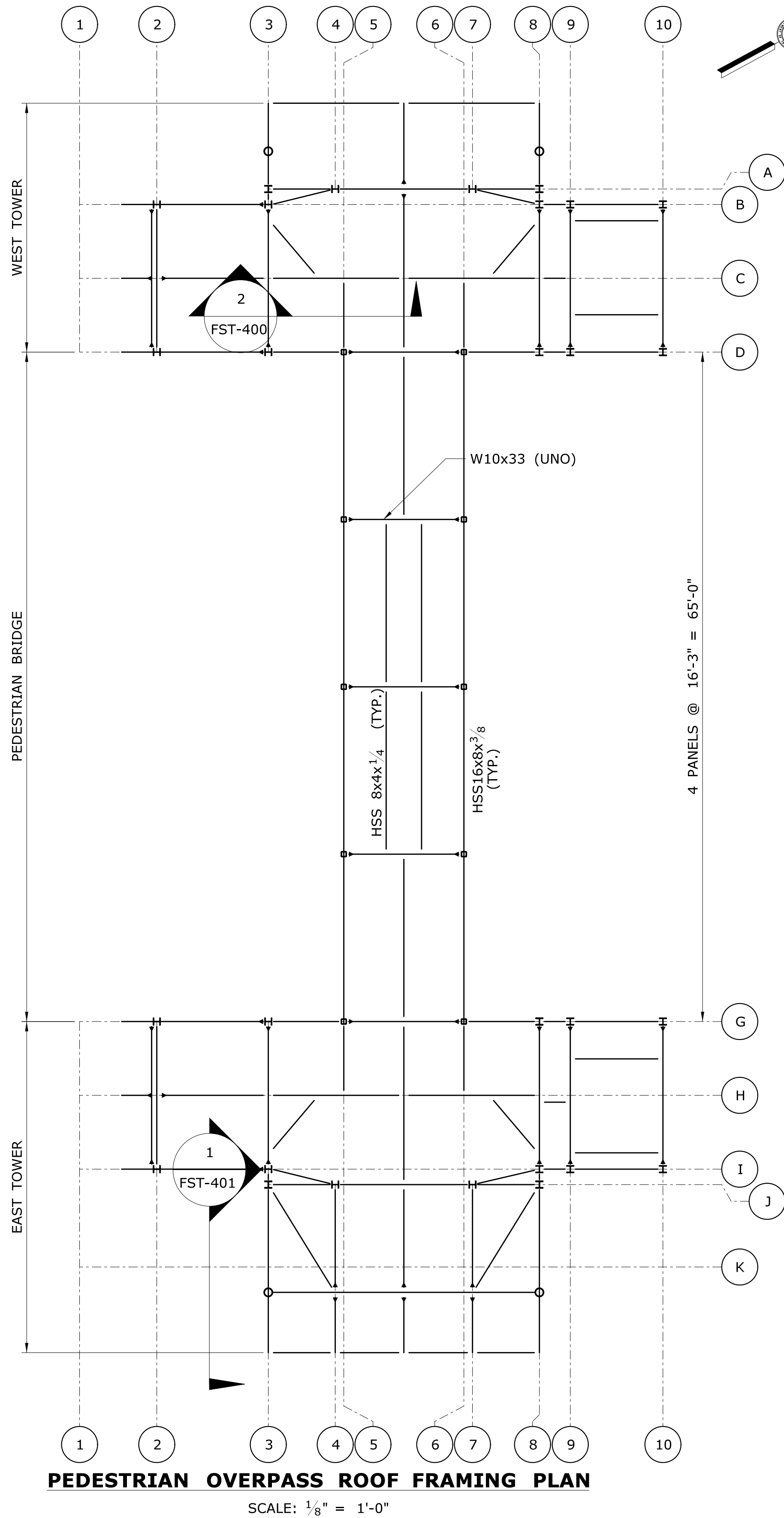


NOTES:

1. FOR STRUCTURAL NOTES, SEE DRAWING NOS. FST-001 TO FST-003.
2. COORDINATE ALL DIMENSIONS AND DETAILS WITH THE ARCHITECTURAL DRAWINGS.
3. ALL ELEVATIONS ARE BASED ON AN ASSUMED VERTICAL DATUM AT T/SLAB EL. (0'-0") OF THE WEST TOWER ON THE PLATFORM LEVEL.
4. SEE DRAWING NO. FST-115 FOR COLUMN SCHEDULE.

REQUIRED MOMENT CONNECTION CAPACITIES

| BEAM | MOMENT (k-ft) |
|--------|---------------|
| W8x18 | 15 |
| W10x33 | 20 |
| W14x22 | 15 |
| W14x30 | 30 |
| W14x43 | 10 |
| W18x35 | 70 |



PEDESTRIAN OVERPASS ROOF FRAMING PLAN

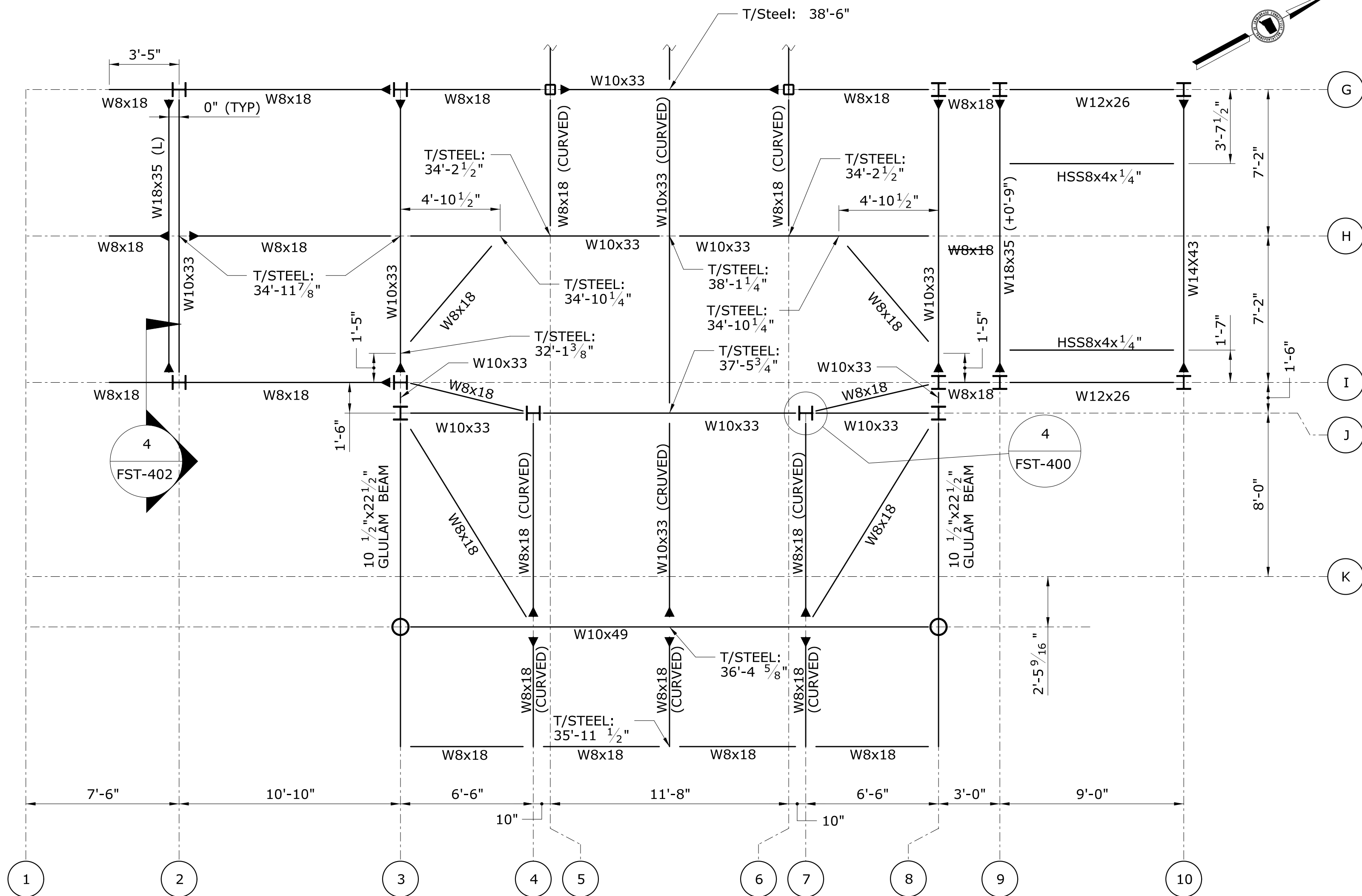
SCALE: 1/8" = 1'-0"

LEGEND:

- ┳ COLUMN
- ▲ MOMENT CONNECTION
- COLUMN
- COLUMN

NOTES:

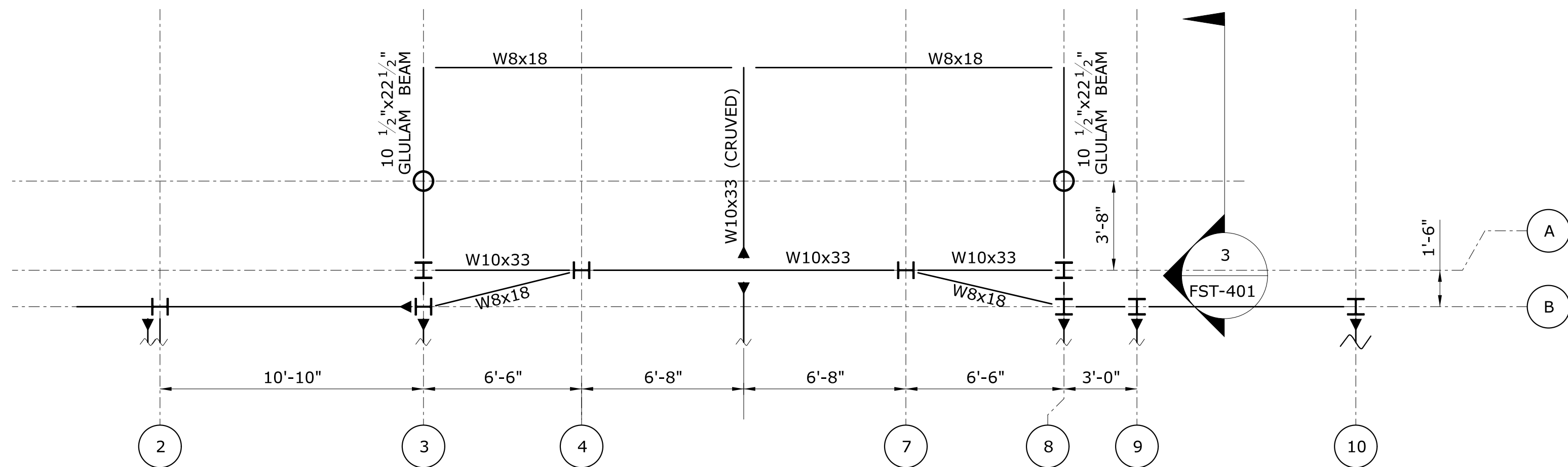
- FOR GENERAL NOTES AND STRUCTURAL NOTES, SEE DRAWING NOS. FST-001 TO FST-003.
- COORDINATE ALL DIMENSIONS AND DETAILS WITH THE ARCHITECTURAL DRAWINGS.
- FOR FRAME ELEVATIONS, SEE DRAWING NO. FST-113.
- FOR TOWER FLOOR FRAMING PLANS, SEE DRAWING NO. FST-112.
- FOR PEDESTRIAN BRIDGE FRAMING DETAILS, SEE DRAWING NOS. FST-500 TO FST-502.
- SEE DRAWING NO. FST-113 FOR COLUMN SCHEDULE.
- ALL ELEVATIONS ARE BASED ON AN ASSUMED VERTICAL DATUM AT T/SLAB EL (0'-0") ON THE PLATFORM LEVEL BELOW. T/STEEL EL = (29'-5 1/2") FROM DATUM ELEVATION UNLESS NOTED (+X'-XX") FROM THIS BASE T/STEEL ELEVATION.
- EAST TOWER ROOF FRAMING IS OPPOSITE WEST TOWER. ALL ELEVATIONS ARE IDENTICAL BETWEEN TOWERS.
- FOR REQUIRED MOMENT CONNECTION CAPACITIES TABLE, SEE DRAWING NO. FST-113.



ROOF FRAMING PLAN - EAST TOWER

(EAST TOWER SHOWN, WEST TOWER OPPOSITE)

SCALE: 1/4" = 1'-0"



ROOF FRAMING PLAN - WEST TOWER


SCALE: 1/4" = 1'-0"

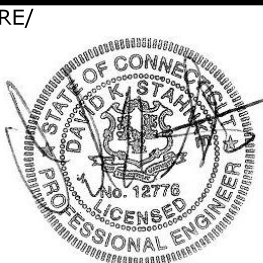
| | | | |
|------|------|----------------------|-----------|
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

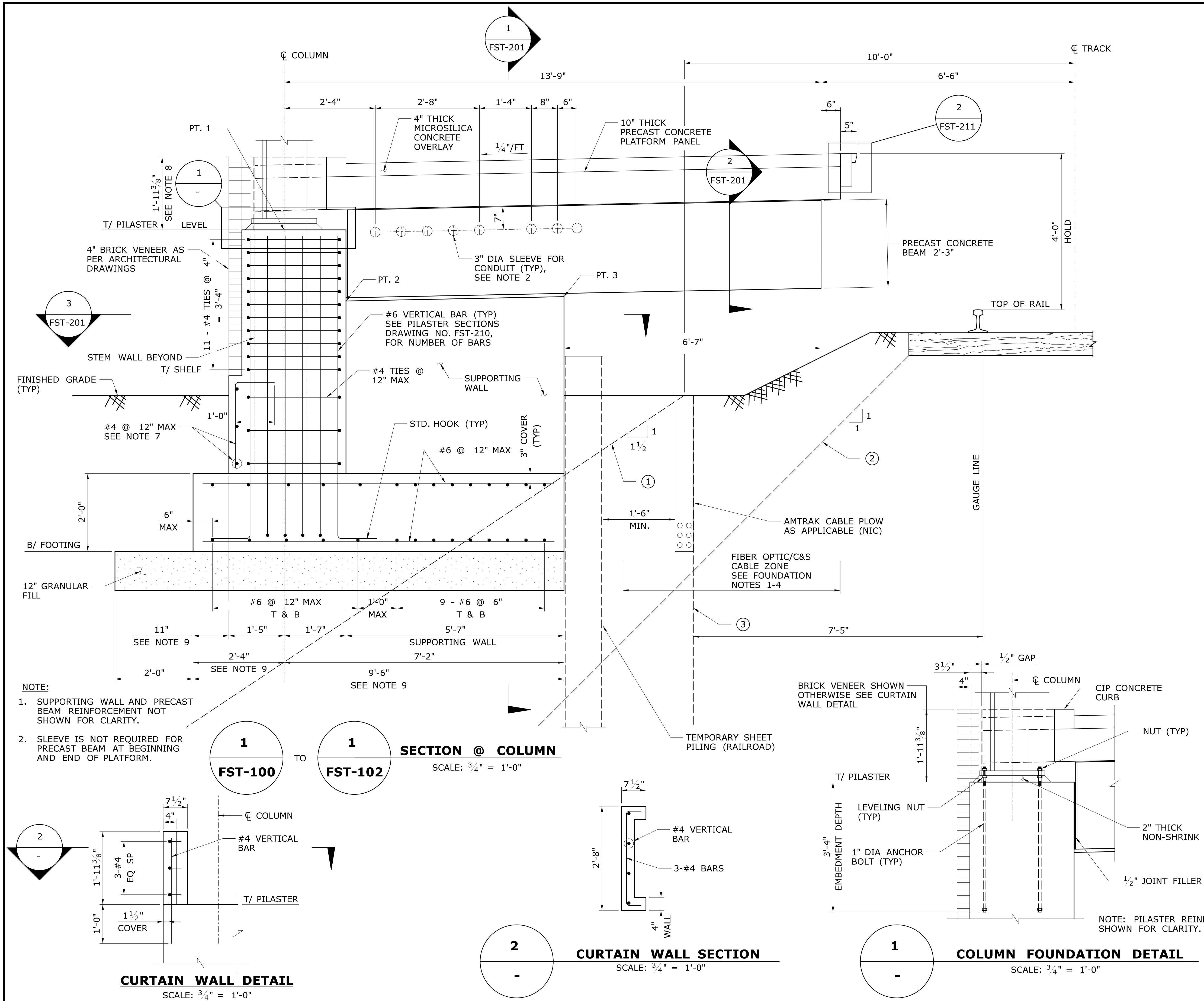
DESIGNER/DRAFTER:
J POPOLI
CHECKED BY:
H BUI
SCALE AS NOTED


STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_114.dgn

SIGNATURE/BLOCK:

TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:
NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR

TOWN:
BERLIN
DRAWING TITLE:
TOWER ROOF
FRAMING PLAN
PROJECT NO.
170-3155
DRAWING NO.
FST-114
SHEET NO.
01.12.019



FOUNDATION NOTES:

- SEE SURVEY PLAN FOR INFORMATION REGARDING ALL EXISTING UTILITY LINE (FIBER OPTIC CABLES, AMTRAK COMMUNICATION AND SIGNAL LINES, ETC). UTILITY OWNERS SHALL BE CONTACTED A MINIMUM OF 14 DAYS IN ADVANCE OF SHEET PILING INSTALLATION OR EXCAVATION AND BE GRANTED ACCESS TO WORK AREA FOR OVERSIGHT OF THE WORK. DRIVING OF SHEETING MAY NOT COMMENCE UNTIL THE UTILITY OWNER AND THE ENGINEER ARE SATISFIED THAT THE WORK WILL NOT ADVERSELY AFFECT THE UTILITY.
- THE CONTRACTOR SHALL PROTECT ALL REMAINING EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION.
- SHEET PILING INSTALLATION SHALL BE IN ACCORDANCE WITH AMTRAK STANDARD ENGINEERING PRACTICES "REQUIREMENTS FOR TEMPORARY SHEETING AND SHORING TO SUPPORT AMTRAK TRACKS", AND WITH THE SPECIAL PROVISION "TEMPORARY SHEET PILING (RAILROAD)". AN APPROVED TRACK MONITORING PROGRAM BY AMTRAK MUST BE IMPLEMENTED DURING THE INSTALLATION OF SHEET PILING.
- HAND DIG EXPLORATORY TRENCHES IN THE AREA OF COMMUNICATION AND SIGNAL CABLES AND OTHER UTILITIES PRIOR TO INSTALLING SHEET PILING.
- CONTRACTOR SHALL MAINTAIN A MINIMUM DEPTH OF 3'-6" FROM FINISH GRADE TO BOTTOM OF FOOTING.
- THE CONTRACTOR SHALL OBTAIN OUTSIDE RAIL ELEVATION AT ALL COLUMN LINE LOCATIONS. ESTABLISHED TOP OF PLATFORM ELEVATIONS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. CONSTRUCTION WILL NOT BE PERMITTED WITHOUT PRIOR APPROVAL ON THE ENGINEER.

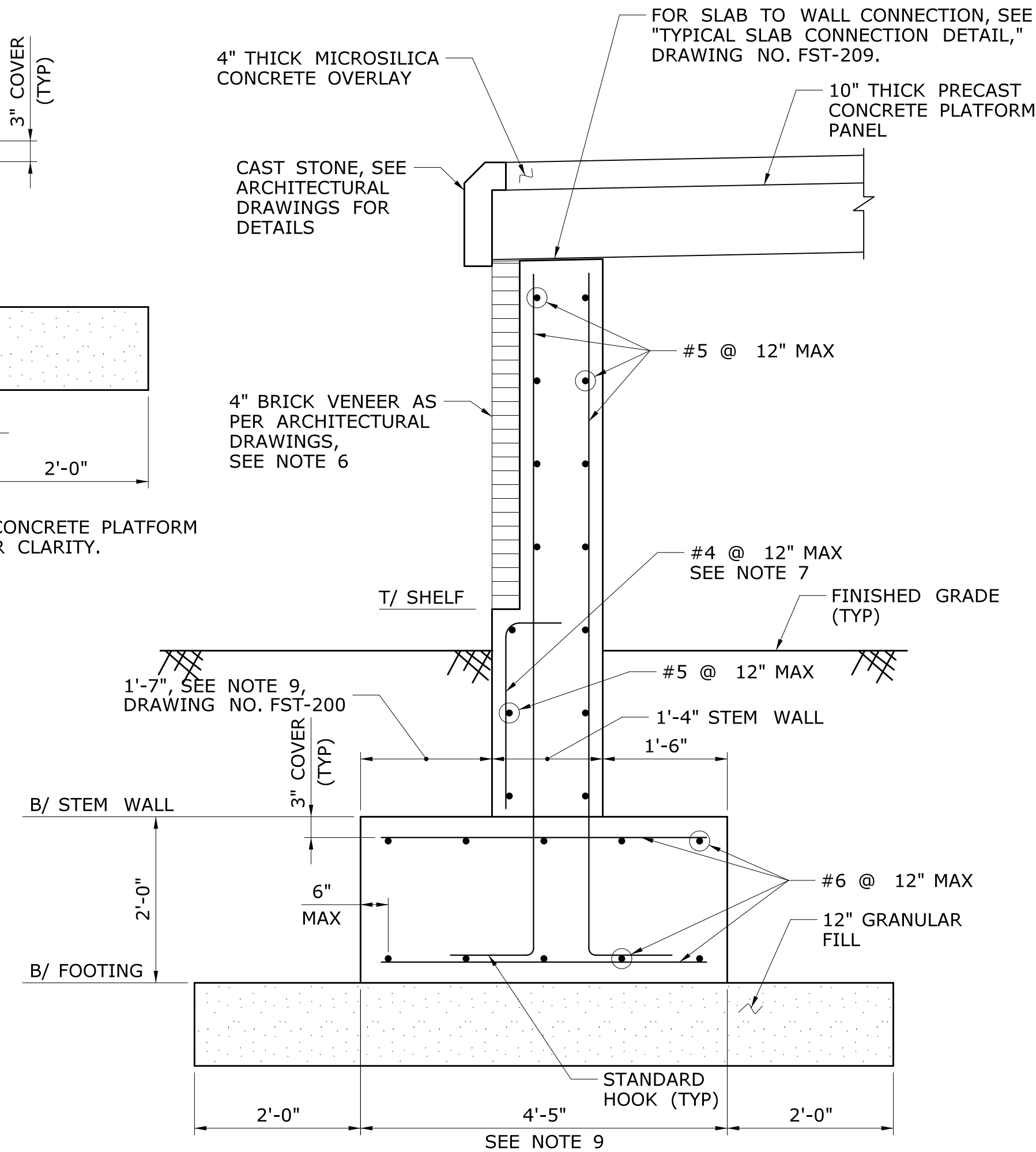
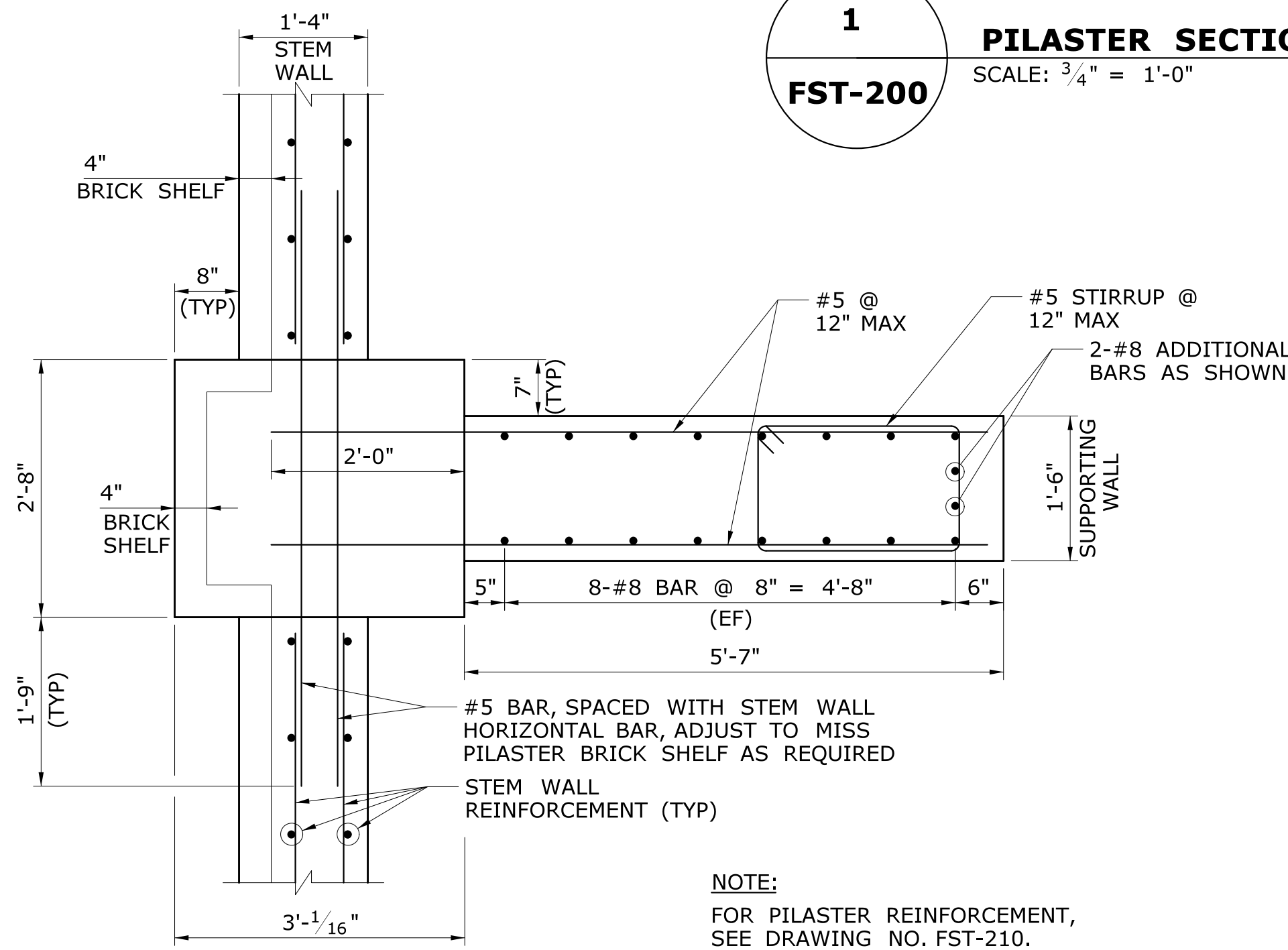
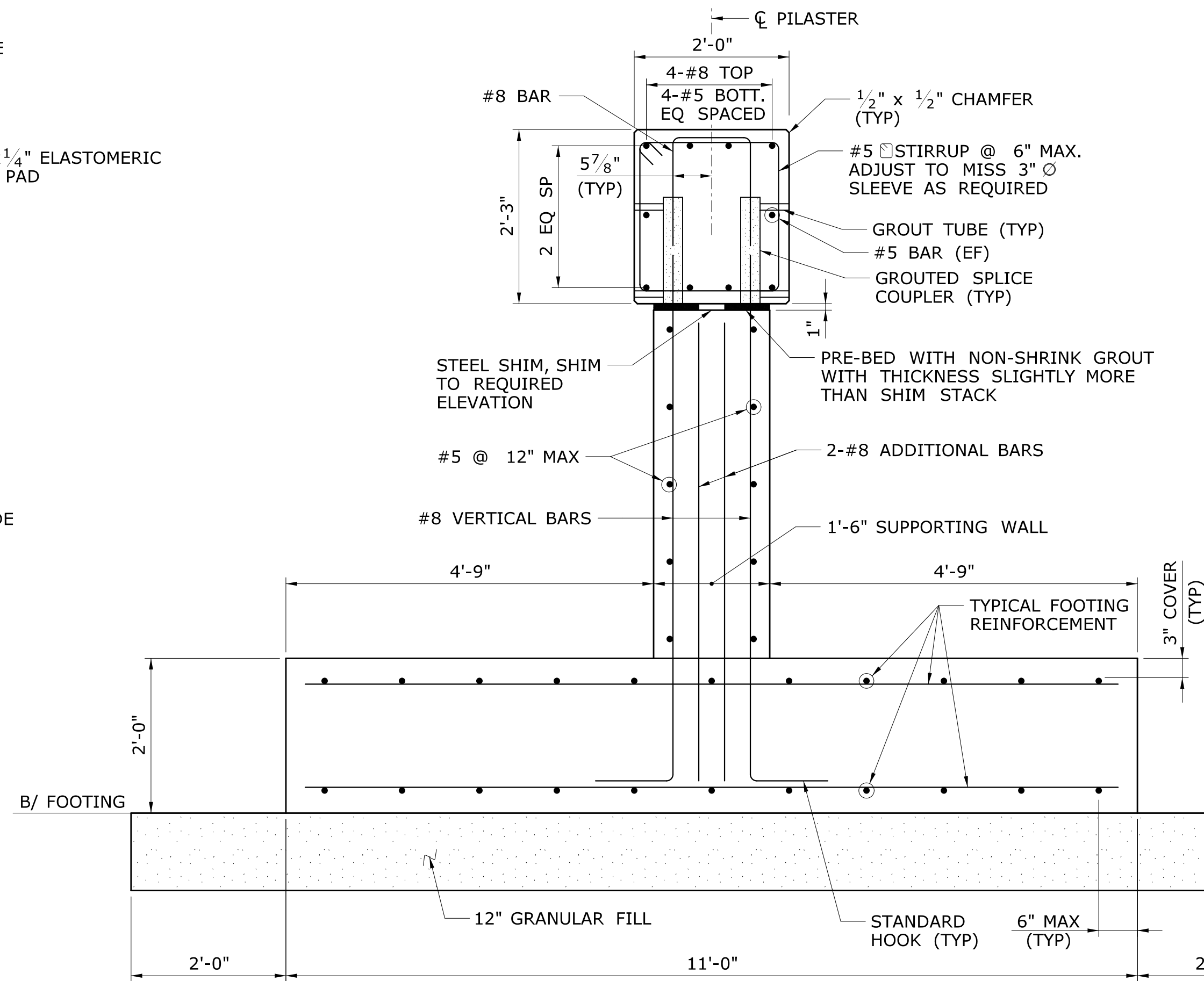
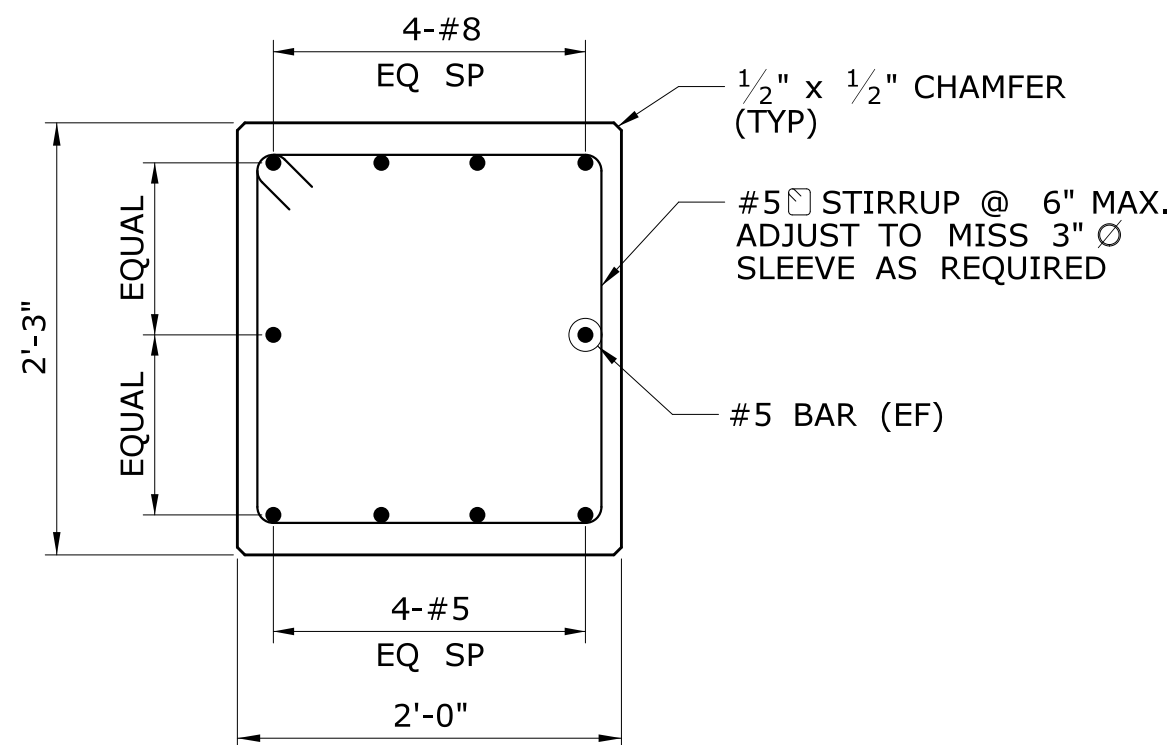
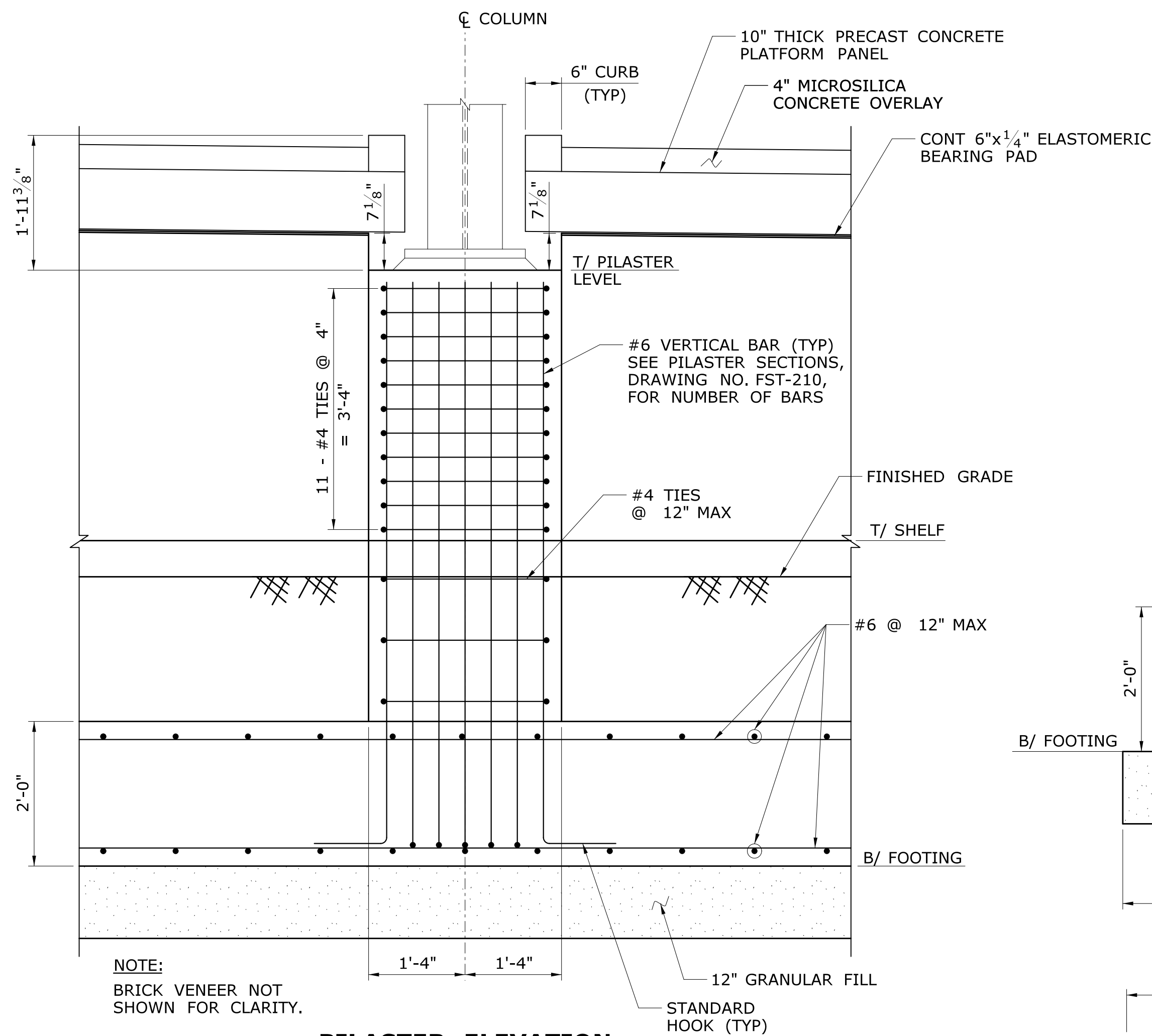
NOTES:

- REFER TO "FOUNDATION" PLANS FOR OVERALL FOUNDATION LAYOUT, HORIZONTAL LIMITS OF SHEET PILING, BOTTOM OF FOOTING ELEVATIONS, TOP OF SHELF ELEVATIONS, AND BRICK VENEER LOCATIONS.
- ALL REINFORCEMENT SHALL HAVE A MINIMUM 2" CLEAR COVER UNLESS NOTED OTHERWISE.
- SEE "GRADING AND DRAINAGE" PLANS FOR FINAL GRADE AT PLATFORM LOCATION.
- REFER TO ARCHITECTURAL DRAWINGS FOR APPROXIMATE TOP OF RAIL ELEVATIONS ALONG PLATFORM.
- SEE TABLE ON DRAWING FST-211 FOR PT. 1 THRU PT. 3 ELEVATIONS.
- 4" BRICK VENEER IS SHOWN. IF BRICK VENEER IS NOT REQUIRED, OMIT SHELF AND RE-ARRANGE THE VERTICAL REINFORCEMENT AS REQUIRED, PROVIDE A MINIMUM 2" CLEAR COVER.
- OMIT THESE BARS AT LOCATION WHERE BRICK VENEER IS NOT REQUIRED.
- 4" BRICK VENEER IS SHOWN. AT LOCATION WHERE BRICK VENEER IS NOT REQUIRED, REPLACE BRICK VENEER WITH A 4" THICK CURTAIN WALL.
- THESE DIMENSIONS ARE SHOWN AT TYPICAL COLUMN SECTION. DIMENSIONS MIGHT VARY AT LOCATIONS ADJACENT TO STAIR AND ELEVATOR TOWER, SIDEWALK STAIRS, SIDEWALK RAMPS AND SNOW MELT MECHANICAL ROOM. REFER TO "FOUNDATION" PLANS FOR OVERALL DIMENSIONS.
- THE CONTRACTOR SHALL PROVIDE TEMPORARY SUPPORT FOR THE PRECAST BEAMS UNTIL THE SPLICE COUPLERS ARE GROUTED AND HAVE REACHED A MINIMUM STRENGTH AS RECOMMENDED BY THE GROUT MANUFACTURER.

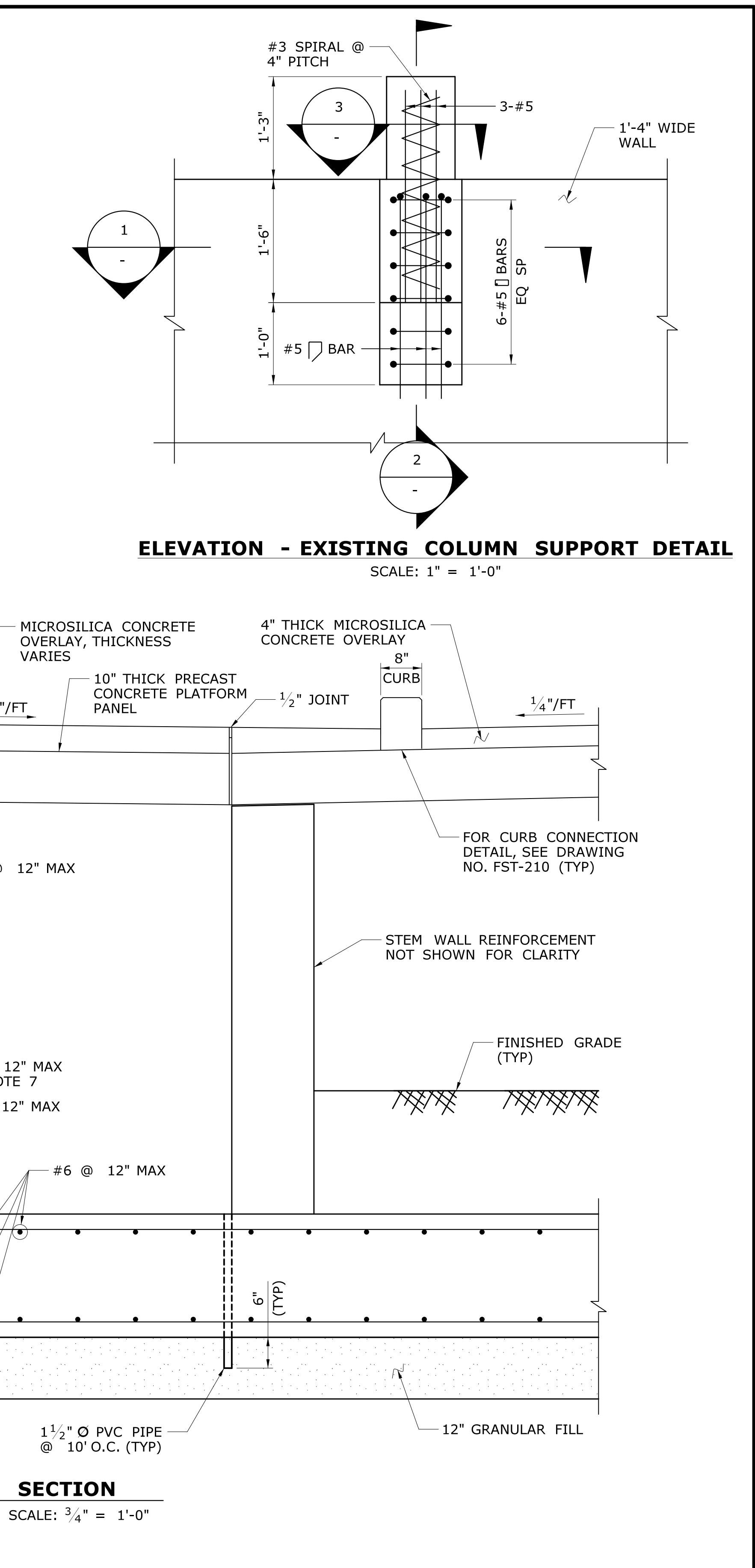
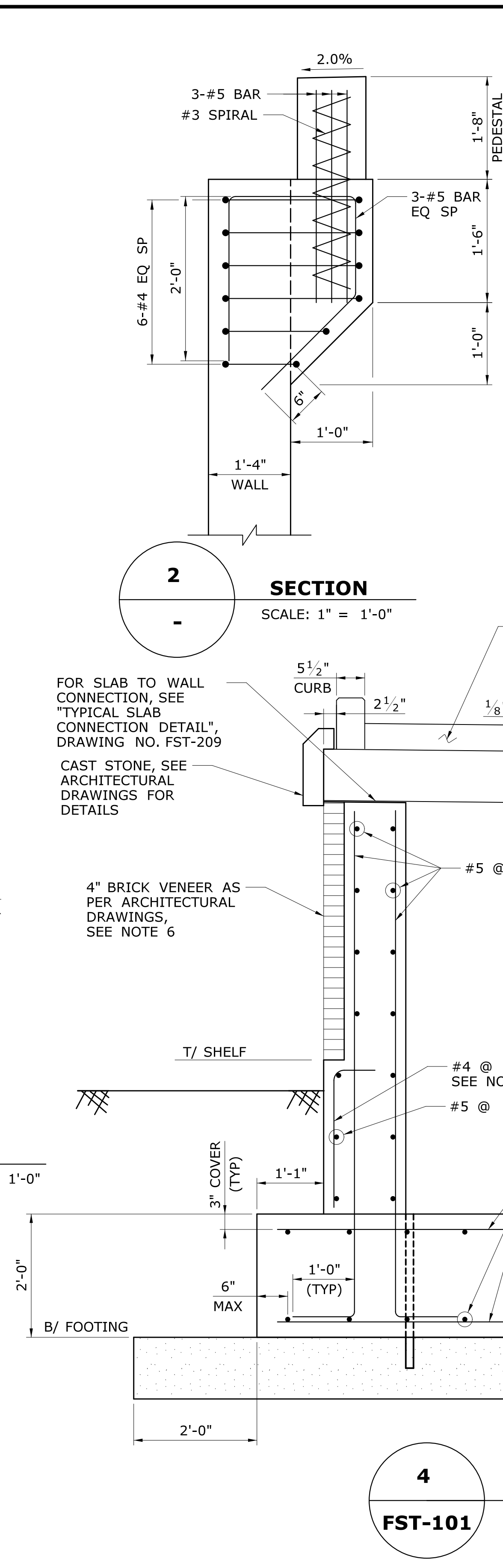
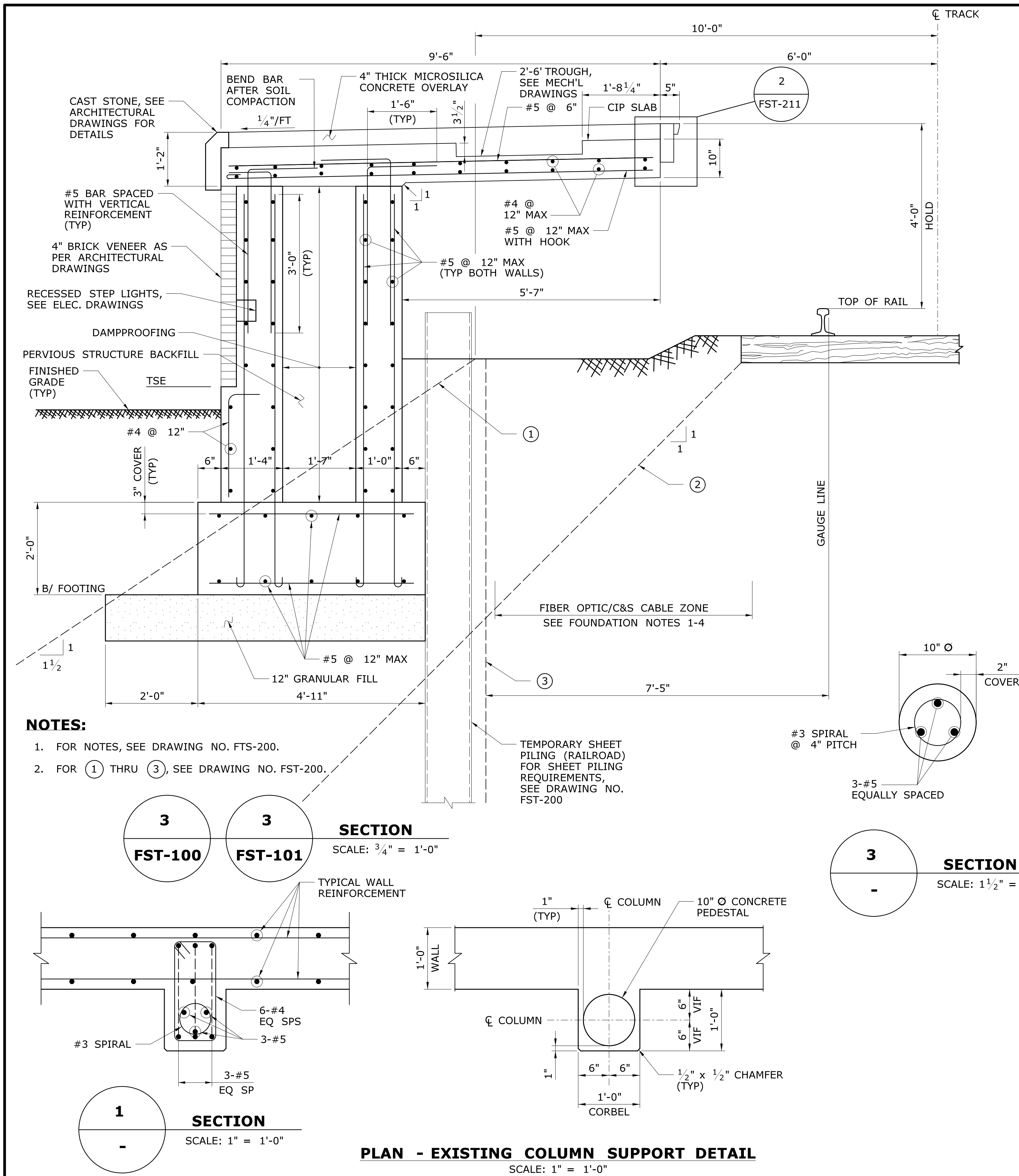
SHEET PILING REQUIREMENTS:

- "THEORETICAL RAILROAD EMBANKMENT LINE". SHEETING/SHORING REQUIRED INSIDE THIS AREA.
- "THEORETICAL UNDERGROUND TRACK DISTURBANCE LINE". EXCAVATIONS WHOSE BOTTOM EXTEND INTO THIS ZONE REQUIRE SHEETING TO BE LEFT IN PLACE AND CUT OFF PER AMTRAK REQUIREMENTS.
- AMTRAK "TOE OF BALLAST SLOPE LINE". NO SHEETING/SHORING WITHIN THIS AREA.
- SHEET PILING SHALL BE DESIGNED FOR ALL APPLICABLE LOADS INCLUDING THE LIVE LOAD EFFECTS OF THE COOPER E-80.

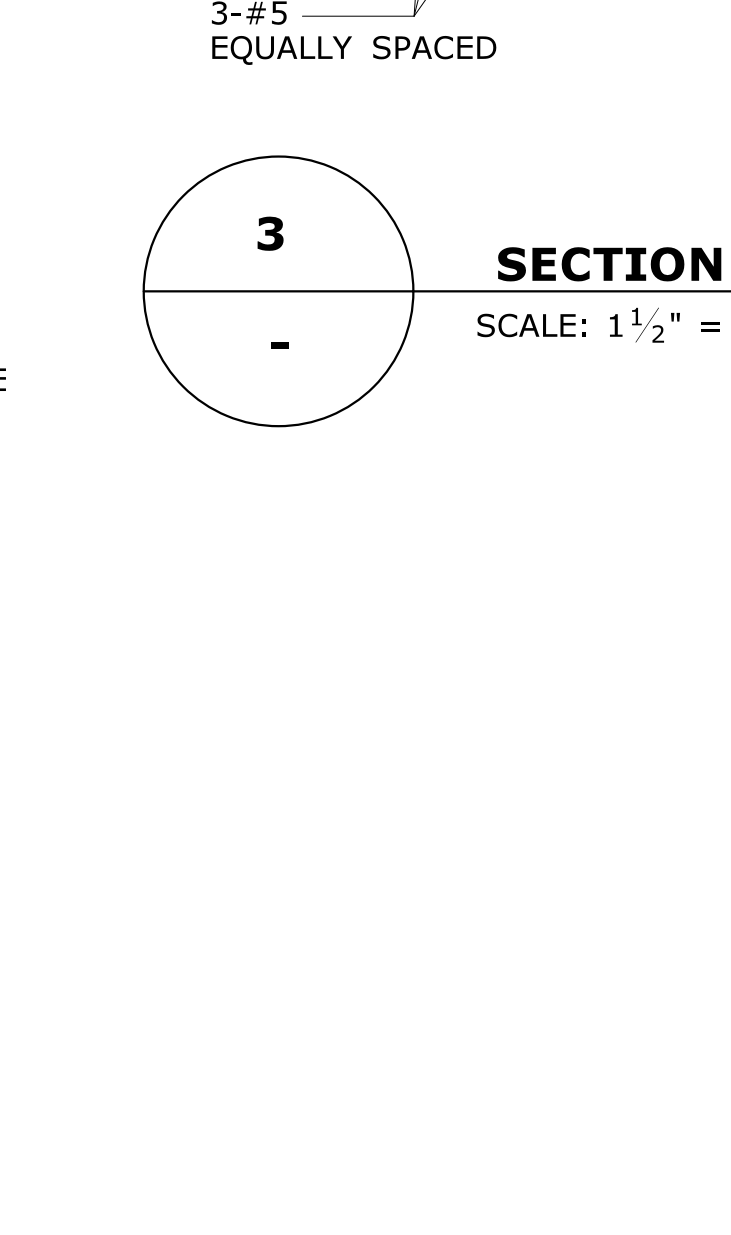
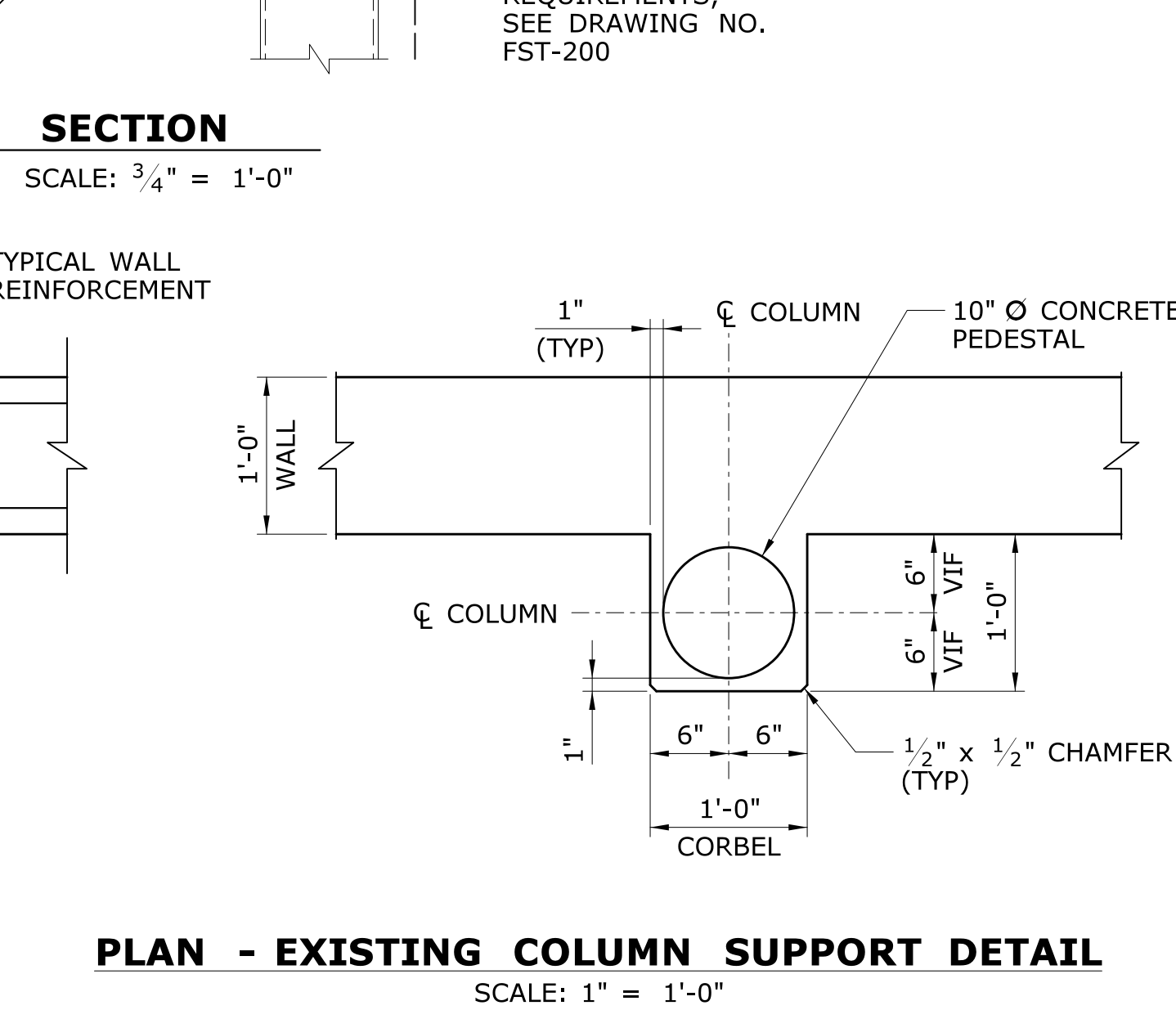
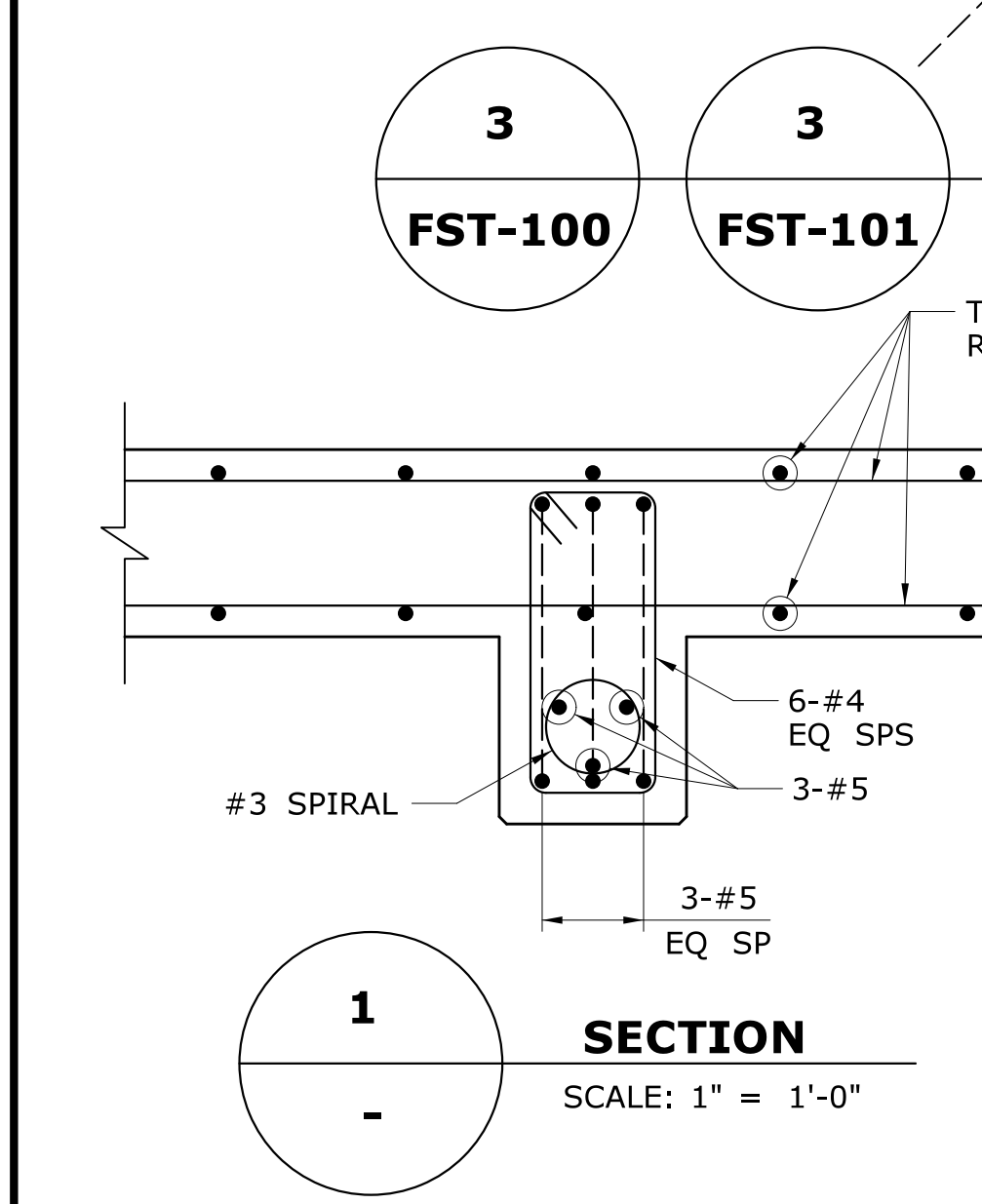
| | | | | | | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|---|--|--|--|--------------------------------|--|
| | | | DESIGNER/DRAFTER: C DONOHUE CHECKED BY: H BUI SCALE AS NOTED | | STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_200.dgn | | SIGNATURE/ BLOCK: TranSystems 530 PRESTON AVENUE MERIDEN, CT 06450 | | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | | TOWN: BERLIN | | PROJECT NO. 170-3155 | |
| | | | | | | | | | | | DRAWING TITLE: PLATFORM SECTIONS & DETAILS 1 | | DRAWING NO. FST-200 | |
| REV. DATE REVISION DESCRIPTION SHEET NO. | | | Plotted Date: 2/26/2014 | | | | | | | | | | SHEET NO. 01.12.021 | |

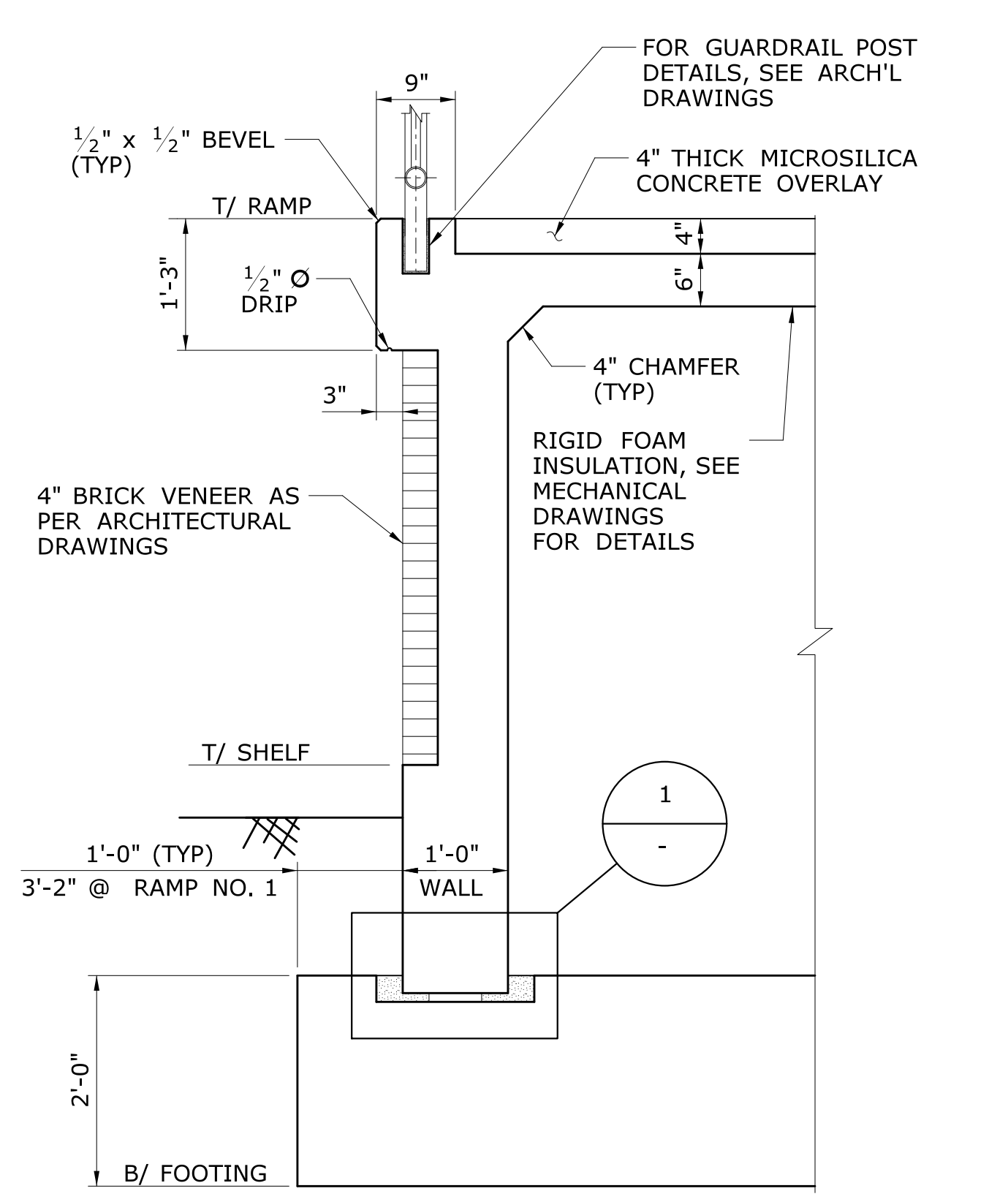
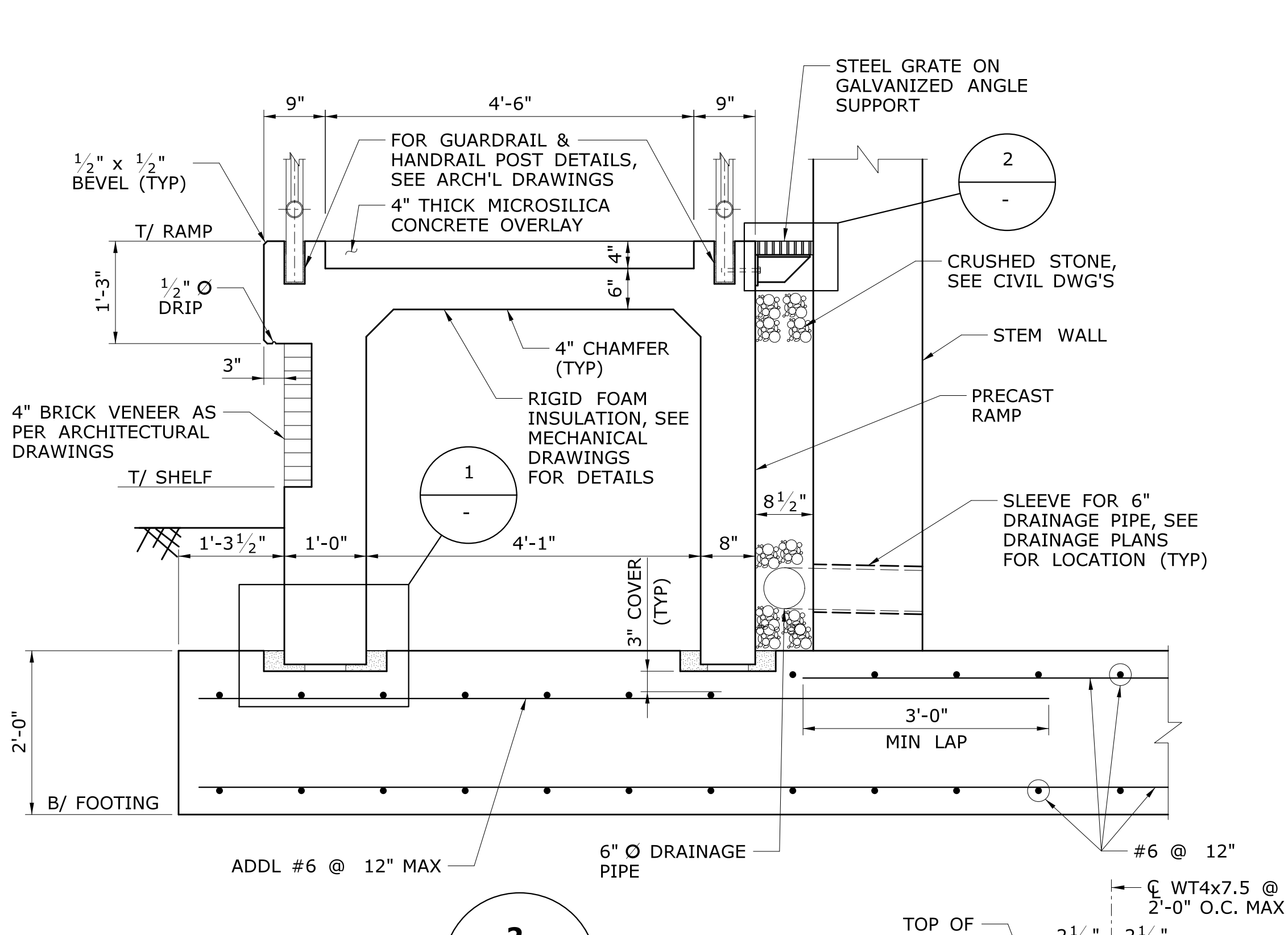
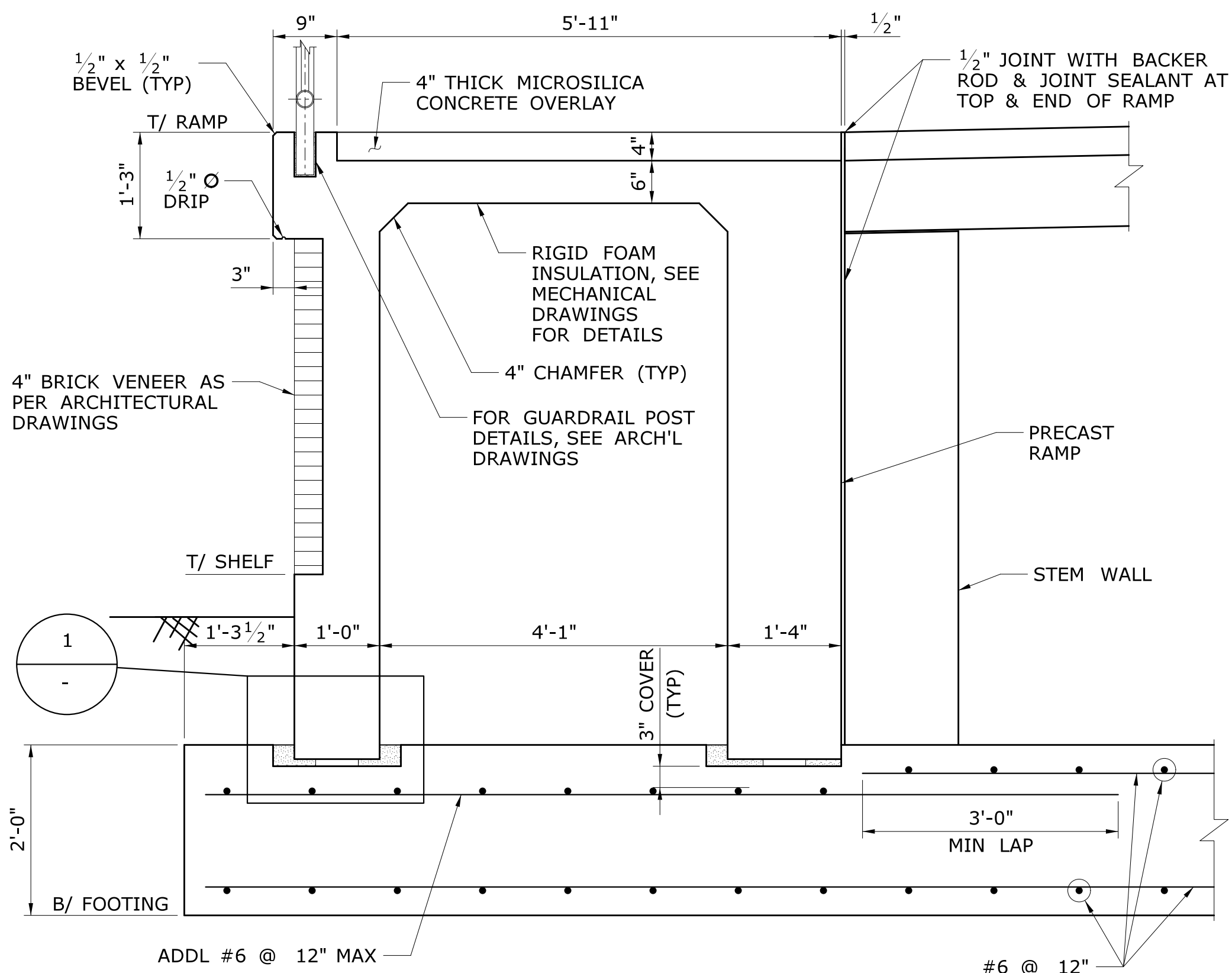


| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|

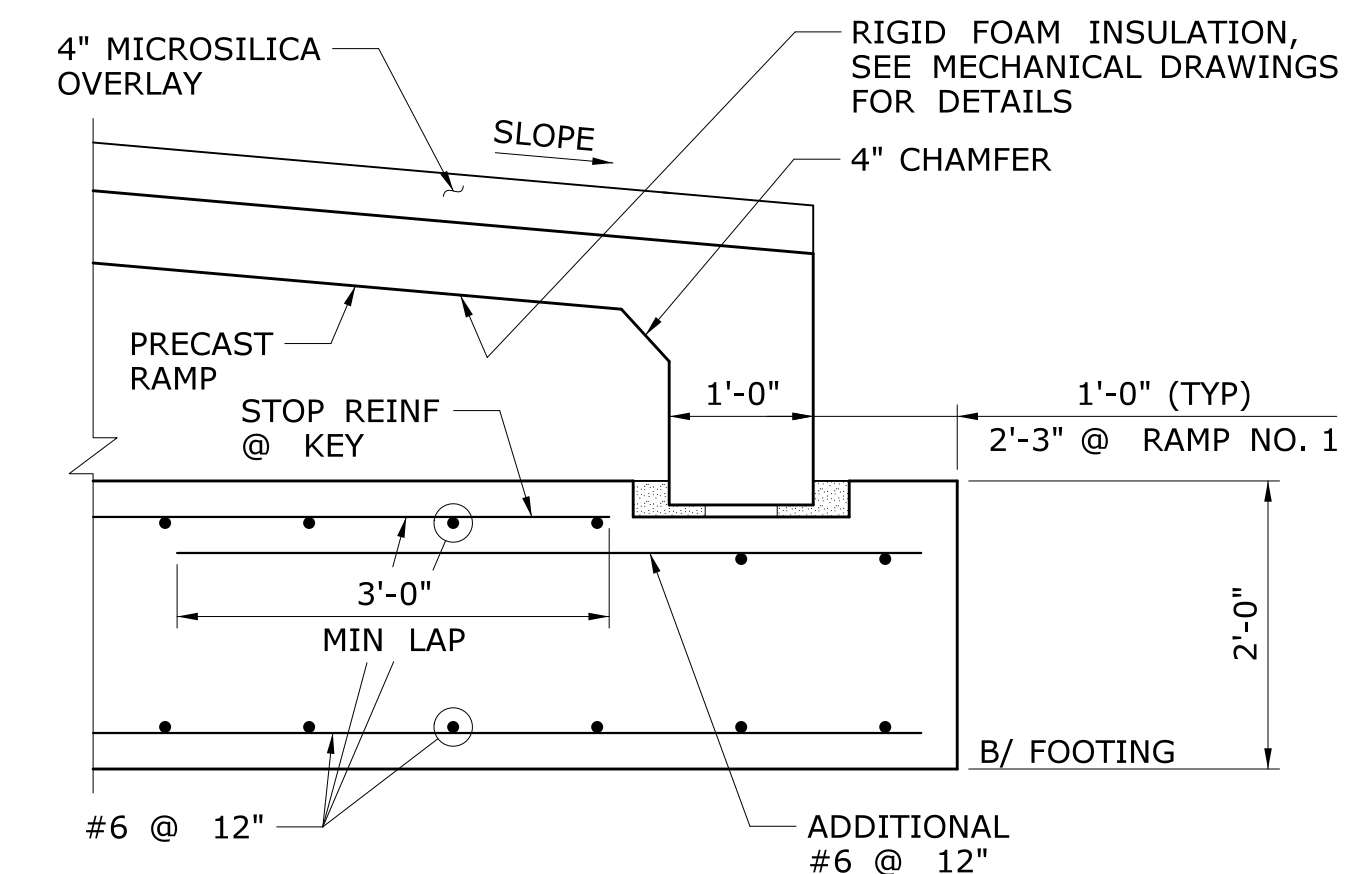
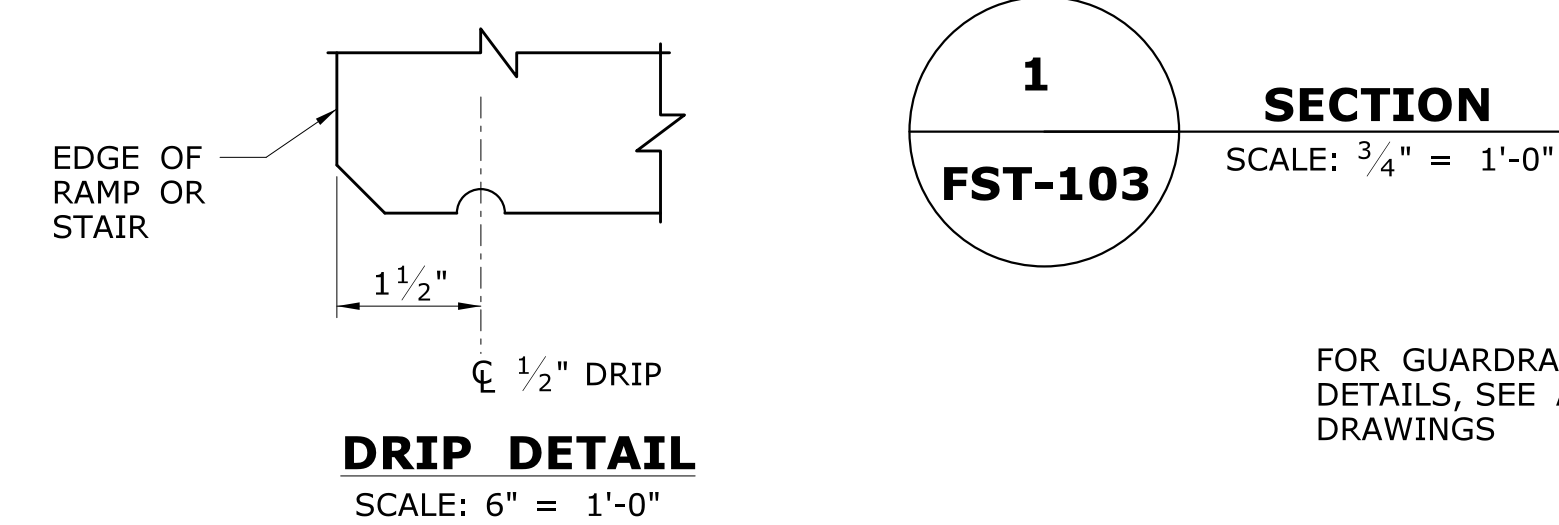


- NOTES:**
1. FOR NOTES, SEE DRAWING NO. FTS-200.
 2. FOR (1) THRU (3), SEE DRAWING NO. FST-200.





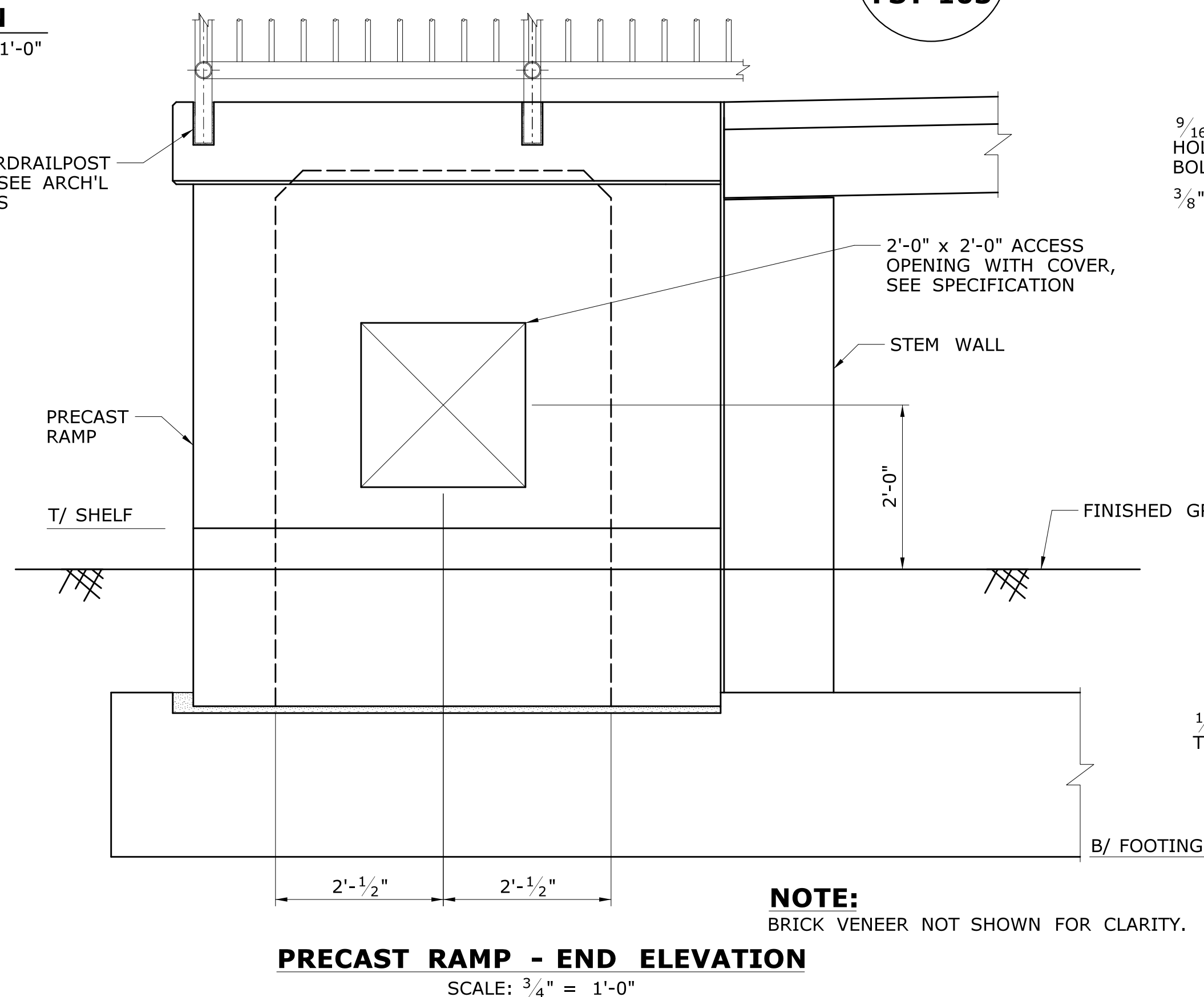
NOTE:
FOOTING REINFORCEMENT
NOT SHOWN FOR CLARITY.



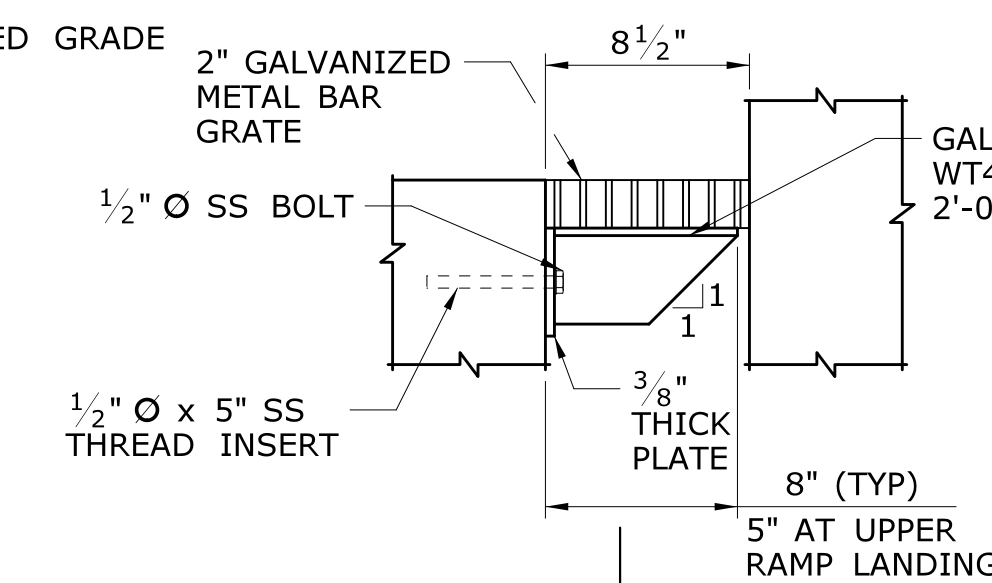
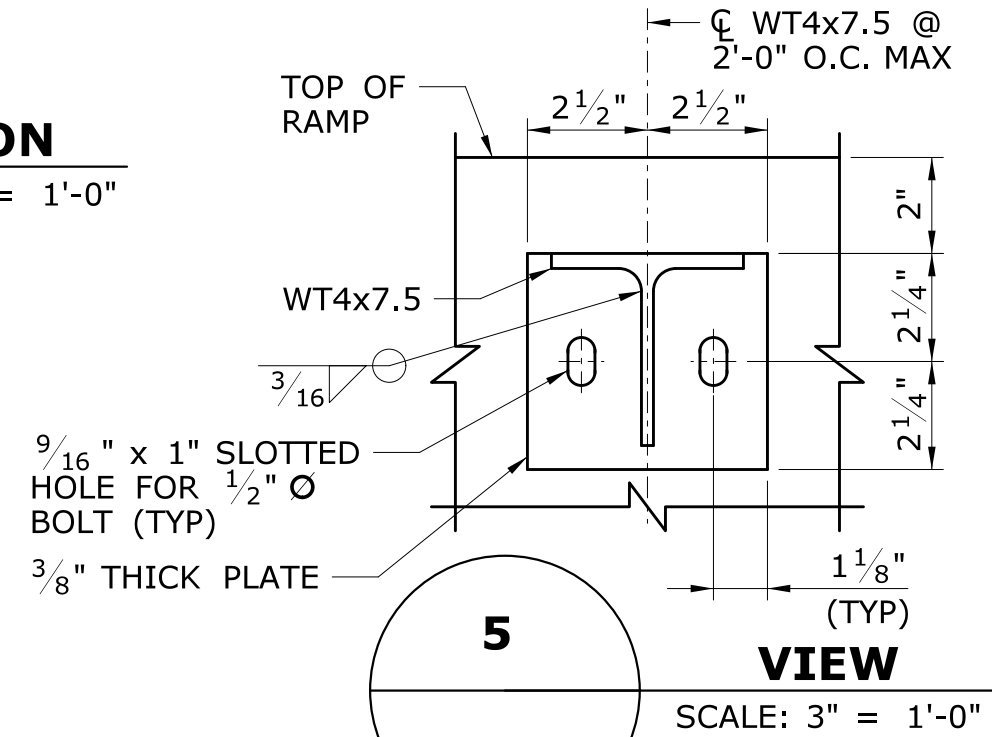
- NOTES:**
- SEE ARCHITECTURAL DRAWINGS FOR RAMP SLOPES.
 - FOR END OF RAMP AND SIDEWALK INTERFACE, SEE DETAIL ON DRAWING NO. FST-210.



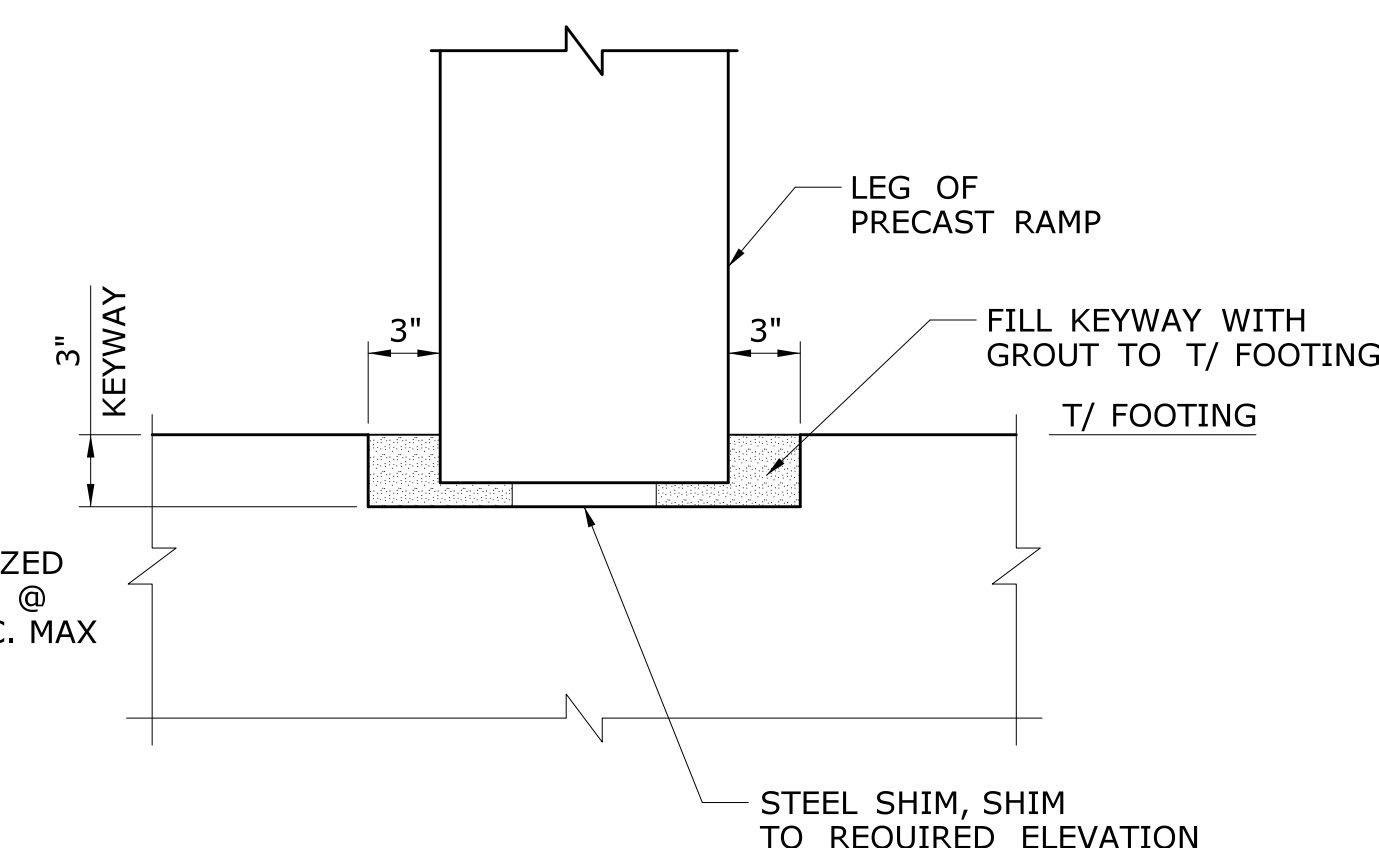
FOR GUARDRAILPOST
DETAILS, SEE ARCH'L
DRAWINGS



NOTE:
BRICK VENEER NOT SHOWN FOR CLARITY.



NOTE:
FORM KEYWAY AROUND CONCRETE PILASTER AS REQUIRED.



| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

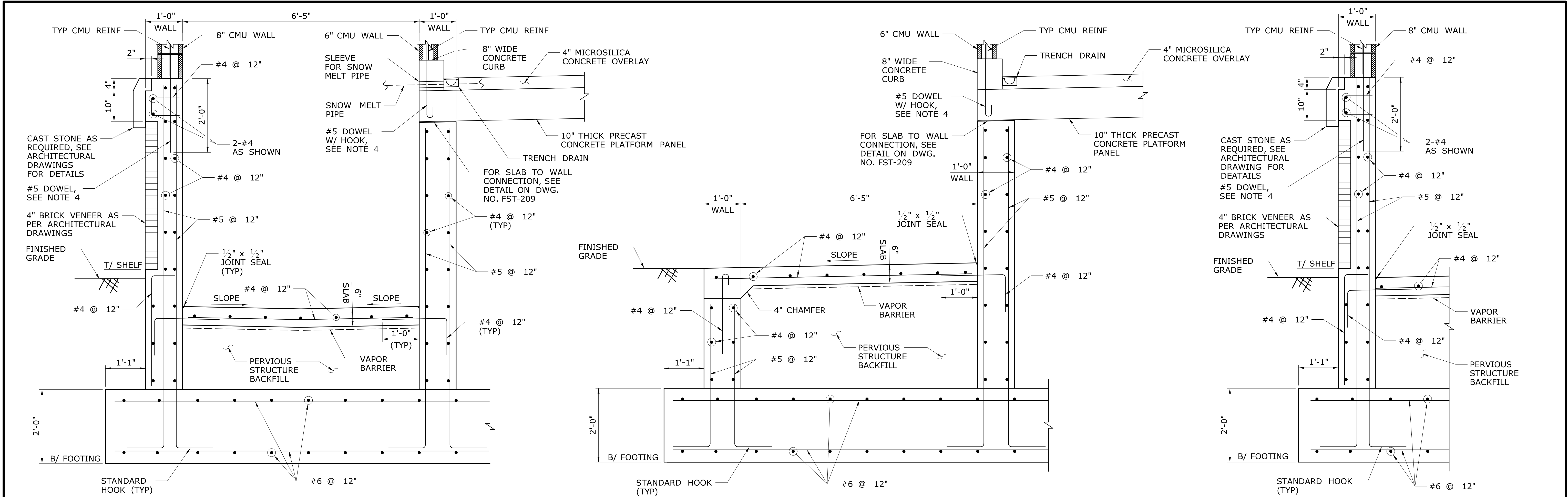
DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_203.dgn

SIGNATURE/
BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

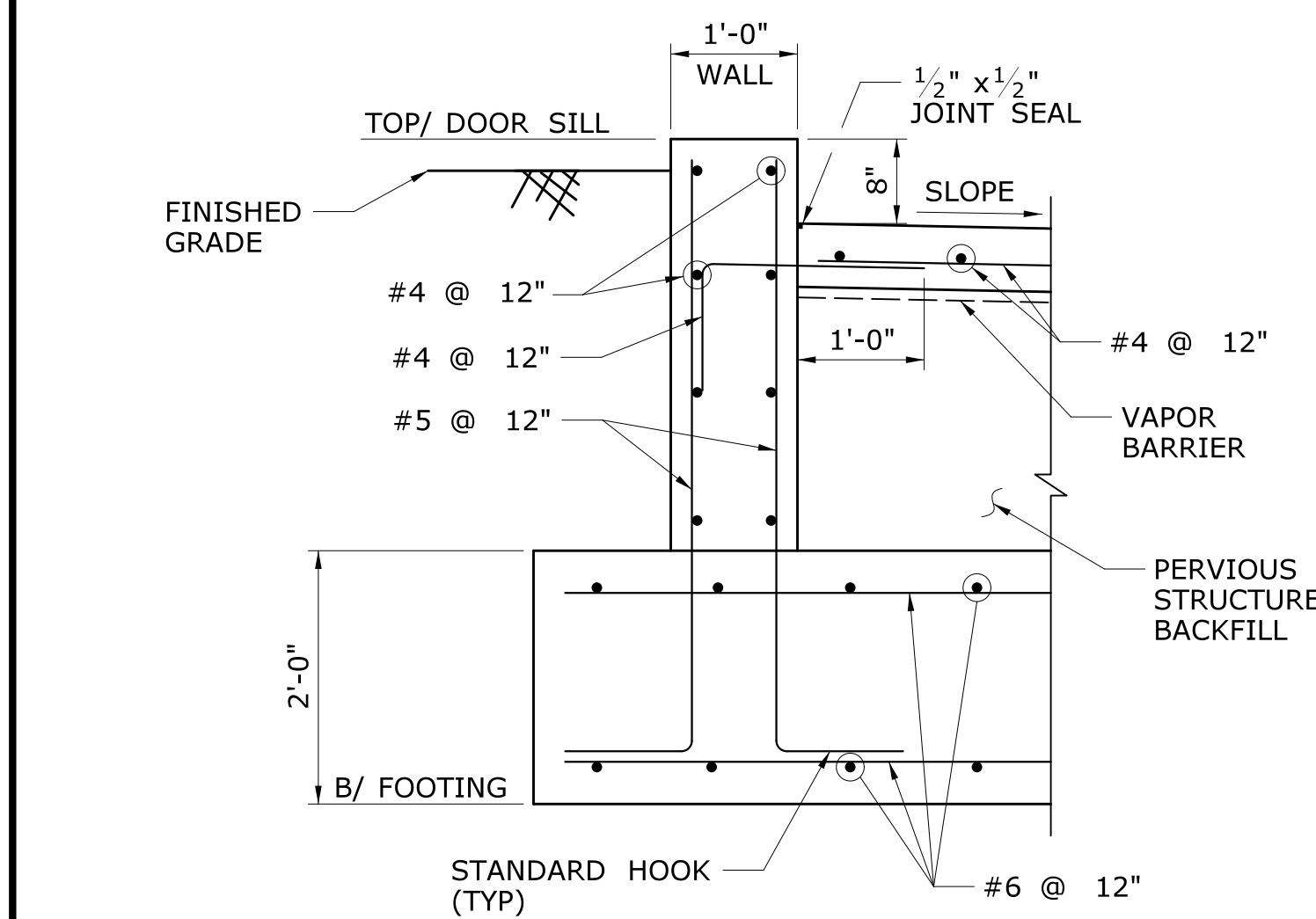
TOWN:
BERLIN
DRAWING TITLE:
**PLATFORM SECTIONS
& DETAILS 4**
PROJECT NO.
170-3155
DRAWING NO.
FST-203
SHEET NO.
01.12.024



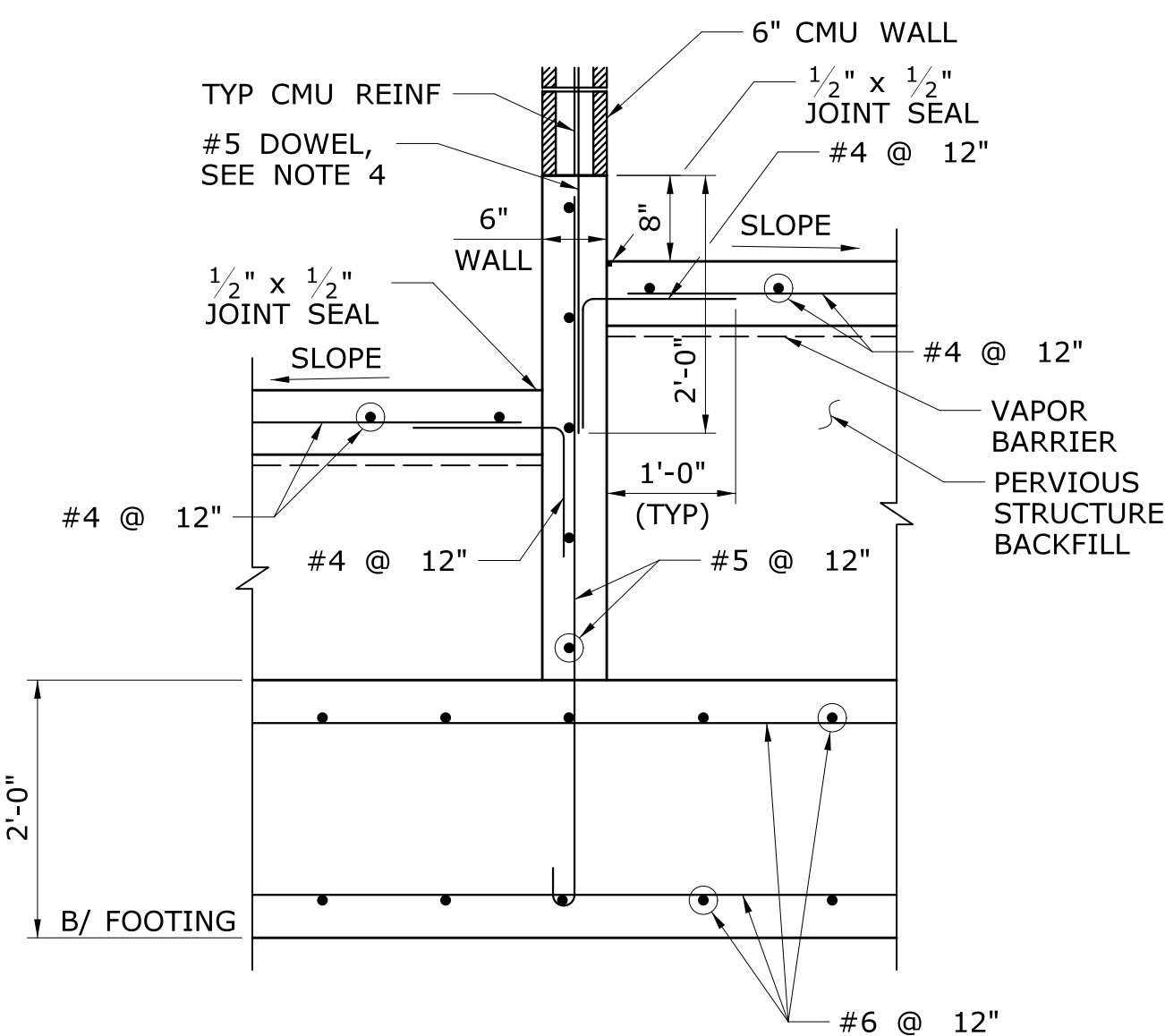
5
SECTION
FST-103 SCALE: $\frac{3}{4}" = 1'-0"$

6
SECTION
FST-103 SCALE: $\frac{3}{4}" = 1'-0"$

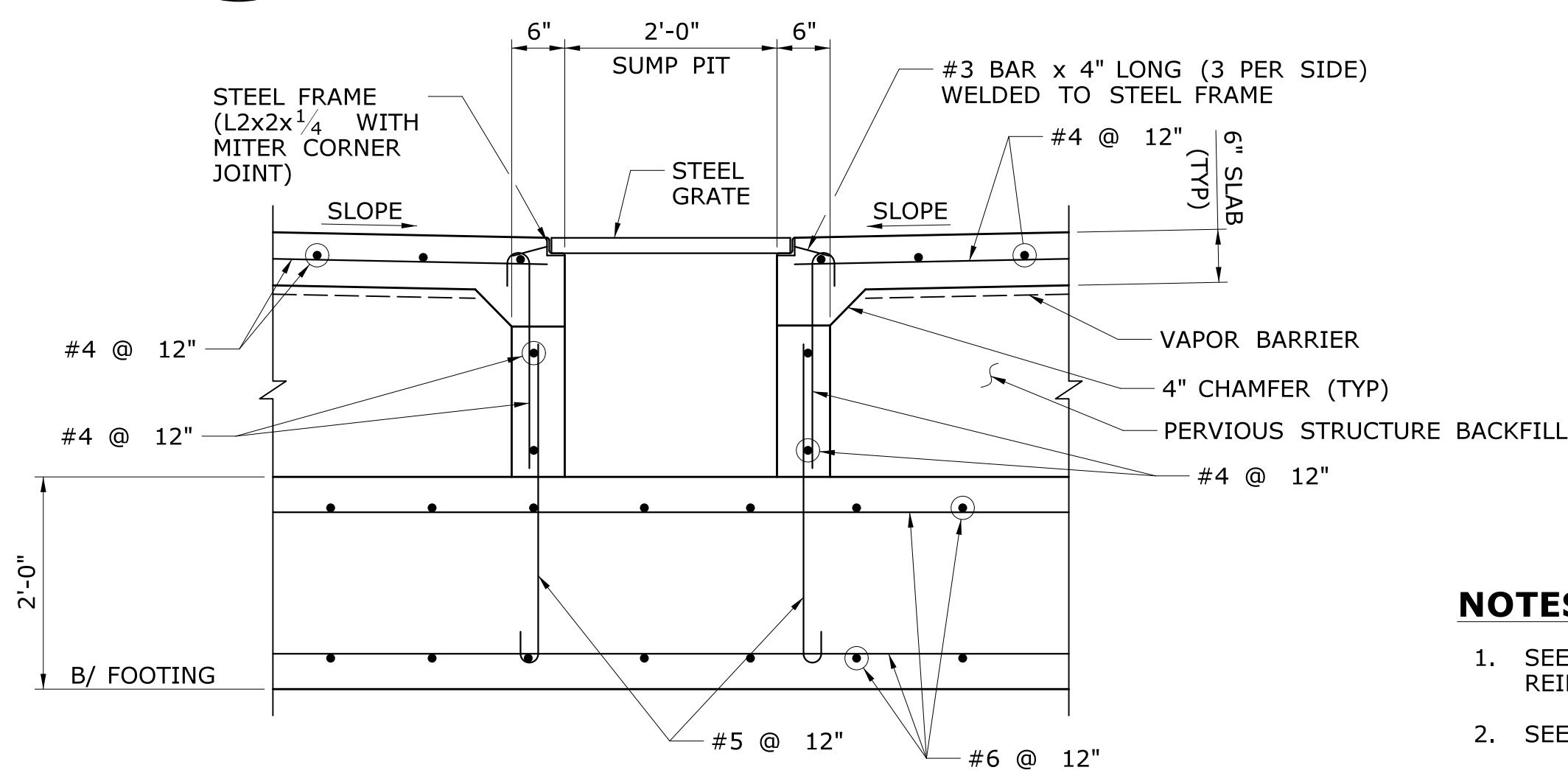
7
SECTION
FST-103 SCALE: $\frac{3}{4}" = 1'-0"$



8
SECTION
FST-103 SCALE: $\frac{3}{4}" = 1'-0"$



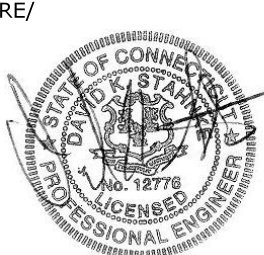
9
SECTION
FST-103 SCALE: $\frac{3}{4}" = 1'-0"$

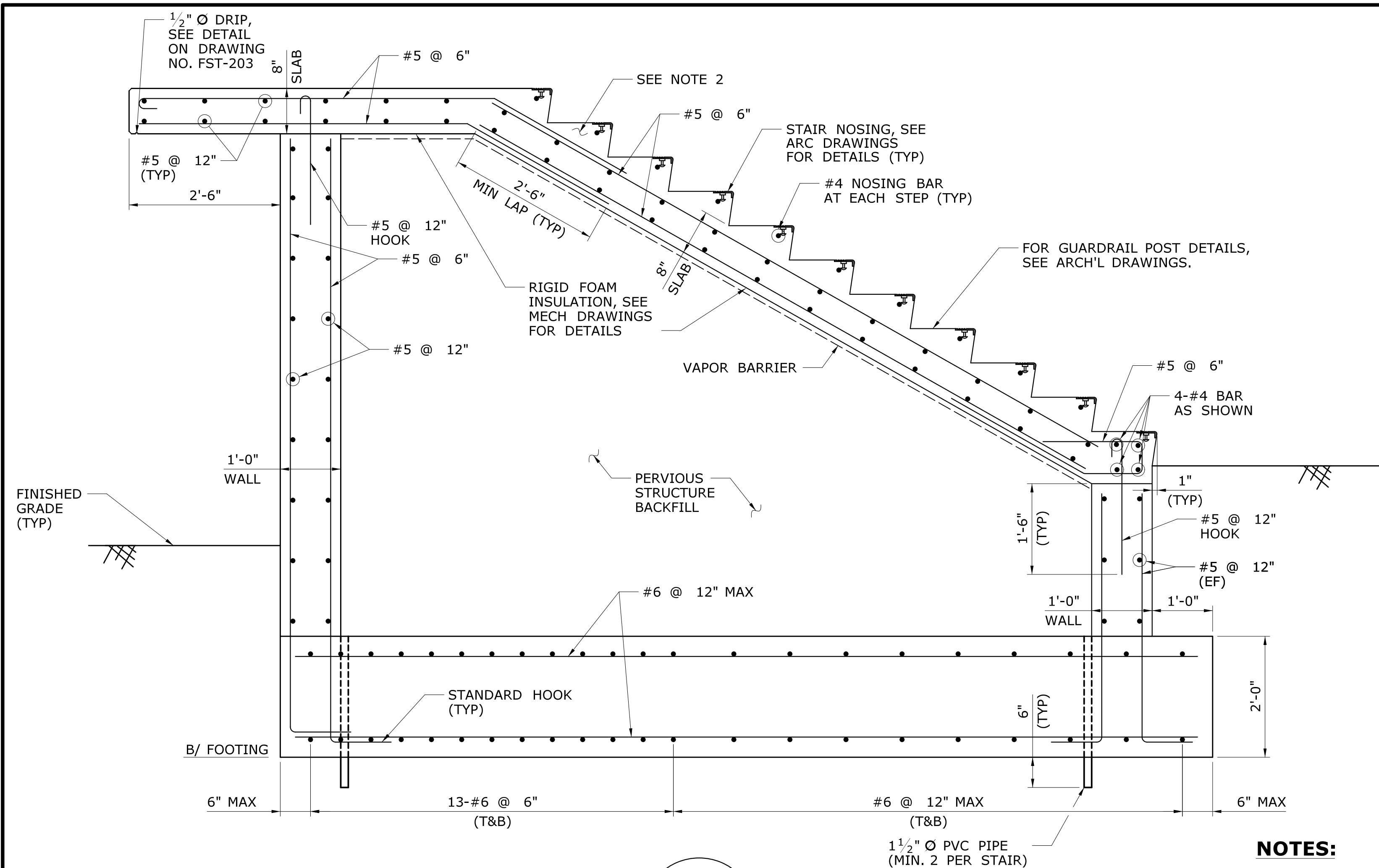


10
SECTION
FST-103 SCALE: $\frac{3}{4}" = 1'-0"$

NOTES:

1. SEE DRAWING NO. FST-200 FOR TYPICAL FOOTING REINFORCEMENT.
2. SEE ARCHITECTURAL DRAWINGS FOR RAMP SLOPES.
3. 12" GRANULAR FILL NOT SHOWN FOR CLARITY.
4. DOWEL BAR SPACING TO MATCH CMU REINFORCEMENT SPACING. PROVIDE MINIMUM 48" BAR DIAMETER LAP LENGTH.

| | | | | | | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|---|--|--|--|--------------------------------|--|
| | | | DESIGNER/DRAFTER: C DONOHUE CHECKED BY: H BUI | | STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION | | SIGNATURE/ BLOCK:  530 PRESTON AVENUE MERIDEN, CT 06450 | | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | | TOWN: BERLIN | | PROJECT NO. 170-3155 | |
| | | | SCALE AS NOTED | | Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_204.dgn | | | | | | DRAWING TITLE: PLATFORM SECTIONS & DETAILS 5 | | DRAWING NO. FST-204 | |
| REV. DATE REVISION DESCRIPTION SHEET NO. | | | Plotted Date: 2/26/2014 | | | | | | | | | | SHEET NO. 01.12.025 | |



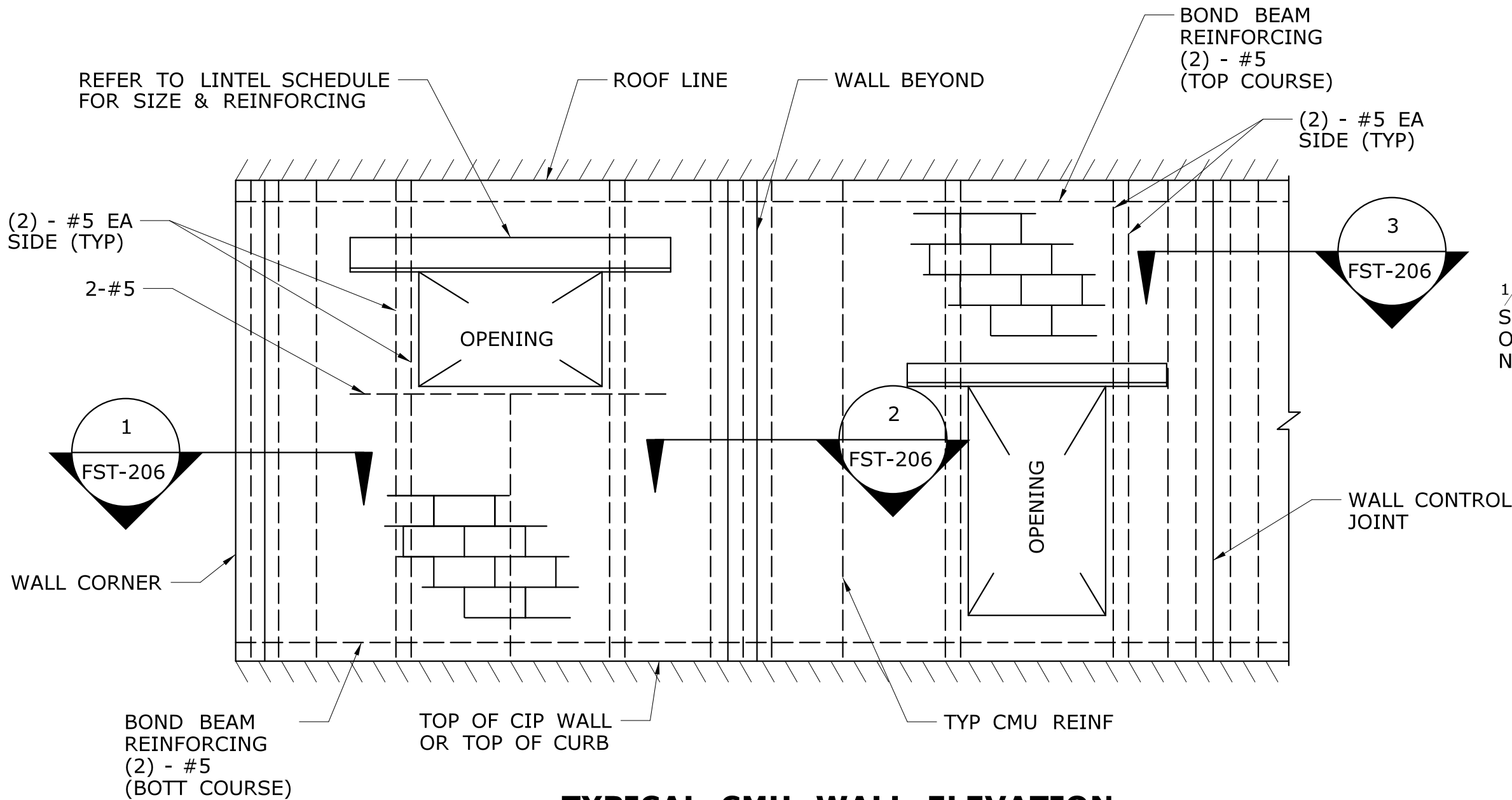
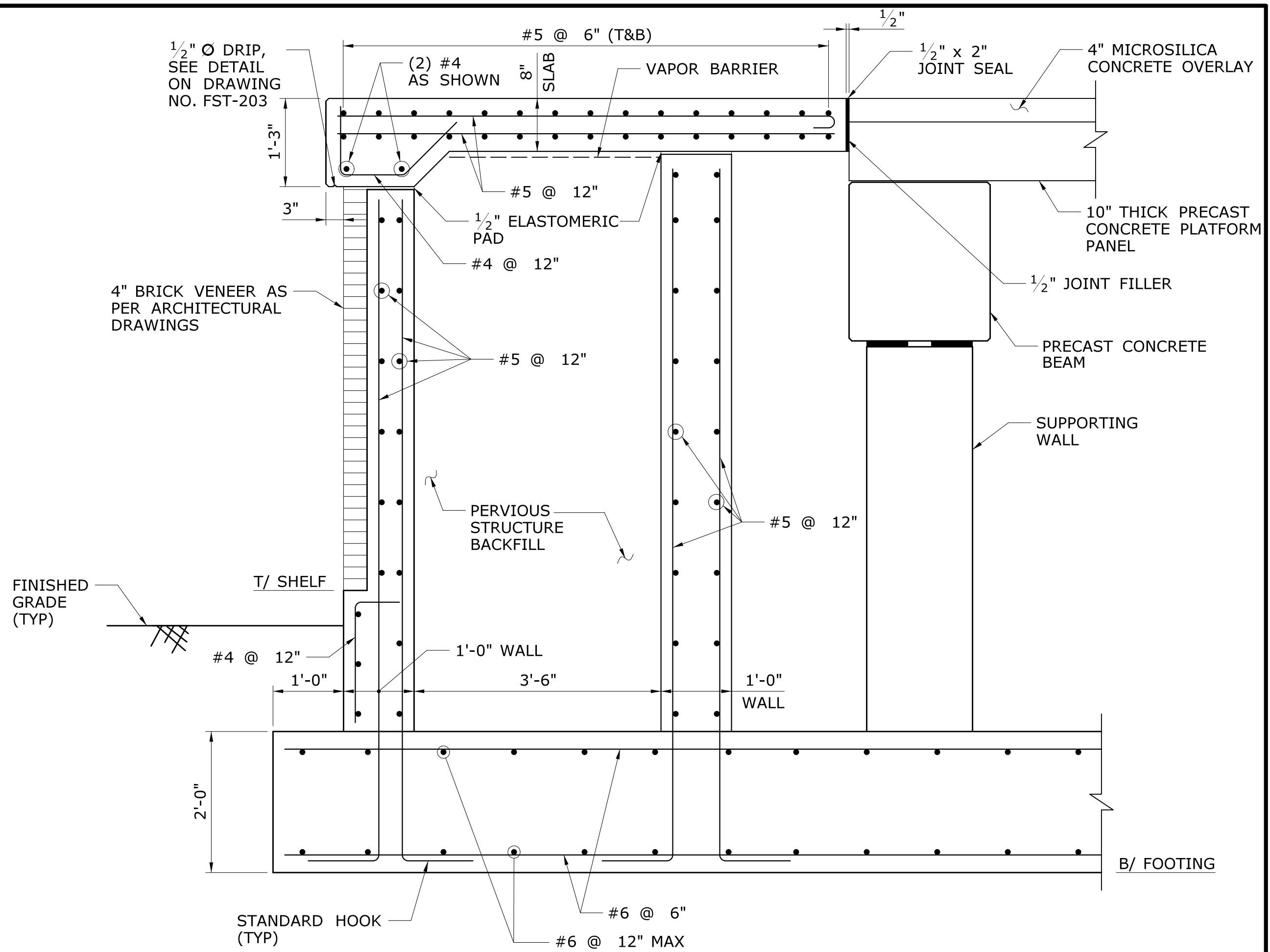
| WALL THK MO SIZE | 6" WALL | 8" WALL |
|---------------------|------------------|------------------|
| UP TO 4'-0" | WT4x10.5 | JL4x3 1/2 x 5/16 |
| 4'-0" TO 6'-0" | WT7x11 | JL5x3 1/2 x 5/16 |
| 6'-0" TO 8'-0" | JL6x3 1/2 x 5/16 | JL6x3 1/2 x 5/16 |

MISCELLANEOUS LOOSE LINTEL
SCHEDULE FOR MASONRY WALLS

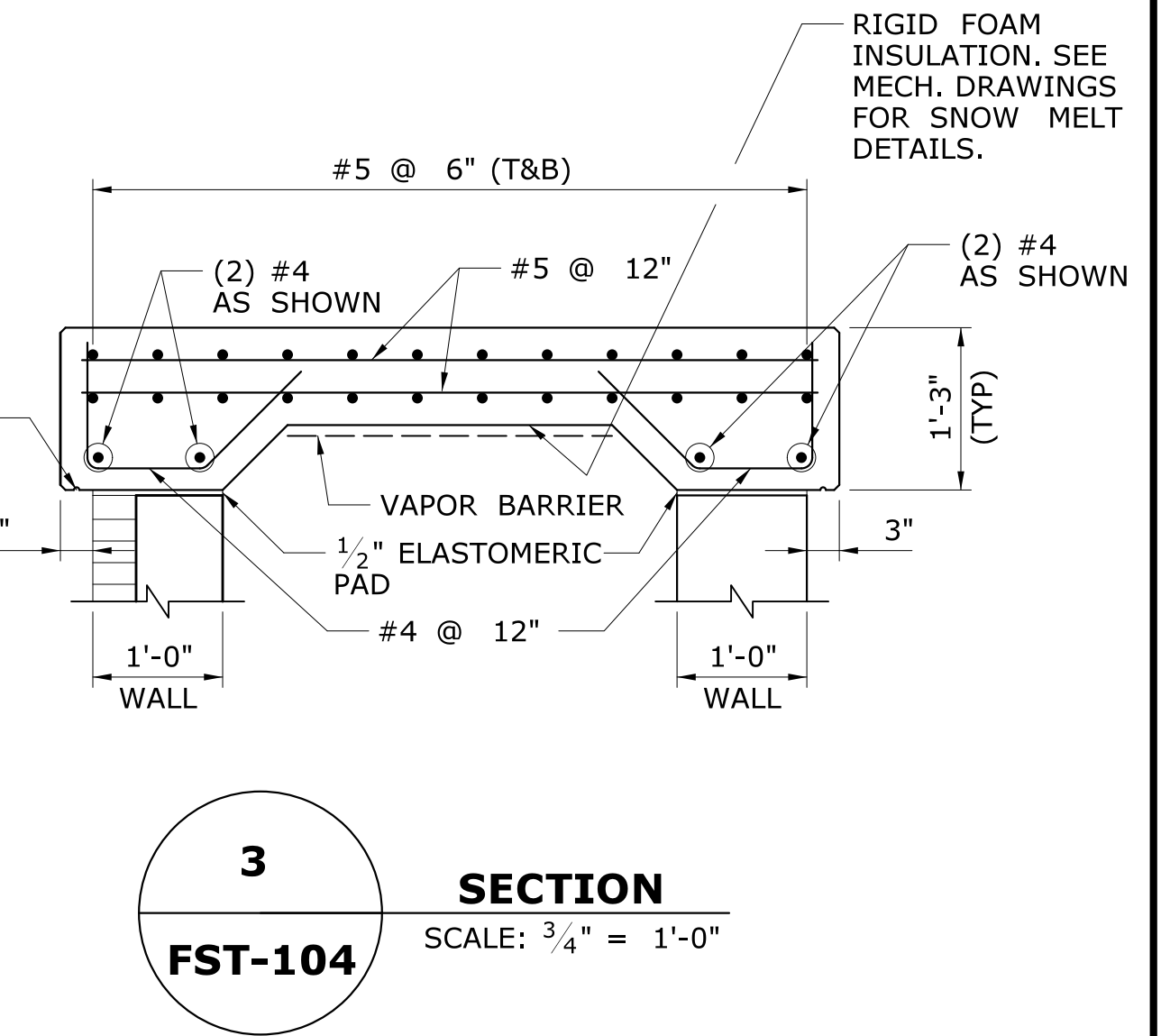
- NOTES:
- RE: ARCHITECTURAL, MECHANICAL, AND ELECTRICAL DRAWINGS FOR OPENING SIZE AND LOCATION.
 - CONNECT 2 ANGLES TO ALL 2 ANGLE LINTELS BACK TO BACK AT 24" OC MAX.
 - ALL ANGLES LONG LEG VERTICAL.
 - BEARING END OF STEEL LINTEL AT CONTROL JOINT SHALL BE WRAPPED WITH BUILDING PAPER.
 - PROVIDE MINIMUM 6" BEARING ON BRICK OR SOLID CONCRETE BLOCK.
 - PROVIDE MINIMUM 8" x WALL THICKNESS x 8" HIGH GROUTED CMU OR BRICK BEARING PAD UNDER ALL LINTELS, UNLESS OTHERWISE NOTED. GROUT JAMBS OF MASONRY OPENINGS 6'-0" AND LARGER FULL HEIGHT FOR 8" MINIMUM WIDTH.
 - CONNECT LINTEL TO COLUMN IF MASONRY OPENING IS ADJACENT TO COLUMN AND LINTEL HAS LESS THAN 8" BEARING ON CMU BEYOND COLUMN FLANGES.
 - PROVIDE STEEL LOOSE LINTELS WHERE MASONRY CONTROL JOINTS GO THRU WALL OPENINGS.

NOTES:

- 12" GRANULAR FILL NOT SHOWN FOR CLARITY.
- SEE MECHANICAL DRAWINGS FOR SNOW MELT LAYOUT AT THE STAIR.



TYPICAL CMU WALL ELEVATION
SCALE: N.T.S.

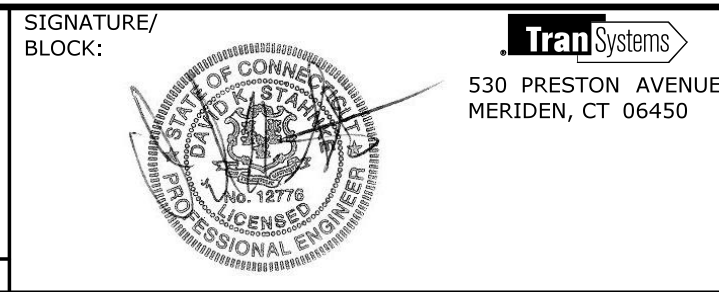
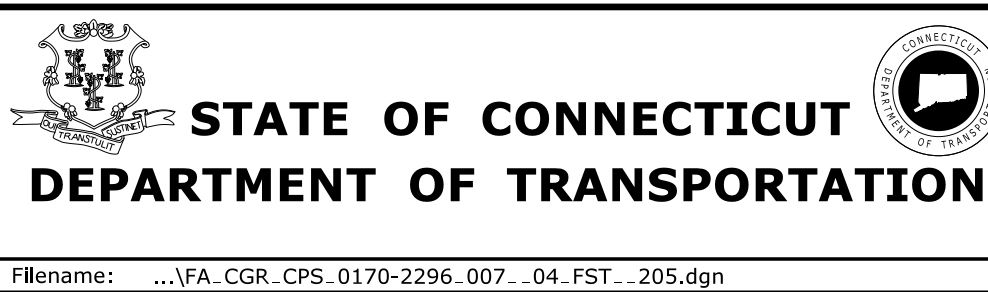


| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

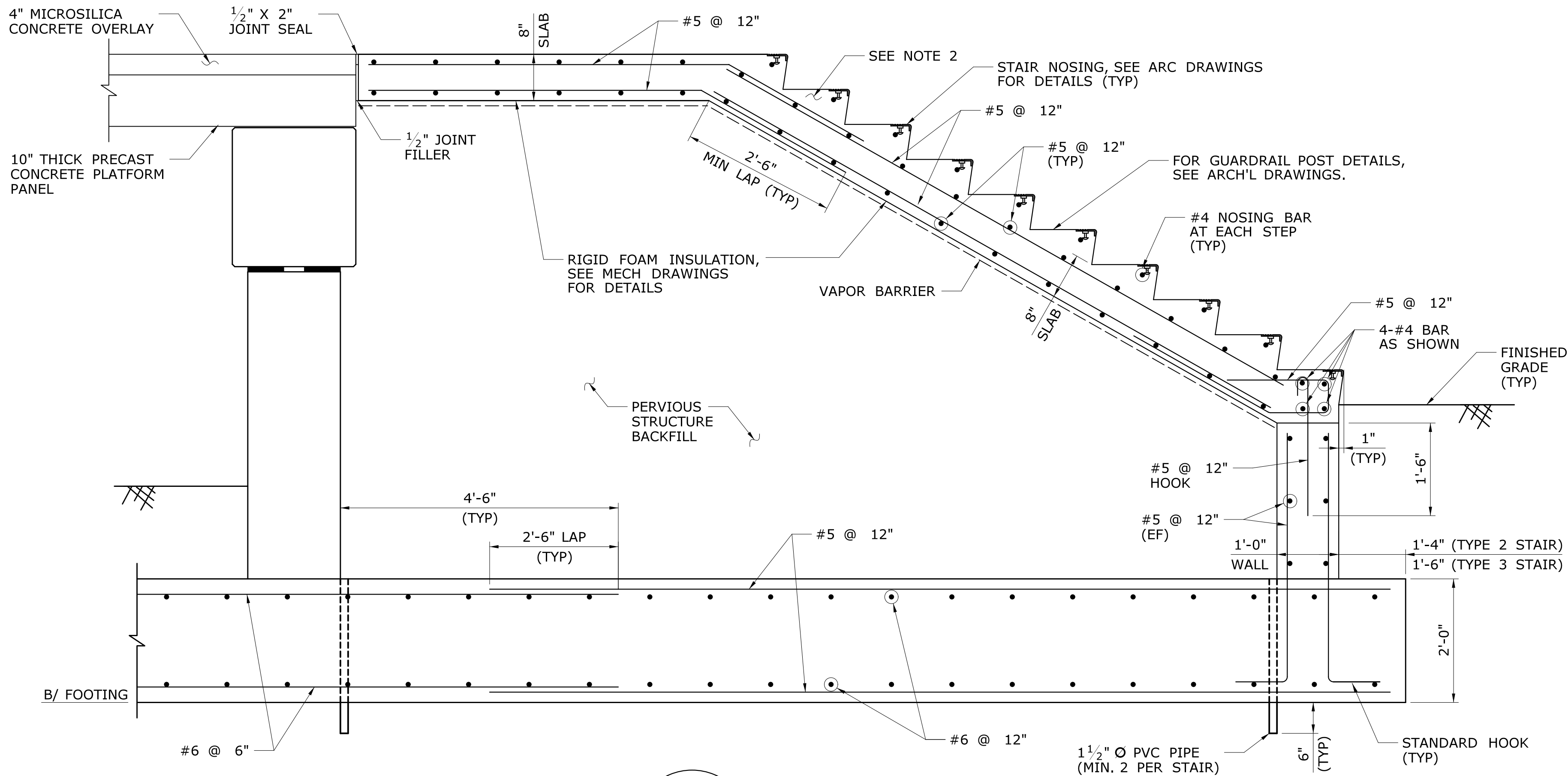
Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED



PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

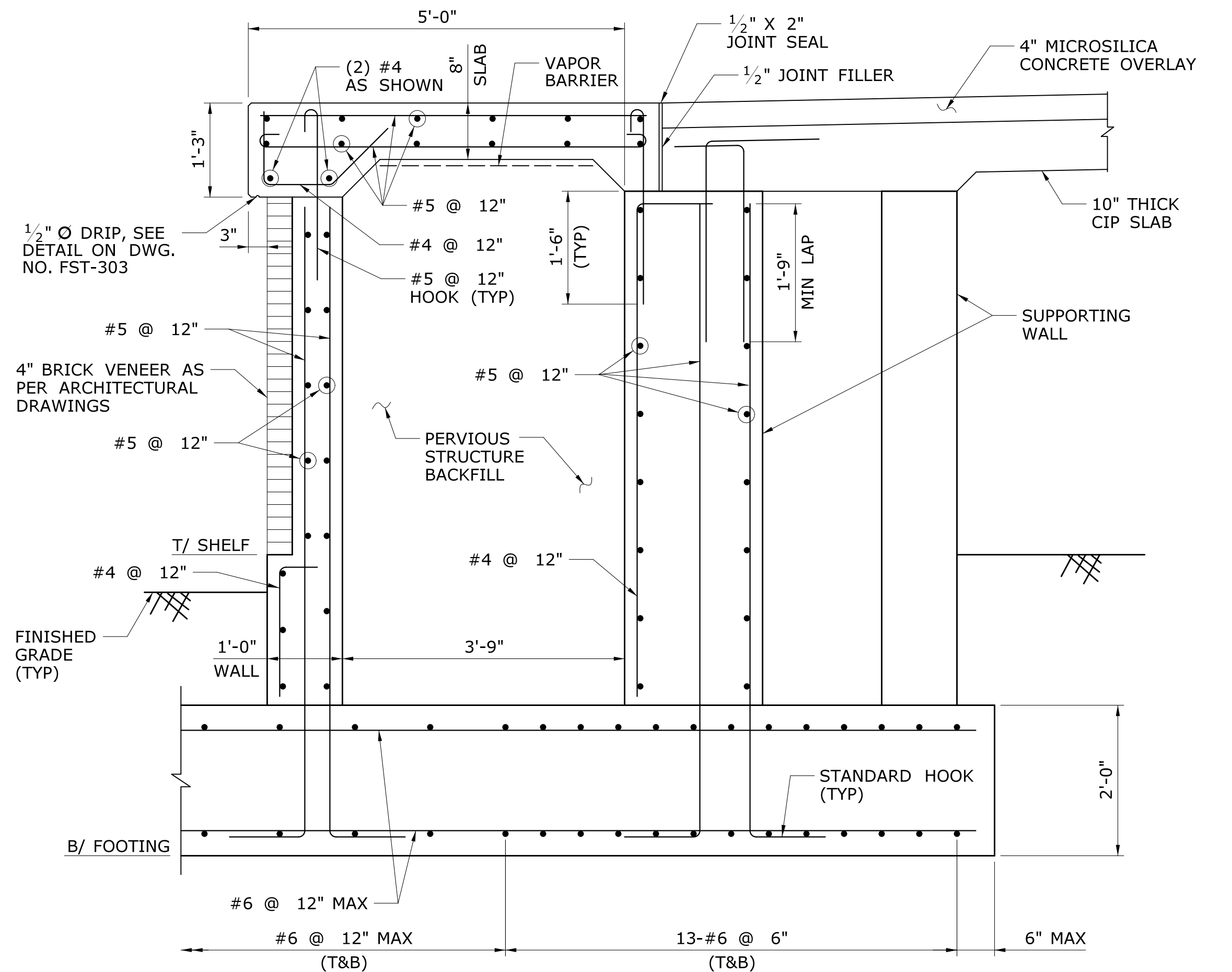
TOWN: **BERLIN**
DRAWING TITLE: **PLATFORM SECTIONS
& DETAILS 6**
PROJECT NO. **170-3155**
DRAWING NO. **FST-205**
SHEET NO. **01.12.026**



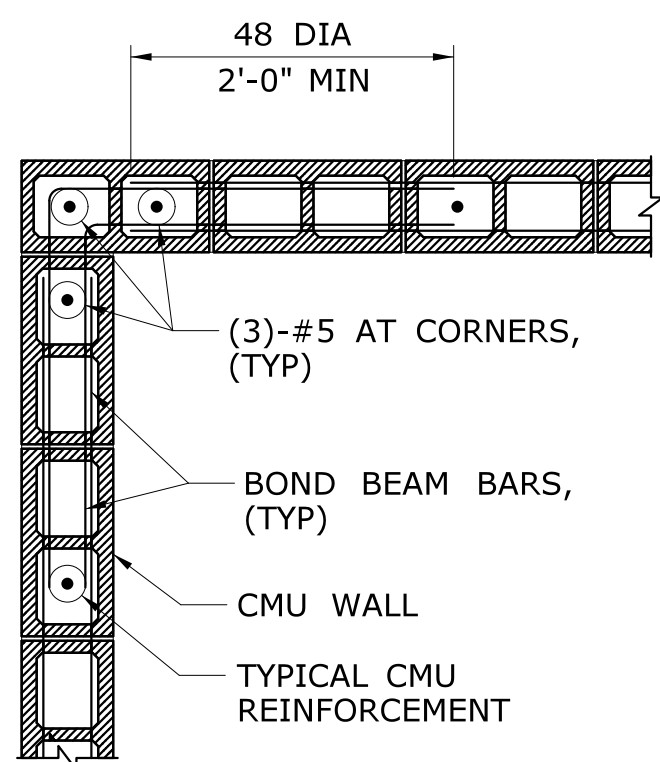
4
FST-104
SECTION
SCALE: $\frac{3}{4}" = 1'-0"$

NOTES:

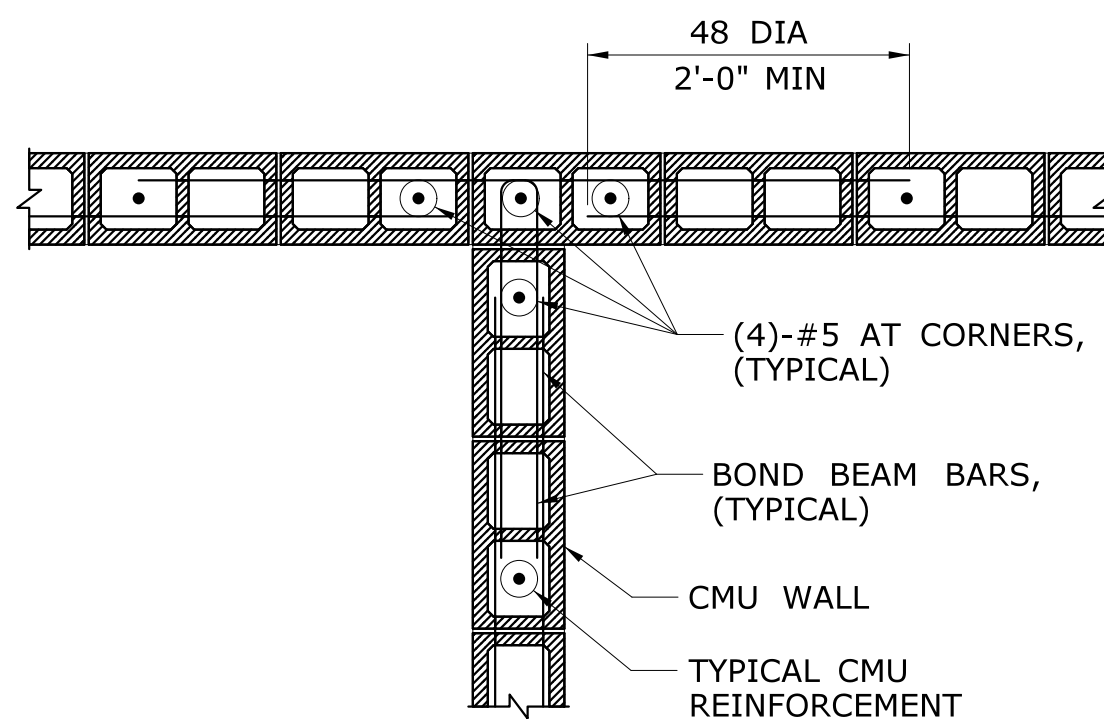
- 12" GRANULAR FILL NOT SHOWN FOR CLARITY.
- SEE MECHANICAL DRAWINGS FOR SNOW MELT LAYOUT AT THE STAIR.



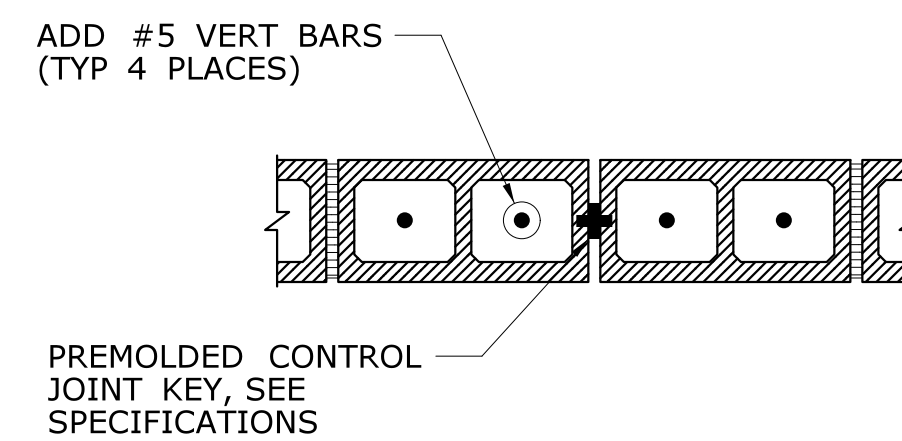
5
FST-104
SECTION
SCALE: $\frac{3}{4}" = 1'-0"$





1
FST-205
TYPICAL CMU CORNER REINFORCEMENT DETAIL
SCALE: N.T.S.

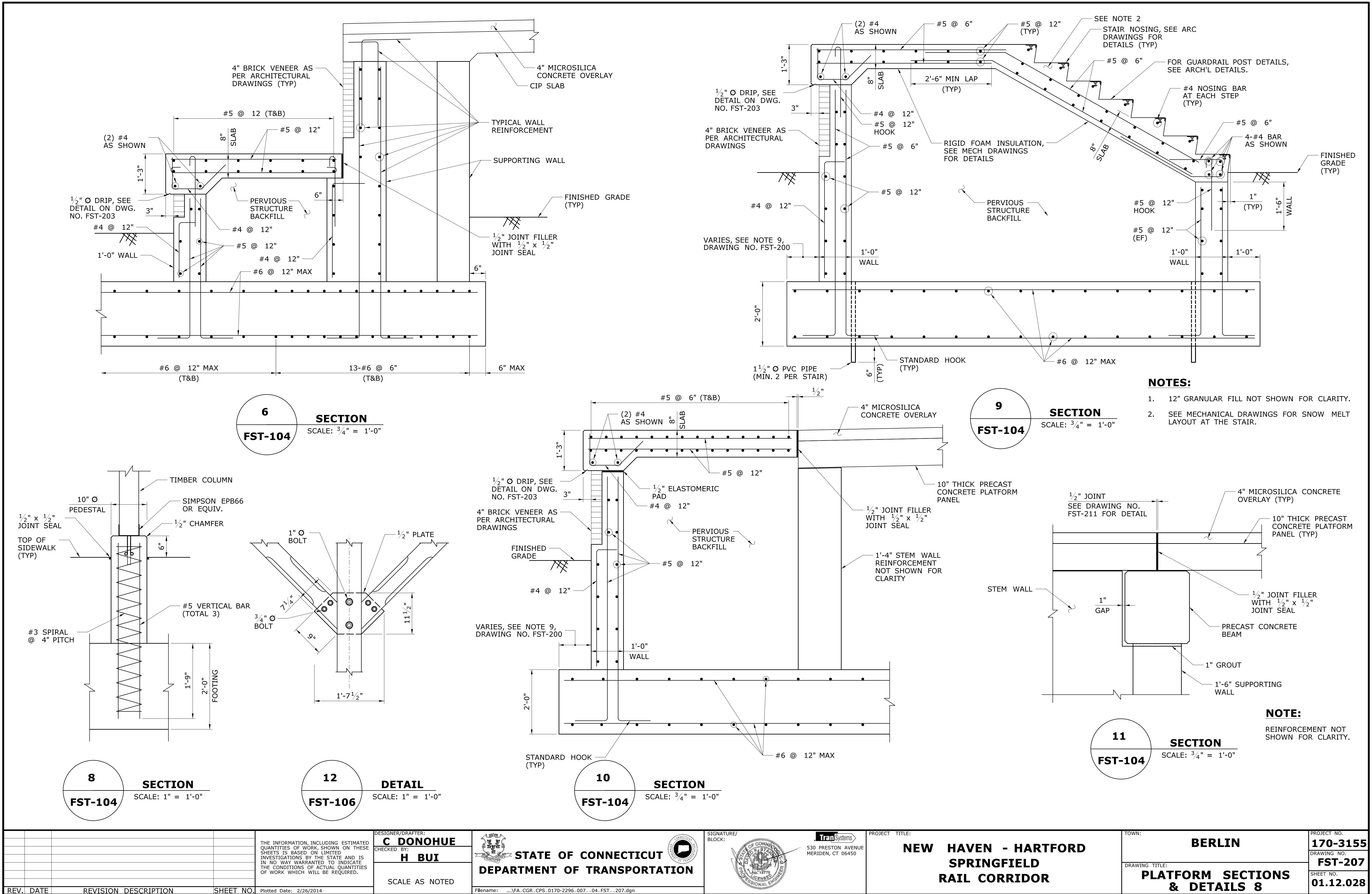


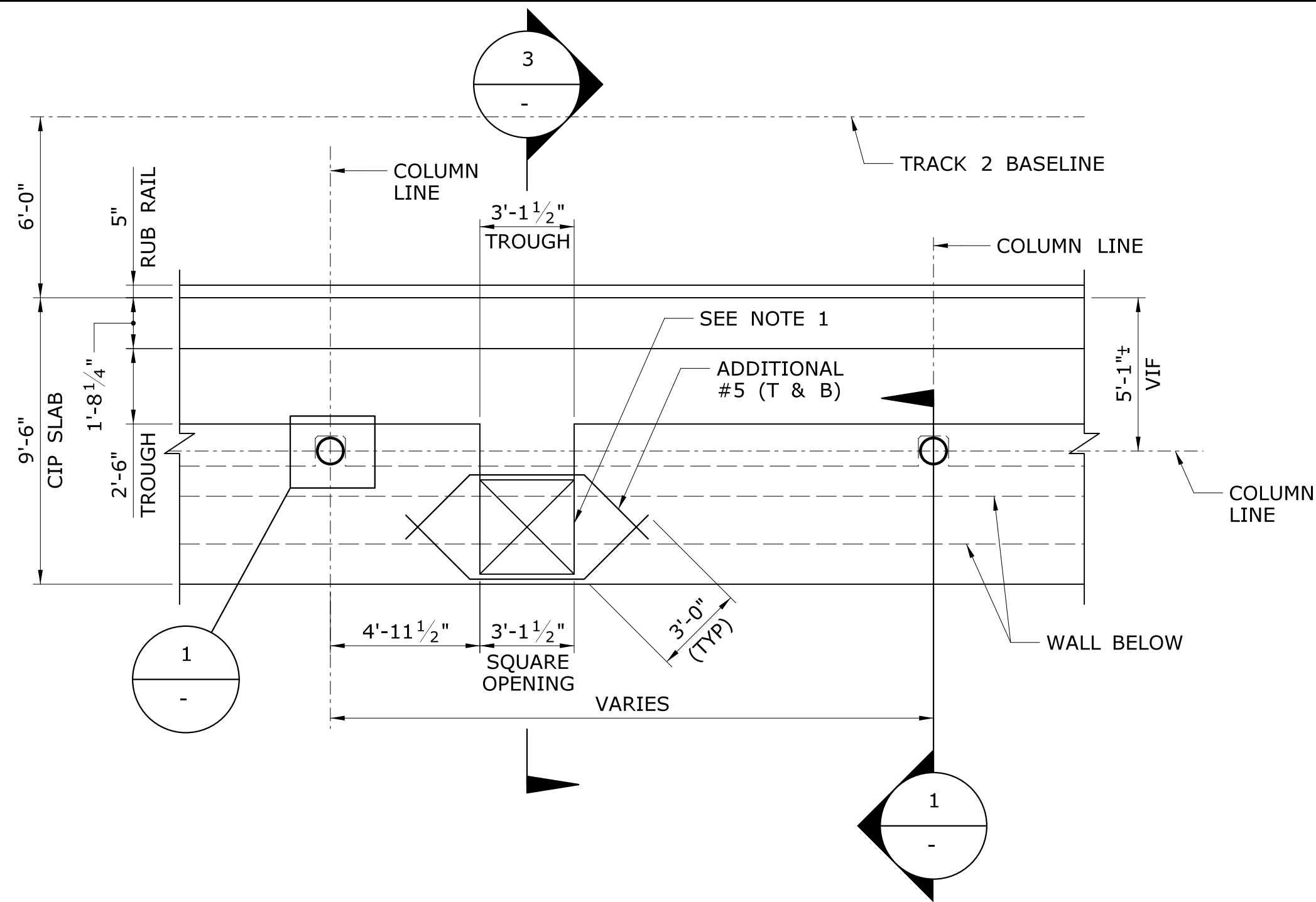
2
FST-205
TYPICAL CMU "T" REINFORCEMENT DETAIL
SCALE: N.T.S.



3
FST-205
TYPICAL CONTROL JOINT IN CMU
SCALE: N.T.S.

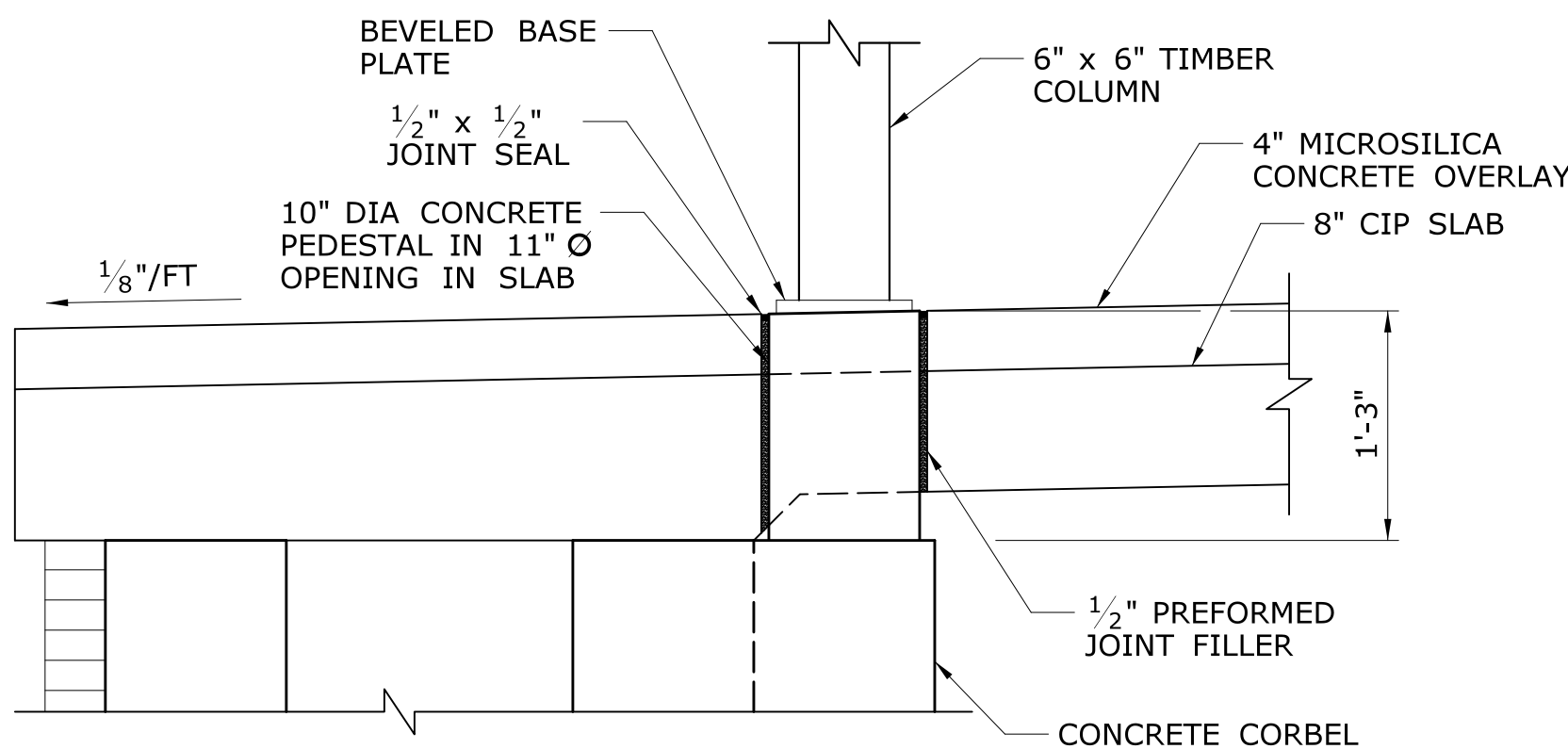
| | | | | | | | | | | | | | | | | | |
|------|--|------|--|---|--|---------------------------------------|--|---|--|--|--|---|--|----------------------------|--|--------------------------------|--|
| | | | | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | | DESIGNER/DRAFTER: C DONOHUE | |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION | | SIGNATURE/ BLOCK:  530 PRESTON AVENUE MERIDEN, CT 06450 | | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | | TOWN: BERLIN | | PROJECT NO. 170-3155 | |
| | | | | | | CHECKED BY: H BUI | | | | DRAWING NO. FST-206 | | | | | | | |
| | | | | | | SCALE AS NOTED | | | | SHEET NO. 01.12.027 | | | | | | | |
| REV. | | DATE | | REVISION DESCRIPTION | | SHEET NO. | | Plotted Date: 2/26/2014 | | Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_206.dgn | | | | | | | |



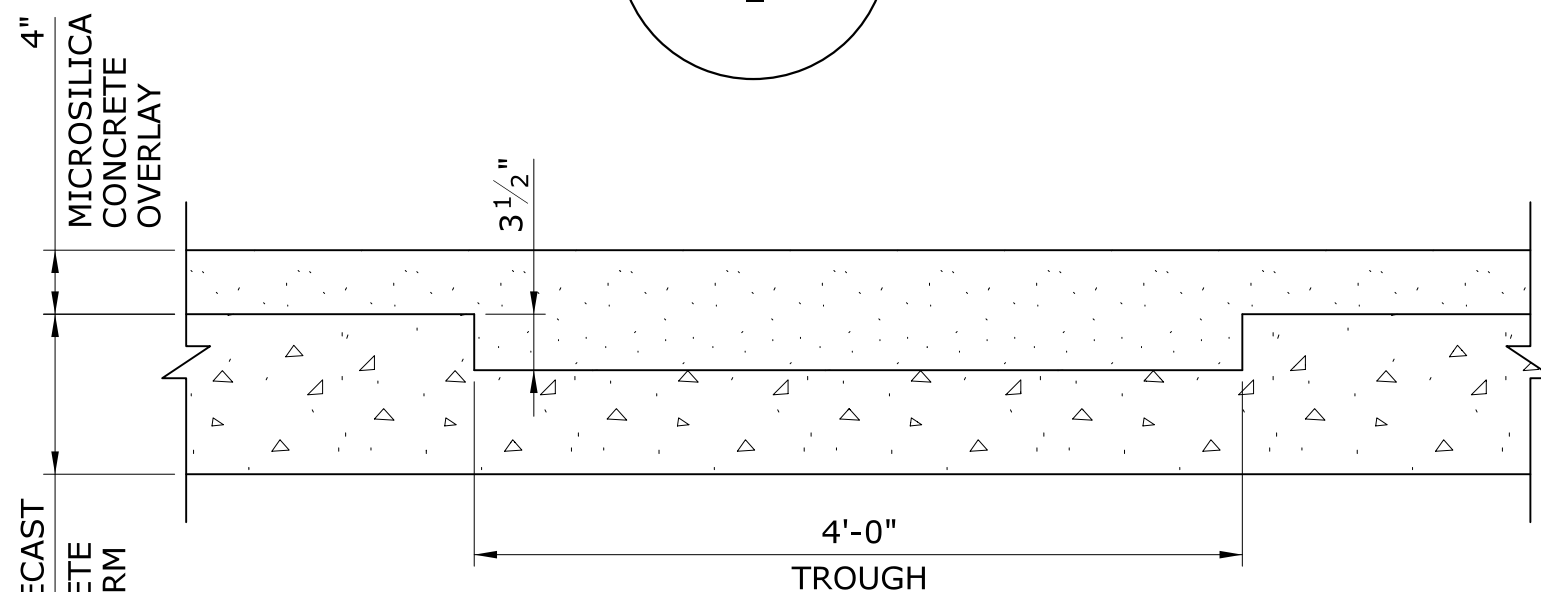


TYPICAL SLAB PLAN (9'-8" PLATFORM)

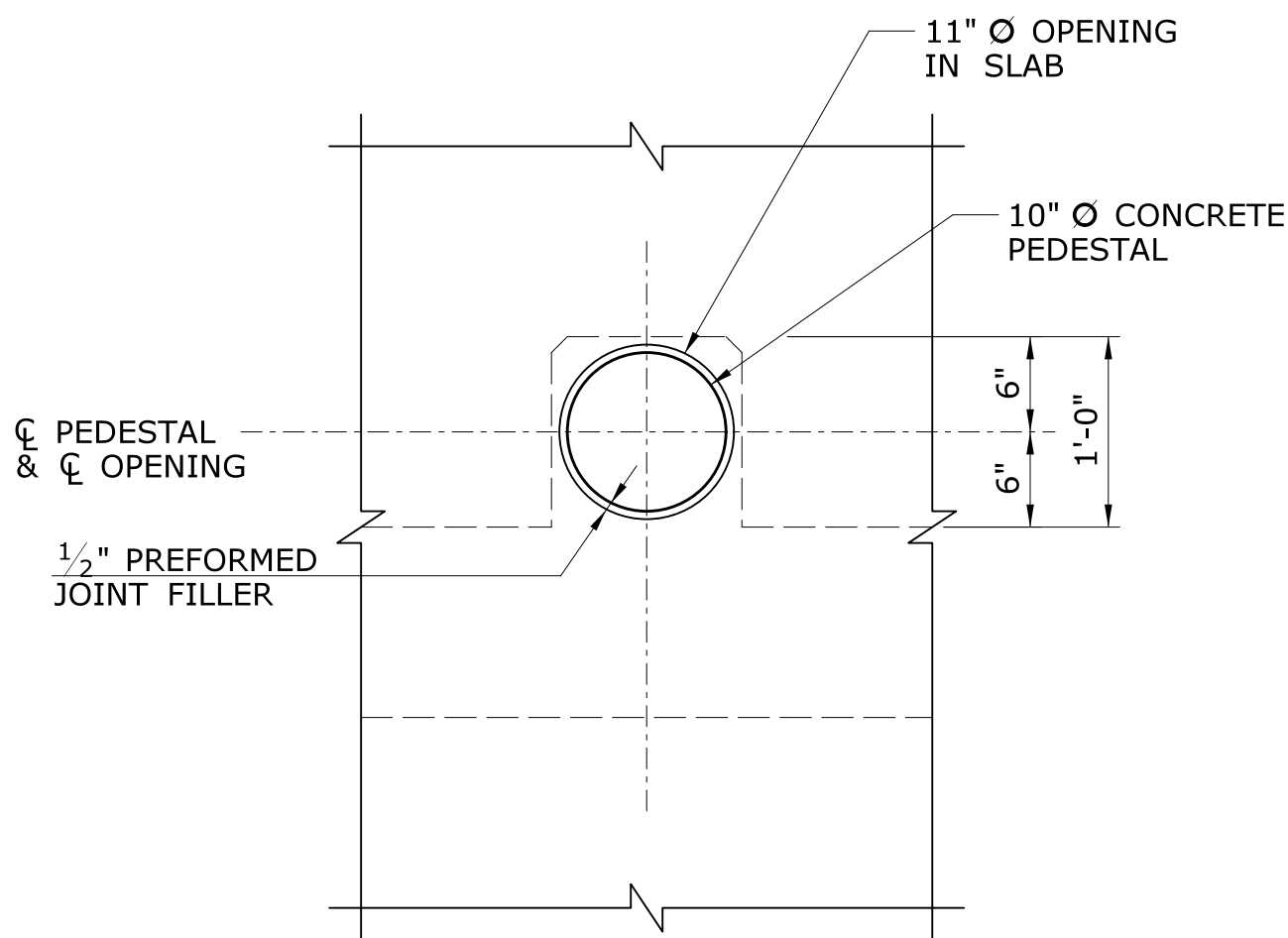
SCALE: 1/4" = 1'-0"



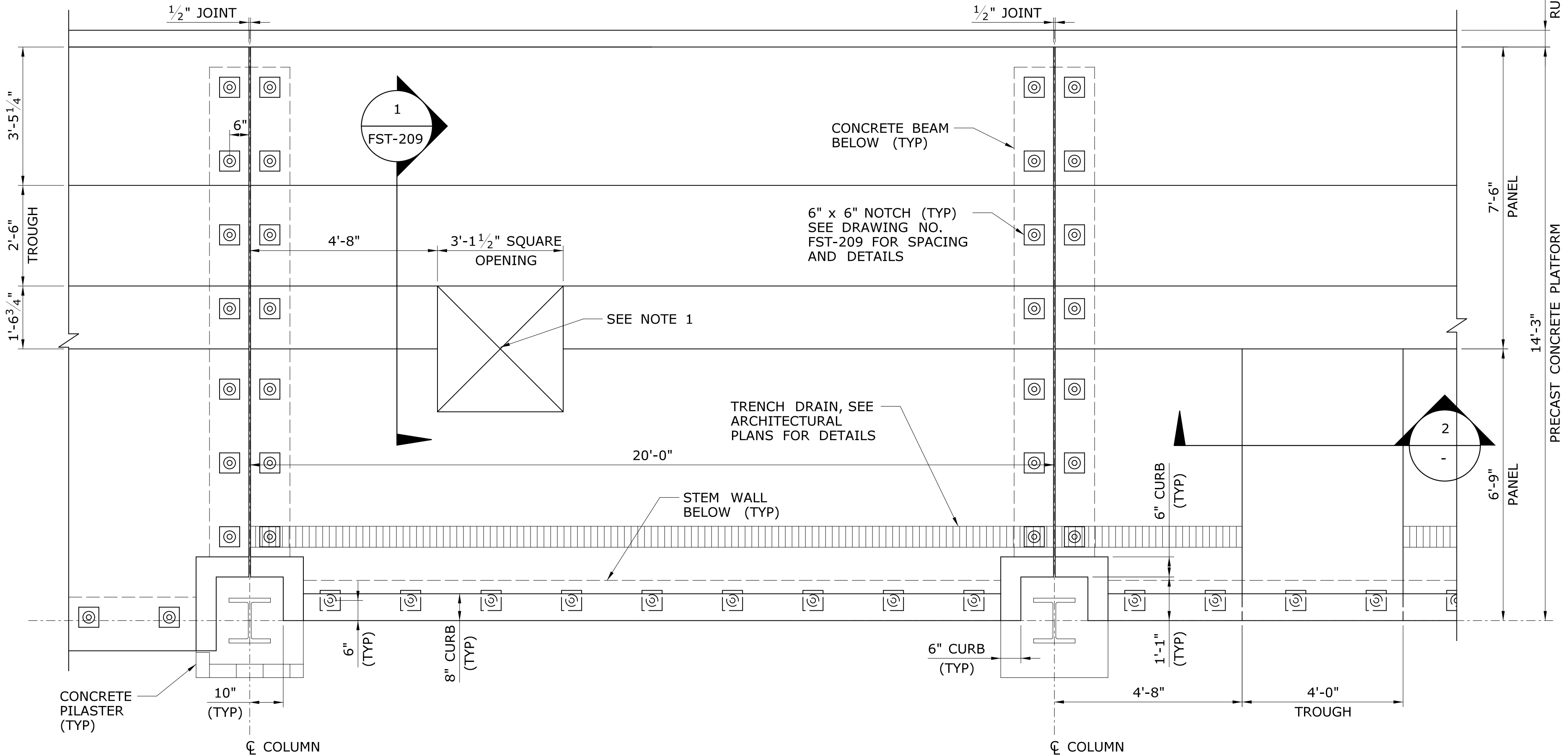
SECTION 1
SCALE: 1" = 1'-0"



SECTION 2
SCALE: 1" = 1'-0"

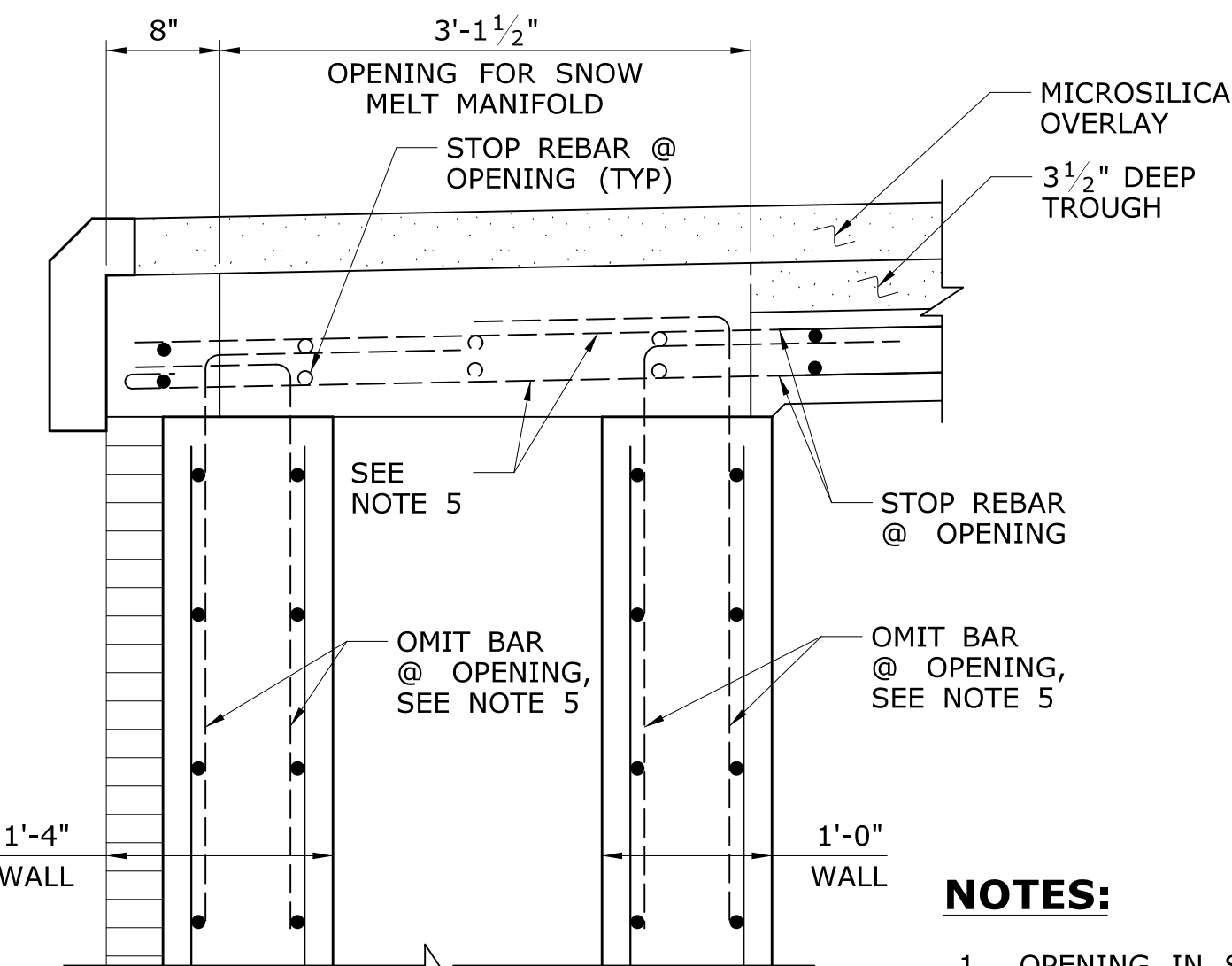


DETAIL 1
SCALE: 1" = 1'-0"

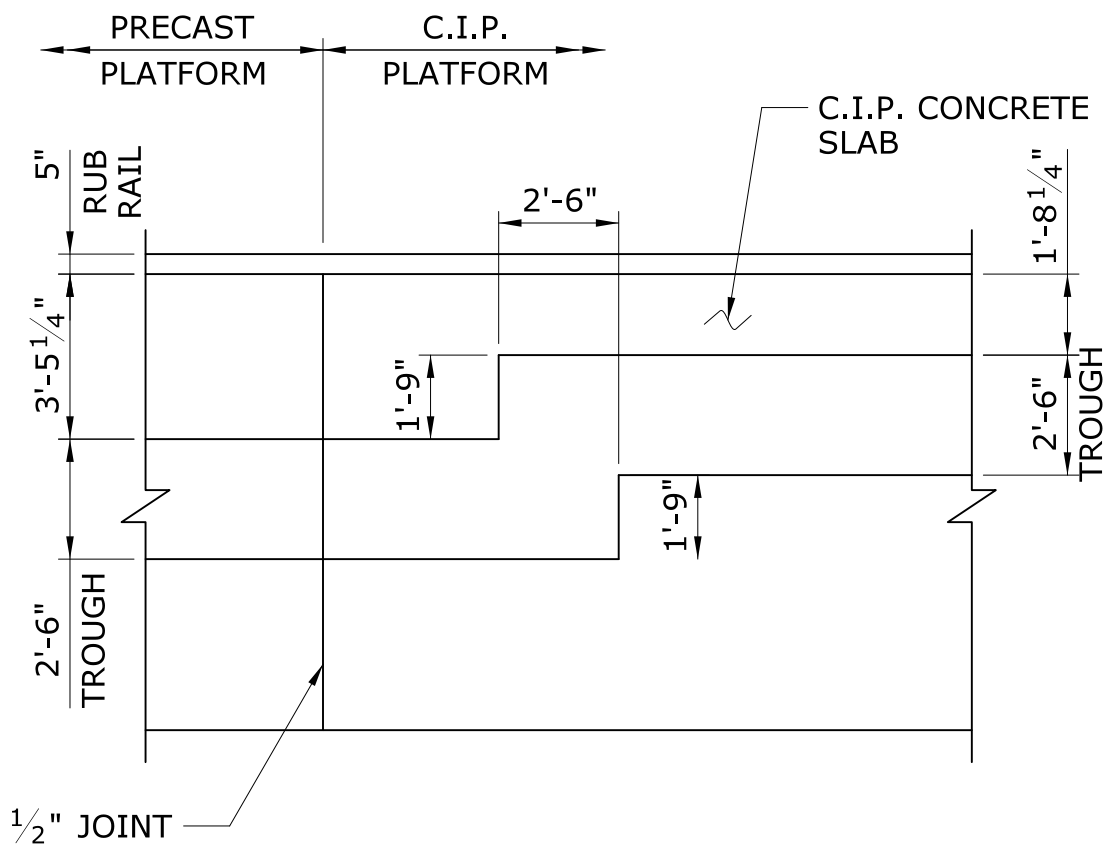


PRECAST PLATFORM AT SNOW MELT EQUIPMENT ROOM

SCALE: 1/2" = 1'-0"



SECTION 3
SCALE: 1" = 1'-0"



TROUGH TRANSITION DETAIL

SCALE: 1/4" = 1'-0"

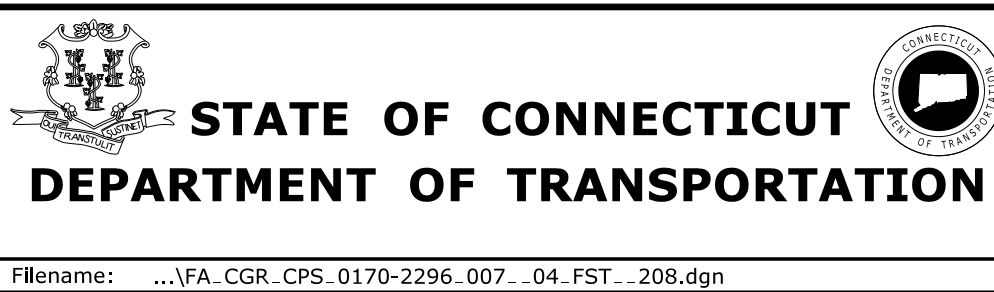
NOTES:

- OPENING IN SLAB FOR SNOW MELT MANIFOLD. SEE "HYDRONIC SNOW MELT PLAN" FOR OVERALL LOCATIONS OF OPENING AND TROUGH.
- THE 4" MICROSILICA CONCRETE OVERLAY SHALL BE REINFORCED WITH 6 x 6 - W2.9 x W2.9 WWF WITH A CLEAR COVER OF 1 1/2".
- FOR CURB REINFORCING DETAILS, SEE DRAWING NO. FST-210.
- FOR TYPICAL REINFORCEMENT DETAIL OF WALL & SLAB, SEE "SECTION 3" ON DRAWING NO. FST-202.
- OMIT THESE BARS AT OPENING FOR SNOW MELT MANIFOLD. PROVIDE ADDITIONAL BARS OF THE SAME SIZE AND SPACING AS WALL SLAB REINFORCEMENT. PLACE ADDITIONAL BARS MIDWAY BETWEEN TYPICAL REINFORCEMENT. ADDITIONAL BARS SHALL BE PROVIDED AT A MINIMUM OF 2'-0" BEYOND THE OPENING.

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

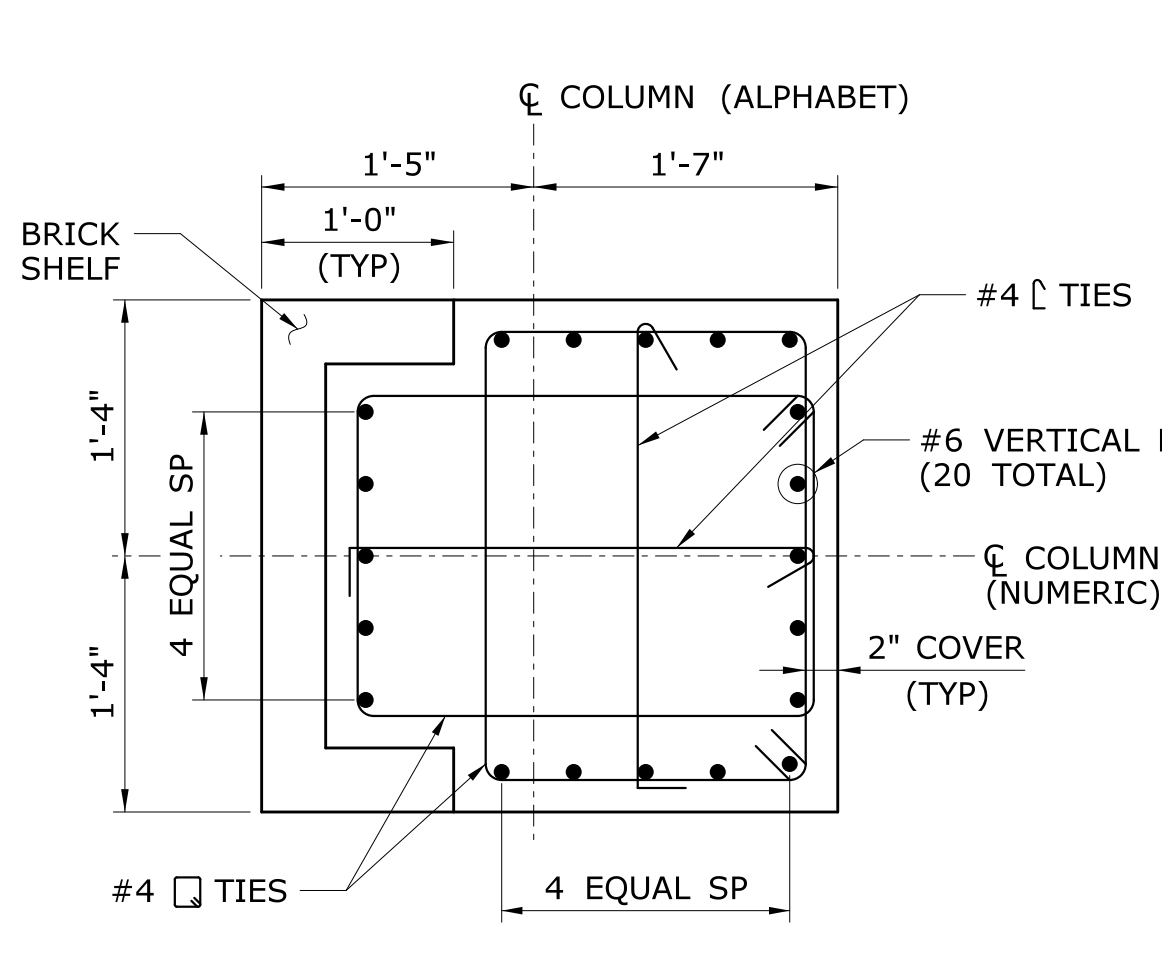


SIGNATURE/
BLOCK:

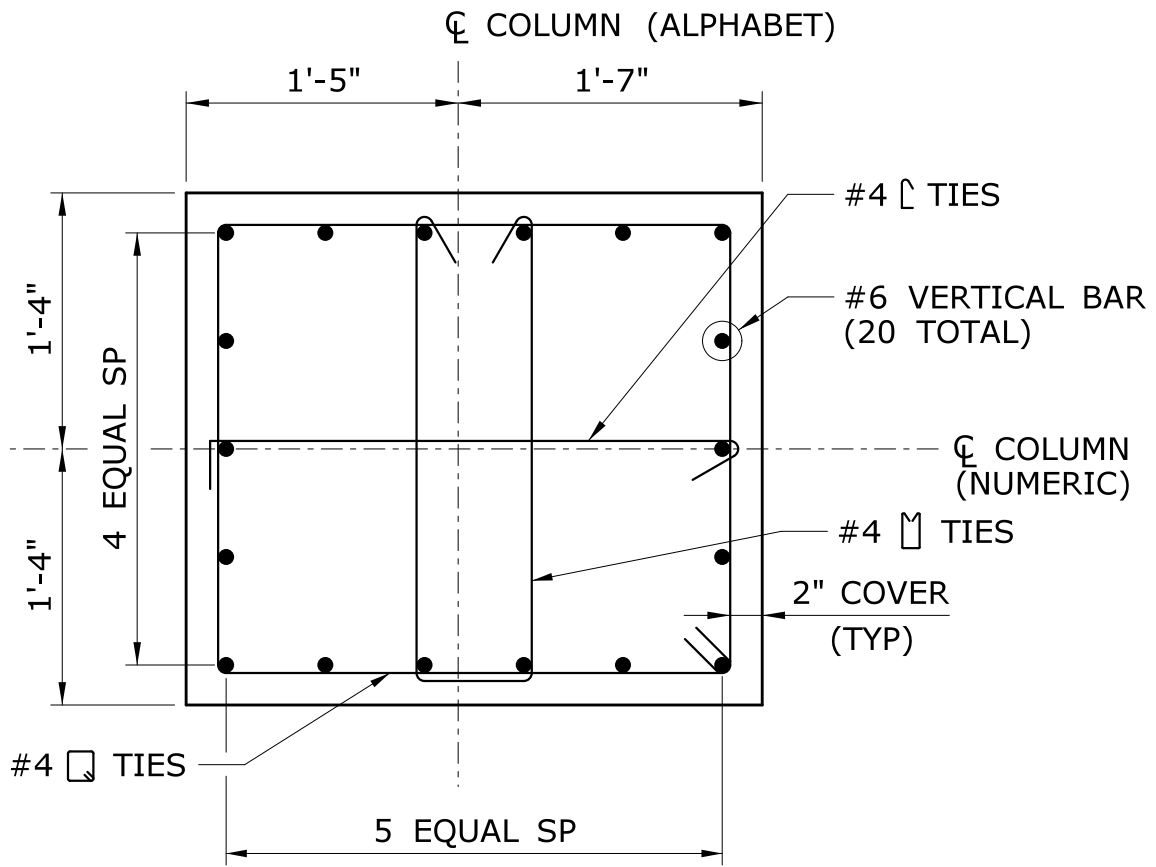
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

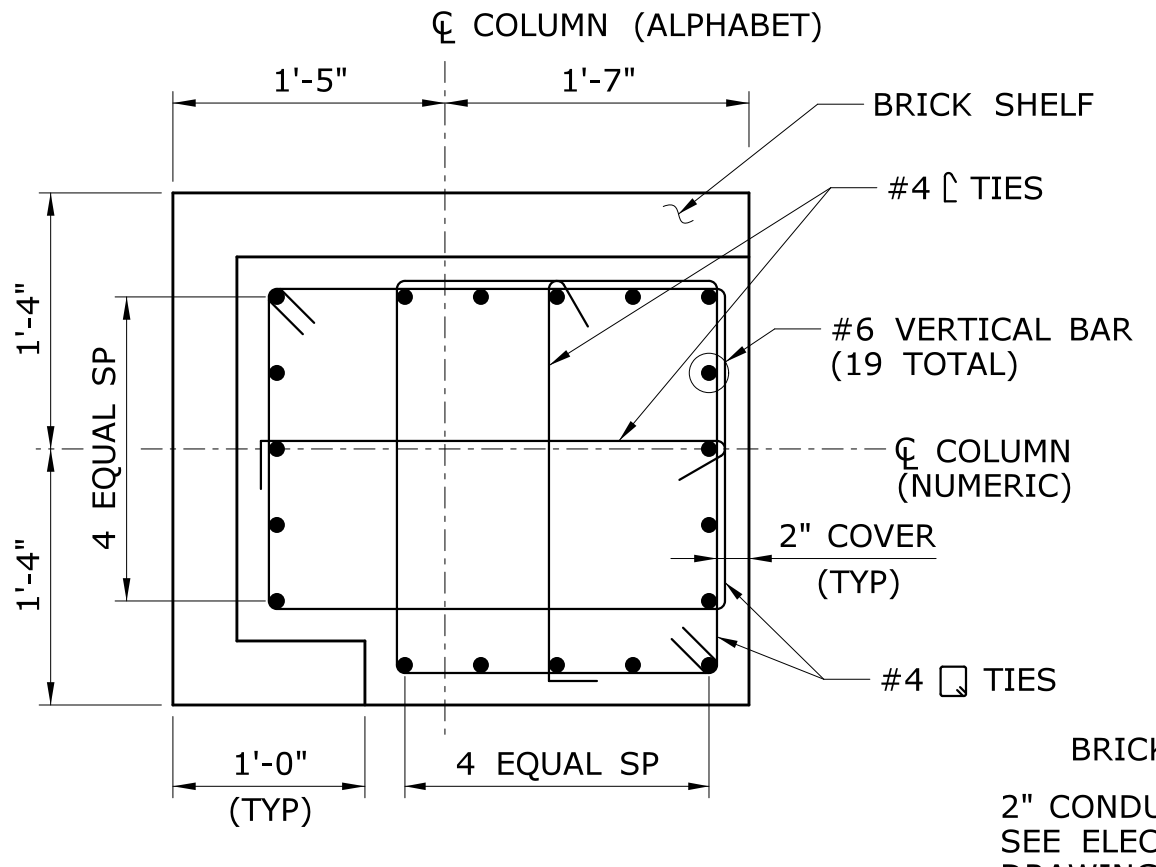
TOWN:
BERLIN
DRAWING TITLE:
**PLATFORM SECTIONS
& DETAILS 9**
PROJECT NO.
170-3155
DRAWING NO.
FST-208
SHEET NO.
01.12.029



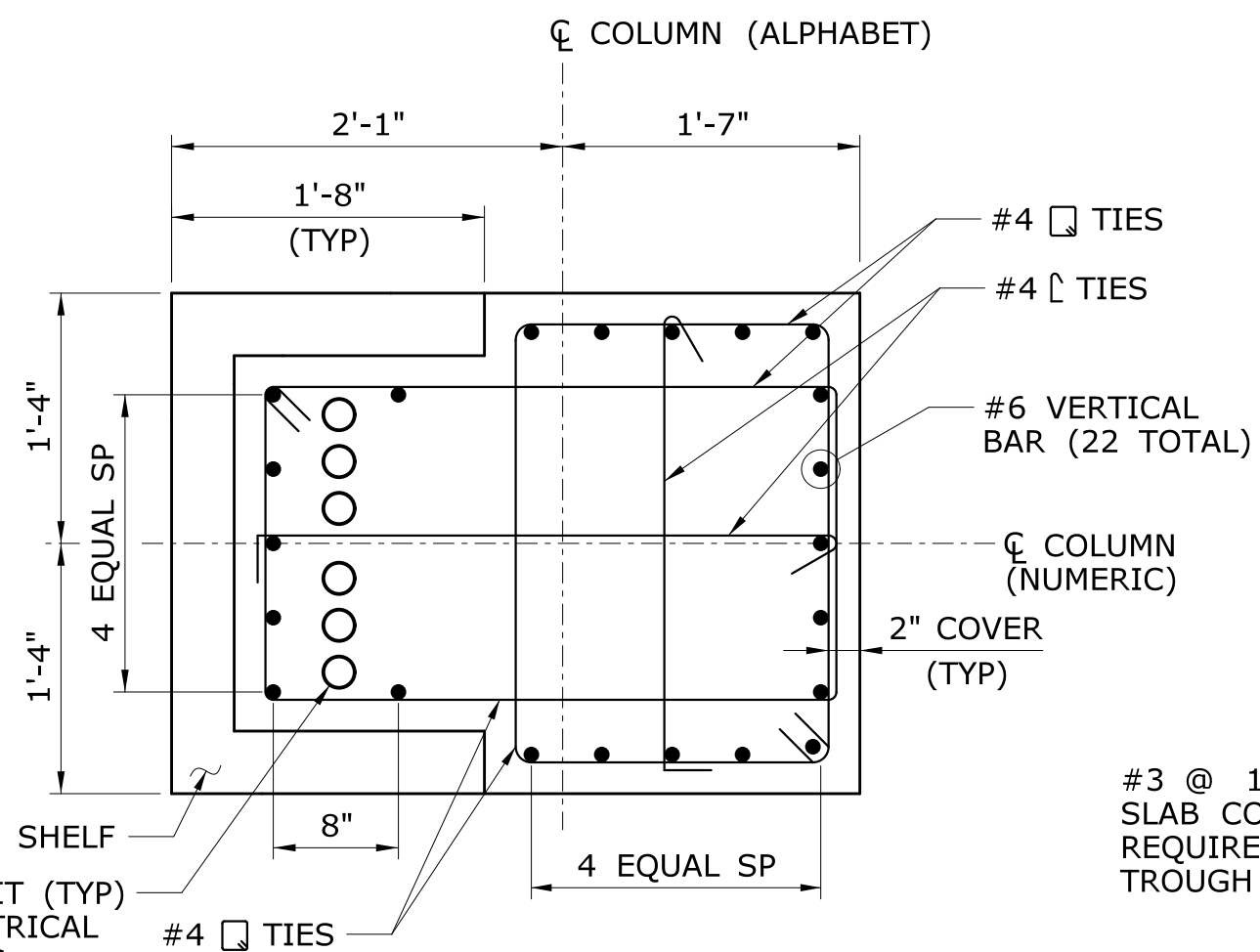
TYPE 1 PILASTER
SCALE: 1" = 1'-0"



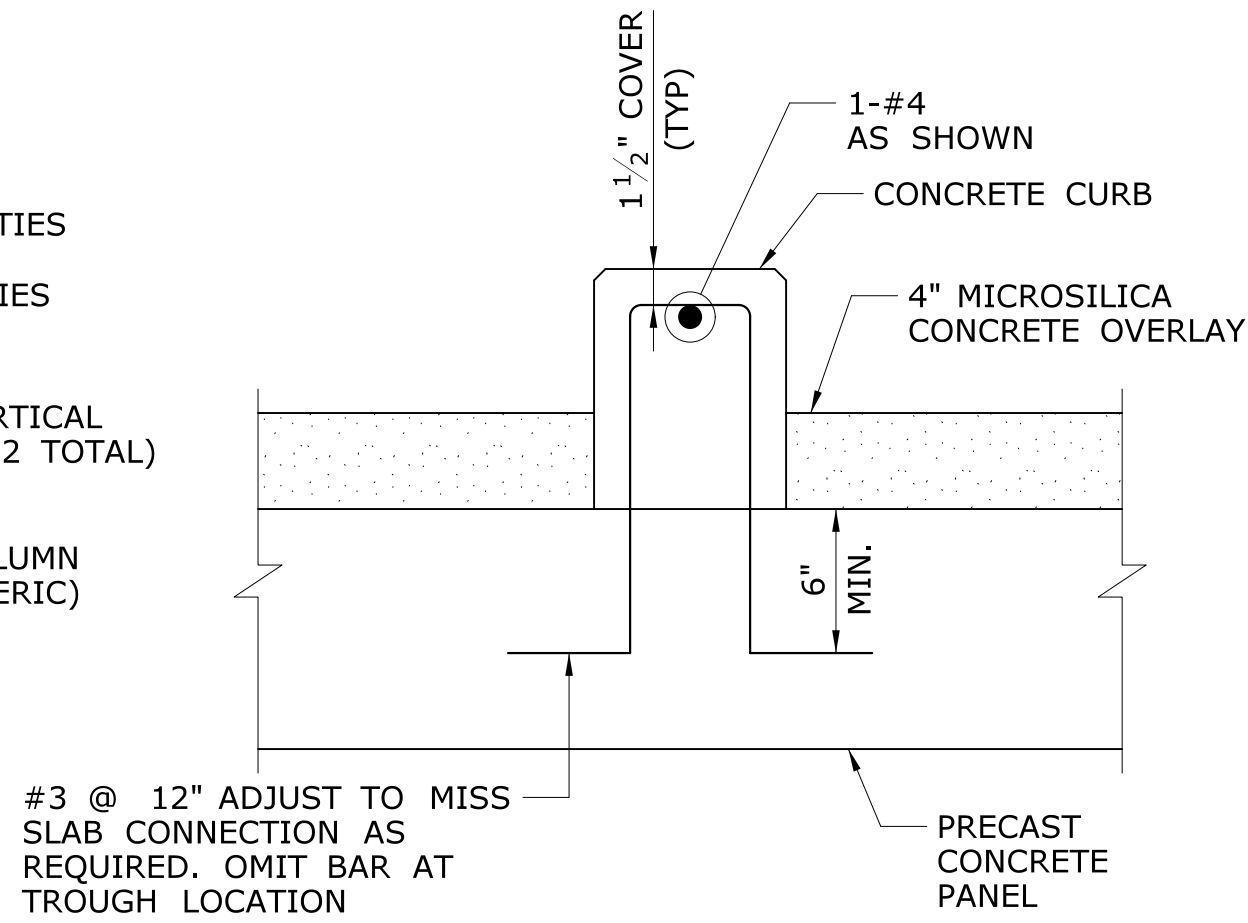
TYPE 2 PILASTER
SCALE: 1" = 1'-0"



TYPE 3 PILASTER
SCALE: 1" = 1'-0"



TYPE 4 PILASTER
SCALE: 1" = 1'-0"



CURB REINFORCING DETAIL
SCALE: 1 1/2" = 1'-0"

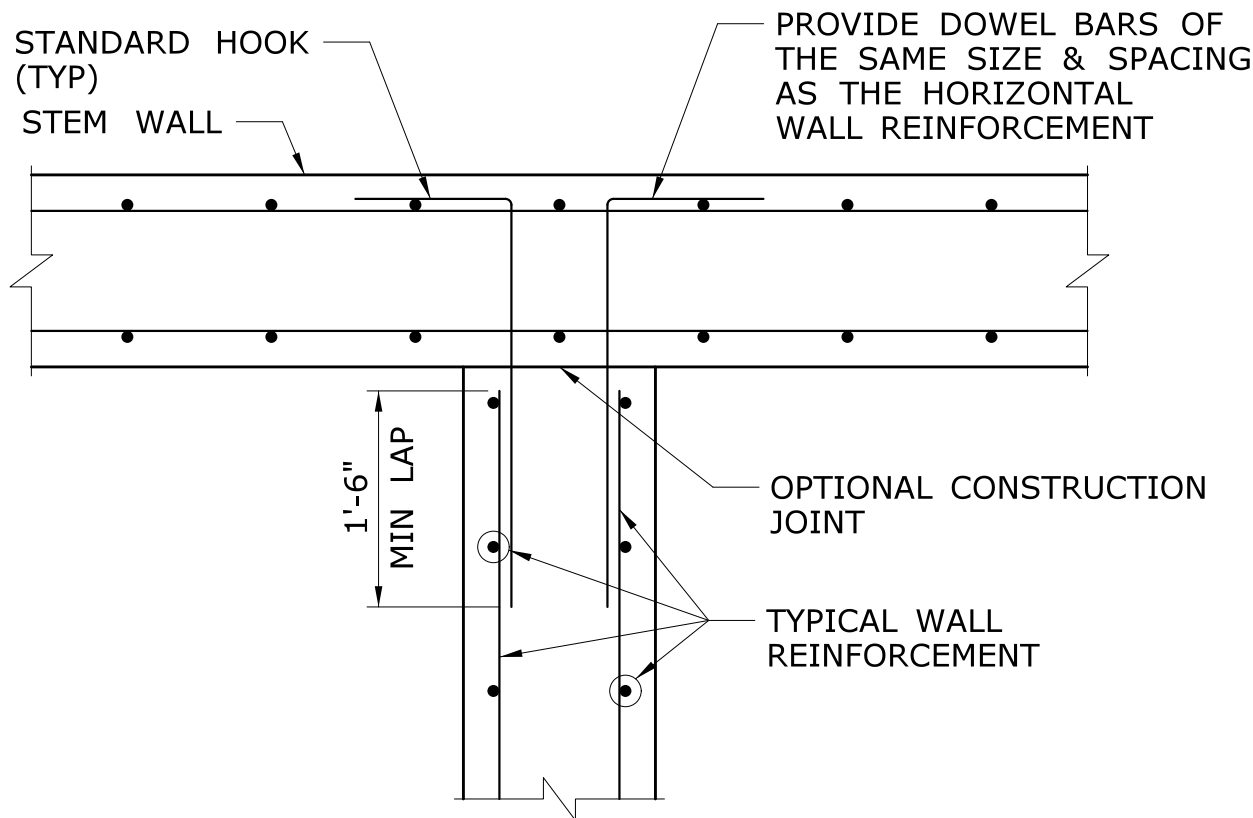
| COLUMN | PILASTER TYPE |
|--------|---------------|
| 1E | TYPE 3 |
| 2E | TYPE 1 |
| 3E | TYPE 1 |
| 4E | TYPE 1 |
| 5E | TYPE 1 |
| 14E | TYPE 4 |
| 15E | TYPE 2 |
| 16E | TYPE 2 |
| 17E | TYPE 2 |
| 18E | TYPE 1 |
| 19E | TYPE 2 |

| COLUMN | PILASTER TYPE |
|--------|---------------|
| 20E | TYPE 1 |
| 21E | TYPE 4 |
| 22E | TYPE 1 |
| 23E | TYPE 1 |
| 24E | TYPE 1 |
| 25E | TYPE 1 |
| 26E | TYPE 3 |

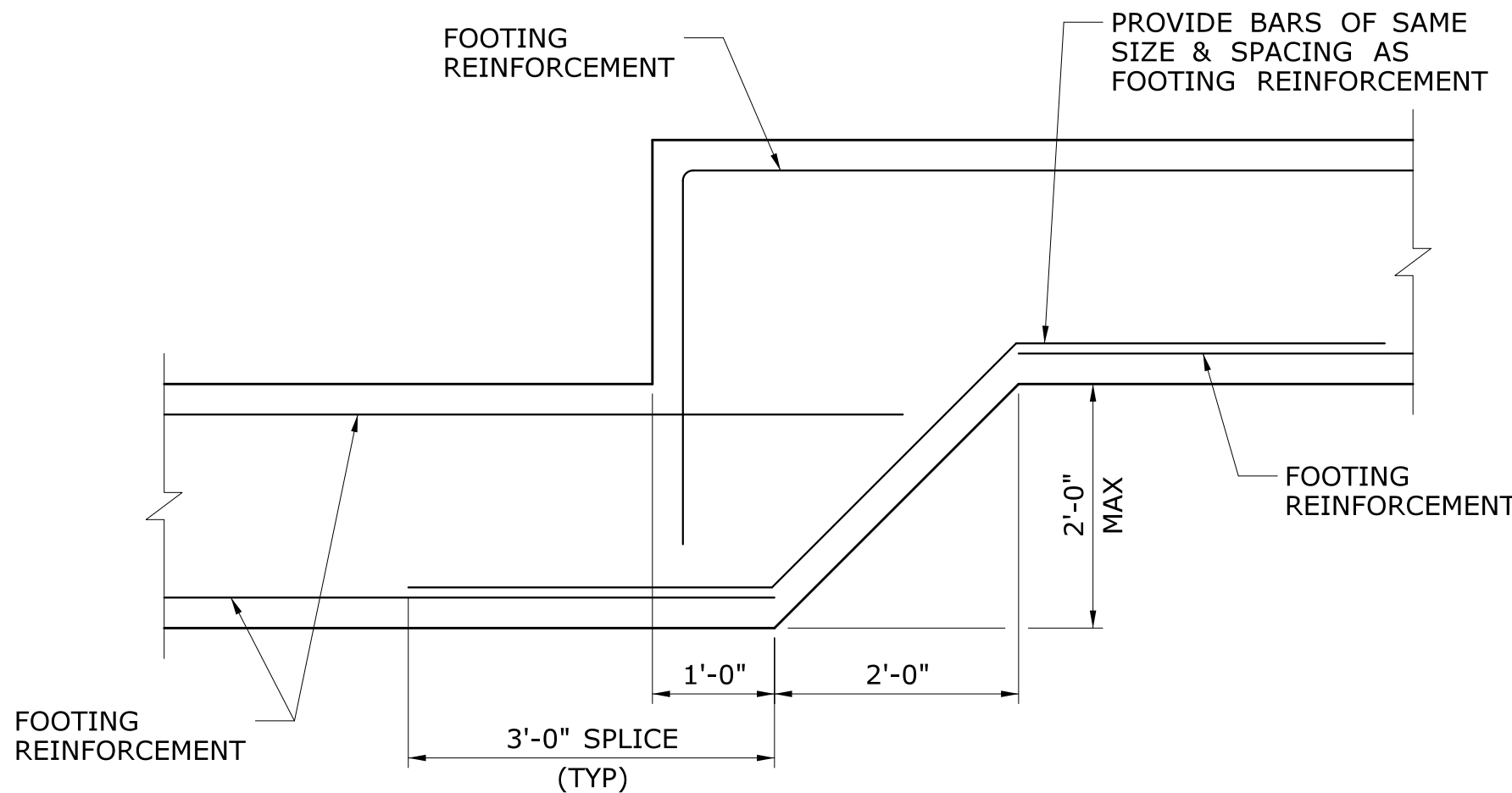
| COLUMN | PILASTER TYPE |
|--------|---------------|
| 6W | TYPE 3 |
| 7W | TYPE 1 |
| 8W | TYPE 1 |
| 9W | TYPE 1 |
| 10W | TYPE 1 |
| 11W | TYPE 1 |
| 12W | TYPE 1 |
| 13W | TYPE 1 |
| 14W | TYPE 4 |
| 15W | TYPE 2 |
| 16W | TYPE 2 |

| COLUMN | PILASTER TYPE |
|--------|---------------|
| 17W | TYPE 2 |
| 18W | TYPE 1 |
| 19W | TYPE 1 |
| 20W | TYPE 1 |
| 21W | TYPE 1 |
| 22W | TYPE 1 |
| 23W | TYPE 2 |
| 24W | TYPE 4 |
| 25W | TYPE 1 |
| 26W | TYPE 1 |
| 27W | TYPE 1 |

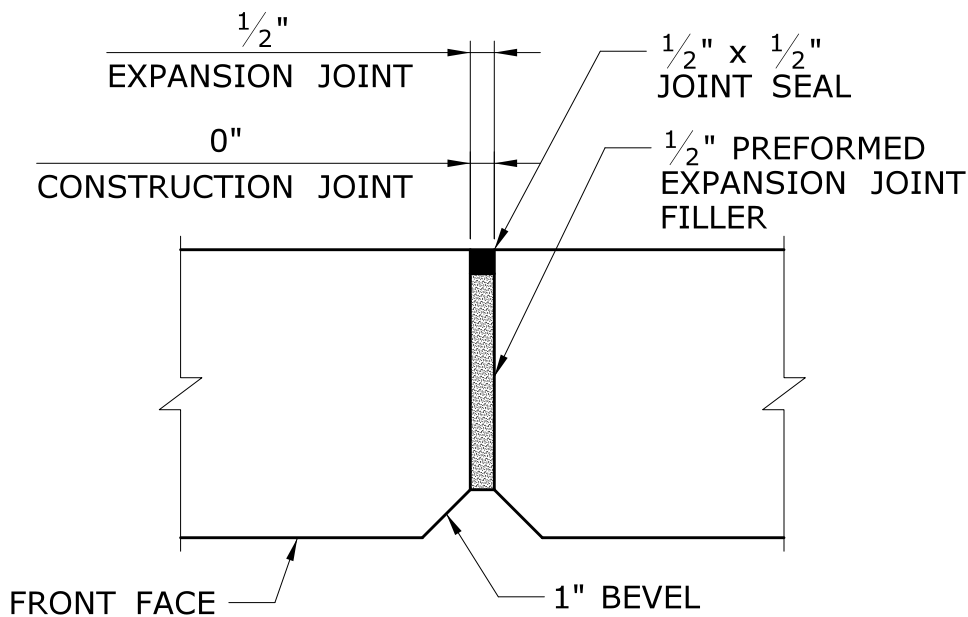
| COLUMN | PILASTER TYPE |
|--------|---------------|
| 28W | TYPE 1 |
| 29W | TYPE 1 |
| 30W | TYPE 1 |
| 31W | TYPE 3 |



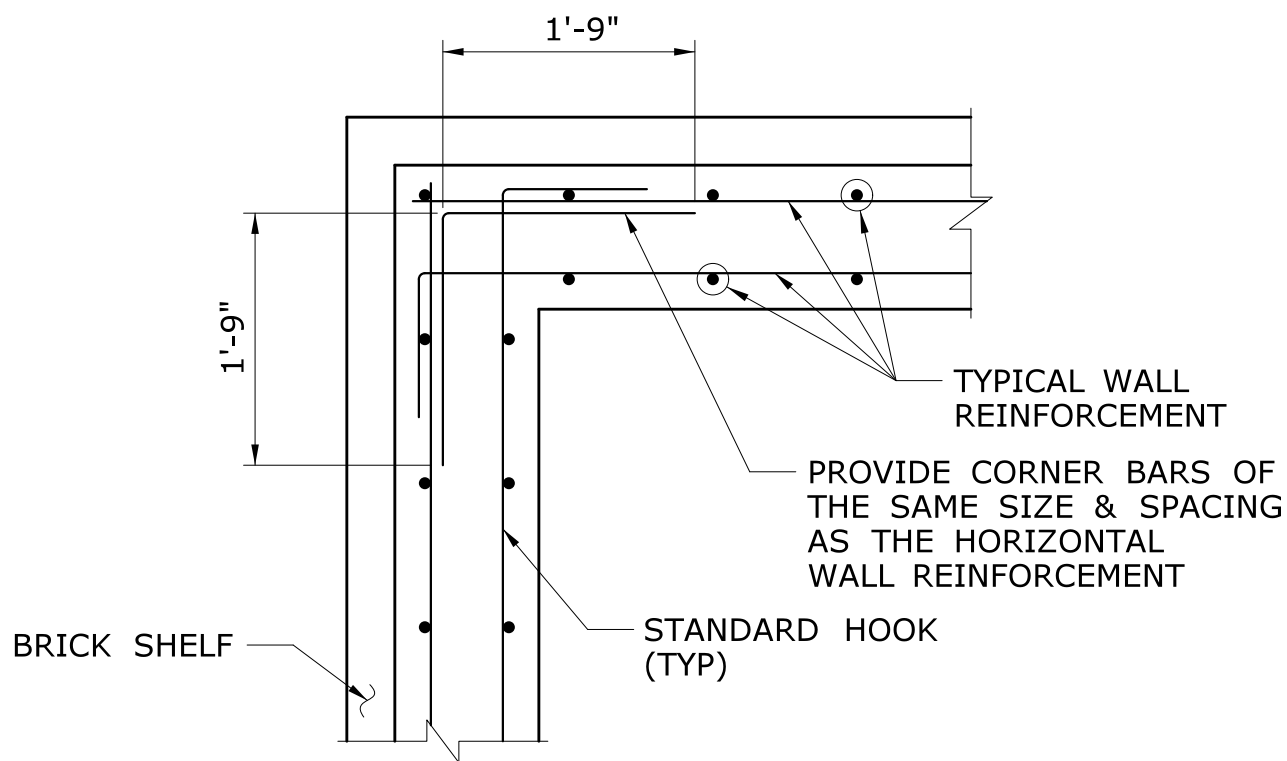
TYPICAL WALL INTERFACE REINFORCING
SCALE: 3/4" = 1'-0"



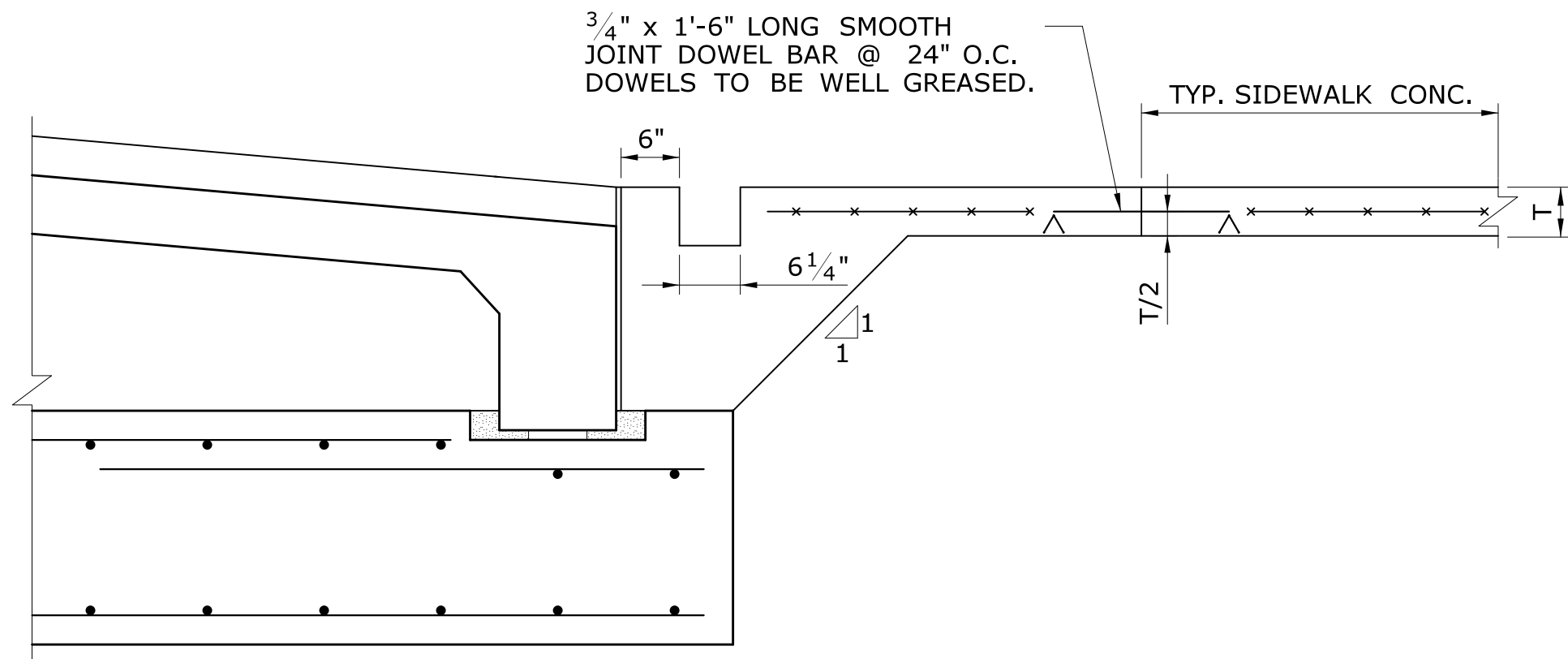
STEPPED FOOTING DETAIL
SCALE: 3/4" = 1'-0"



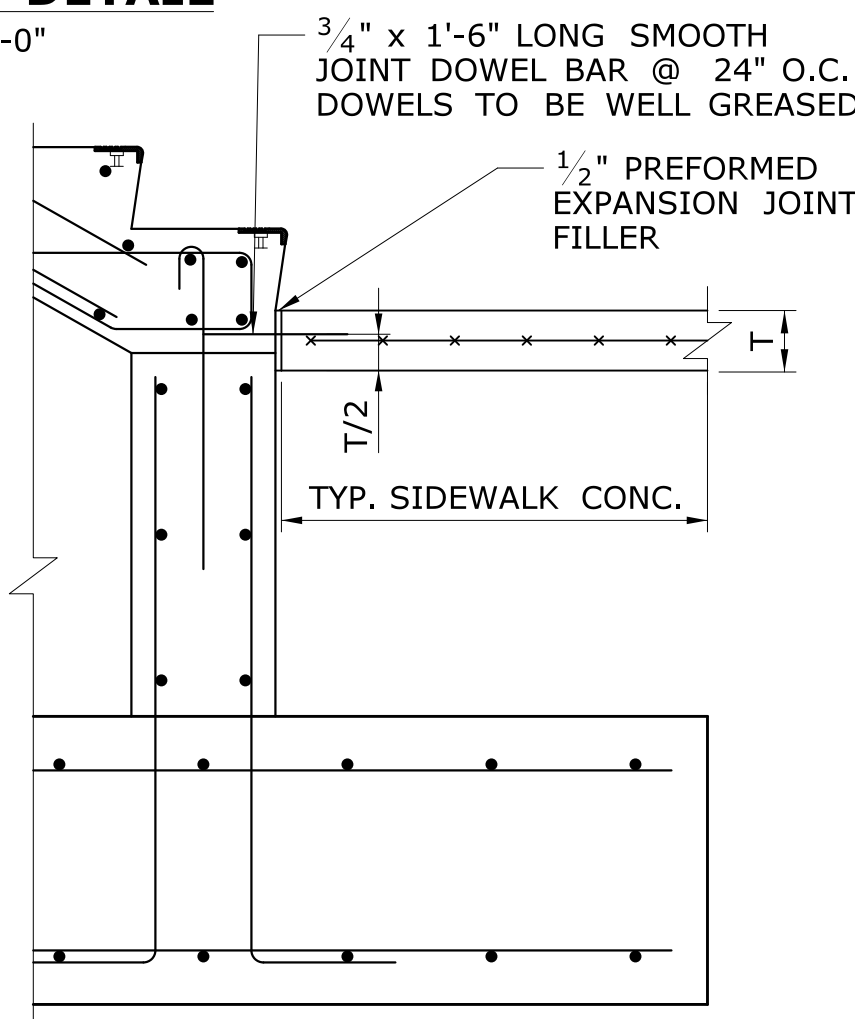
VERTICAL STEM JOINT DETAIL
SCALE: 3" = 1'-0"



TYPICAL WALL CORNER REINFORCING
SCALE: 3/4" = 1'-0"



TYPICAL PRECAST RAMP/SIDEWALK DETAIL
SCALE: 3/4" = 1'-0"



TYPICAL CIP STAIR/SIDEWALK DETAIL
SCALE: 3/4" = 1'-0"

NOTES:

- JOINT SEAL TO EXTEND FROM TOP OF FOOTING TO TOP OF WALL.
- NO REINFORCEMENT SHALL PASS THROUGH EXPANSION JOINTS. REINFORCEMENT SHALL PASS THROUGH CONSTRUCTION JOINTS.
- JOINT SEAL AND PREFORM EXPANSION JOINT FILLER SHALL BE INCLUDED IN THE CONCRETE ITEM.

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

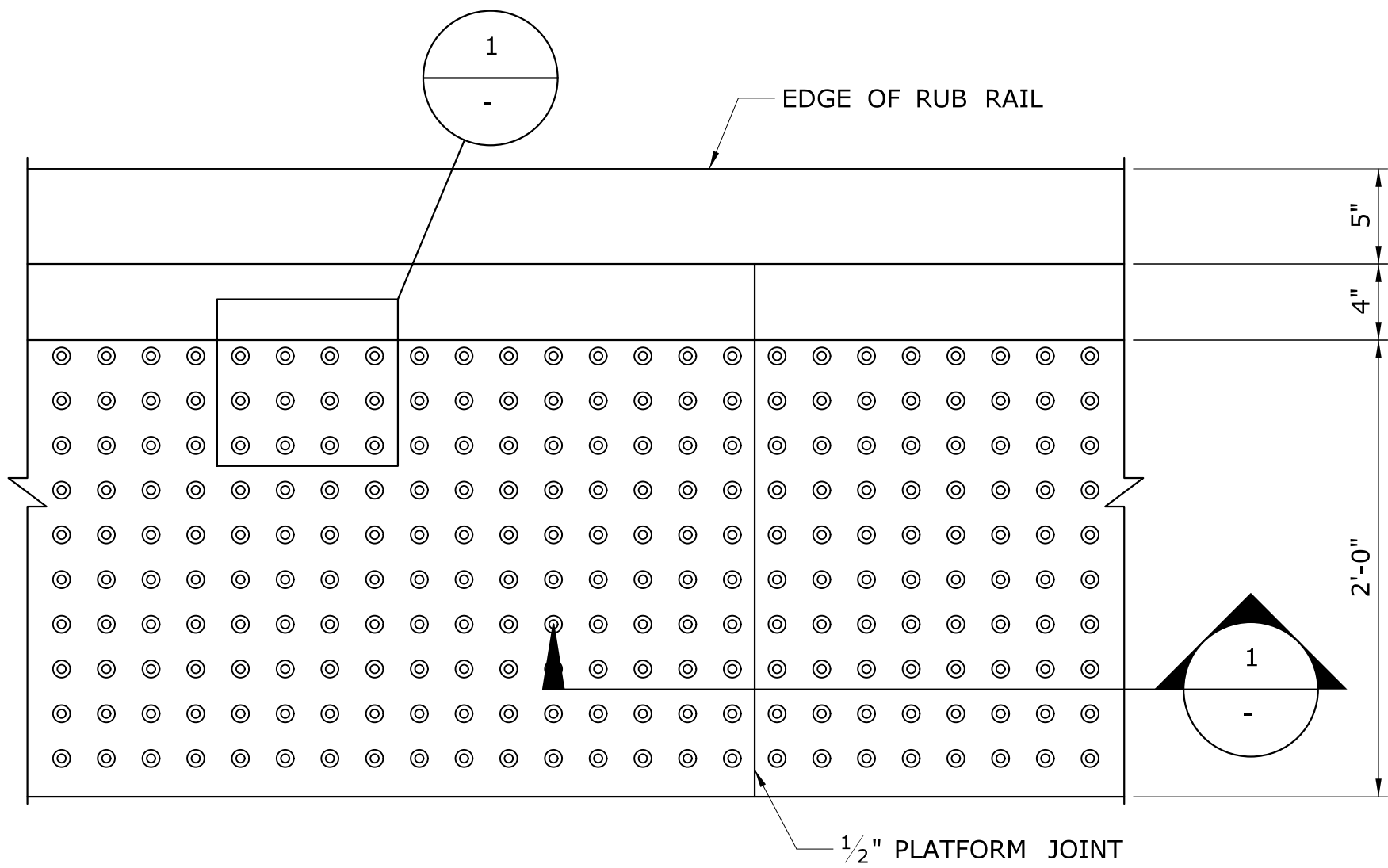
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_210.dgn

SIGNATURE/BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

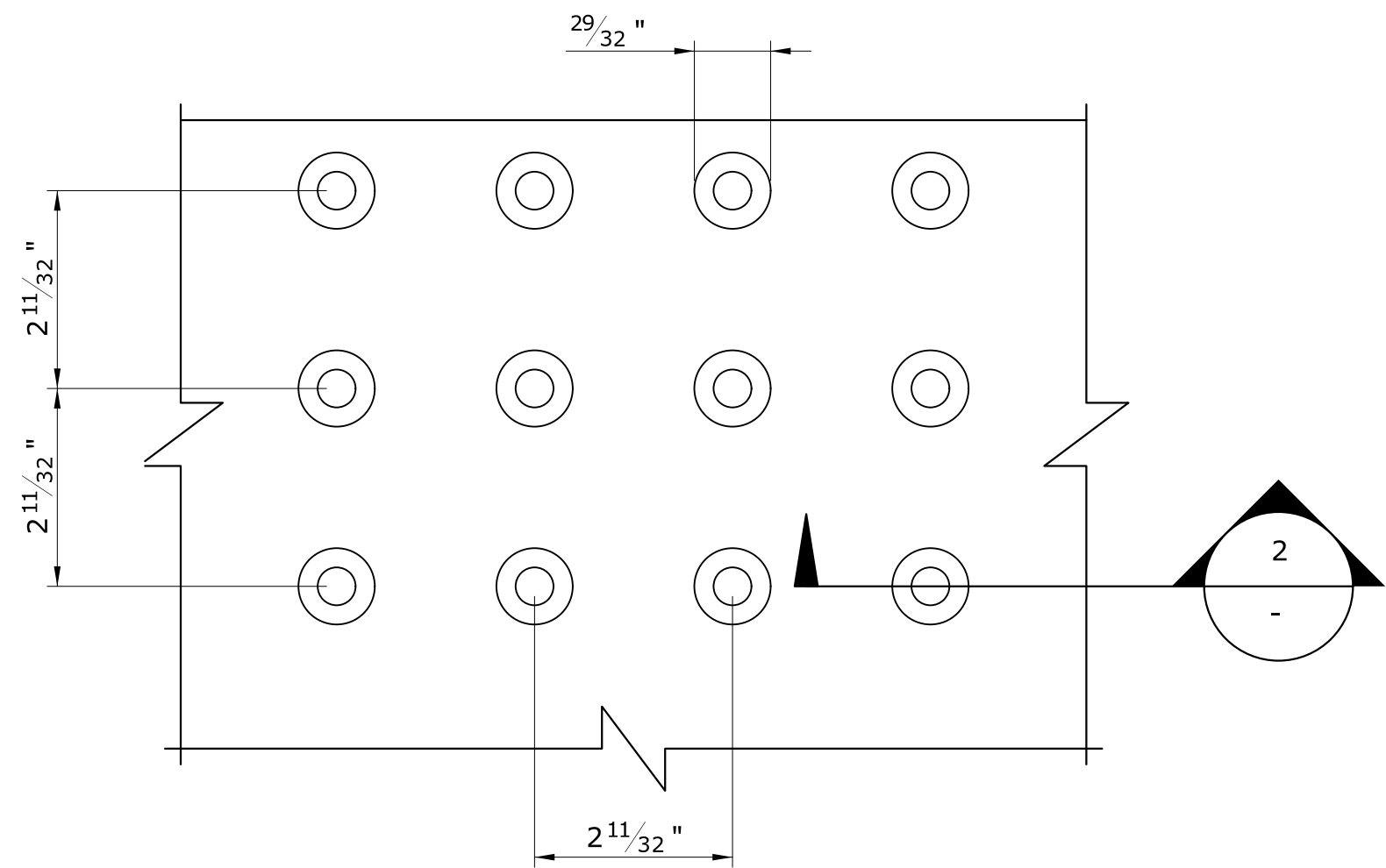
PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**PLATFORM SECTIONS
& DETAILS 11**

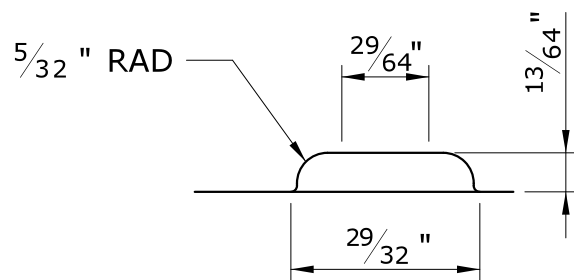
PROJECT NO.
170-3155
DRAWING NO.
FST-210
SHEET NO.
01.12.031



TACTILE WARNING STRIP PLAN
SCALE: 1 1/2" = 1'-0"

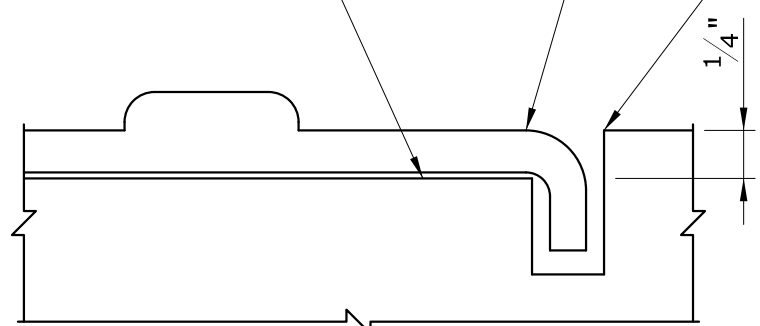


1
-
DETAIL
SCALE: 6" = 1'-0"



2
-
SECTION
SCALE: 1" = 1"

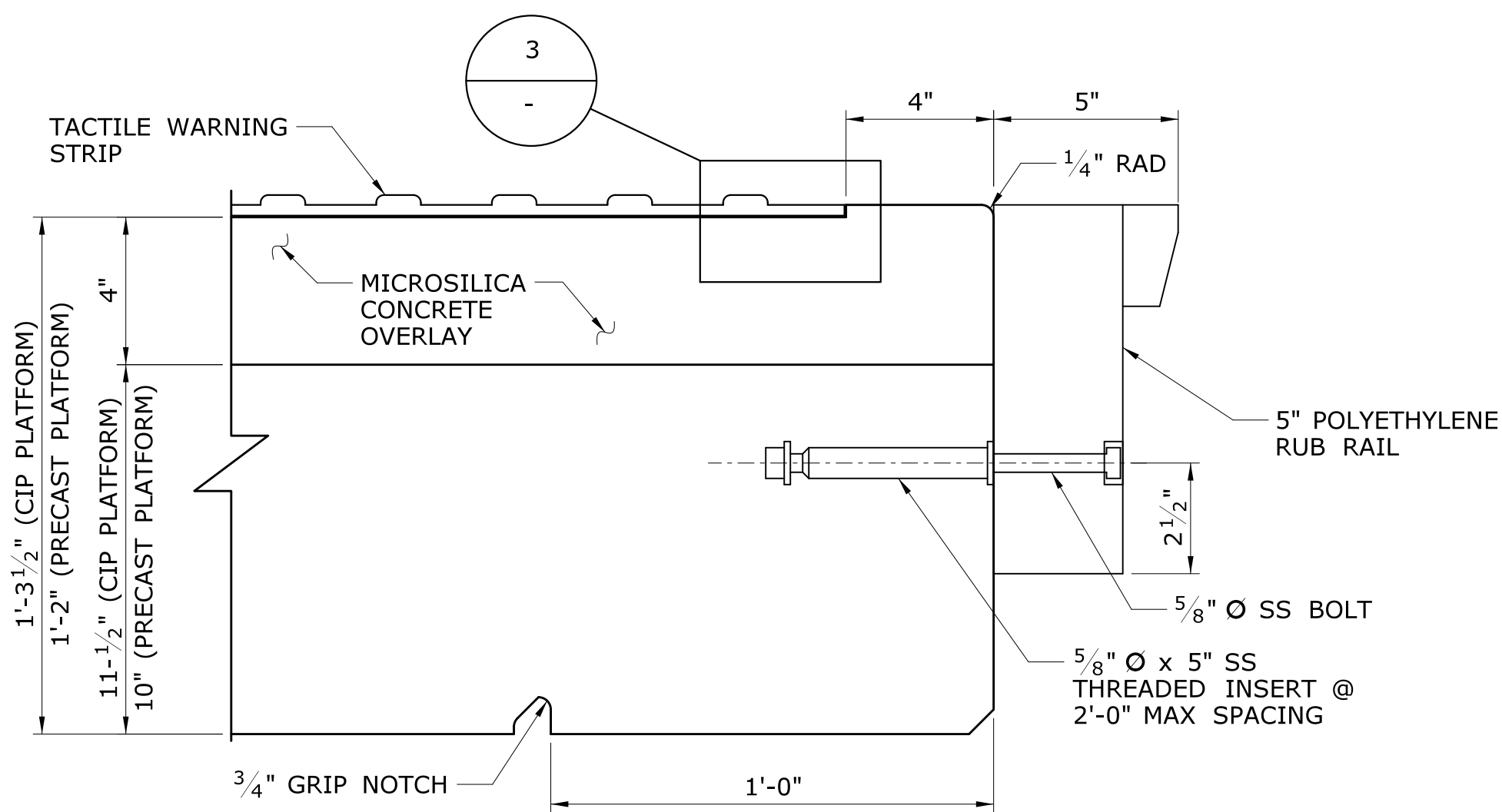
MILL CONCRETE SURFACE PER
MANUFACTURER'S RECOMMENDATIONS.



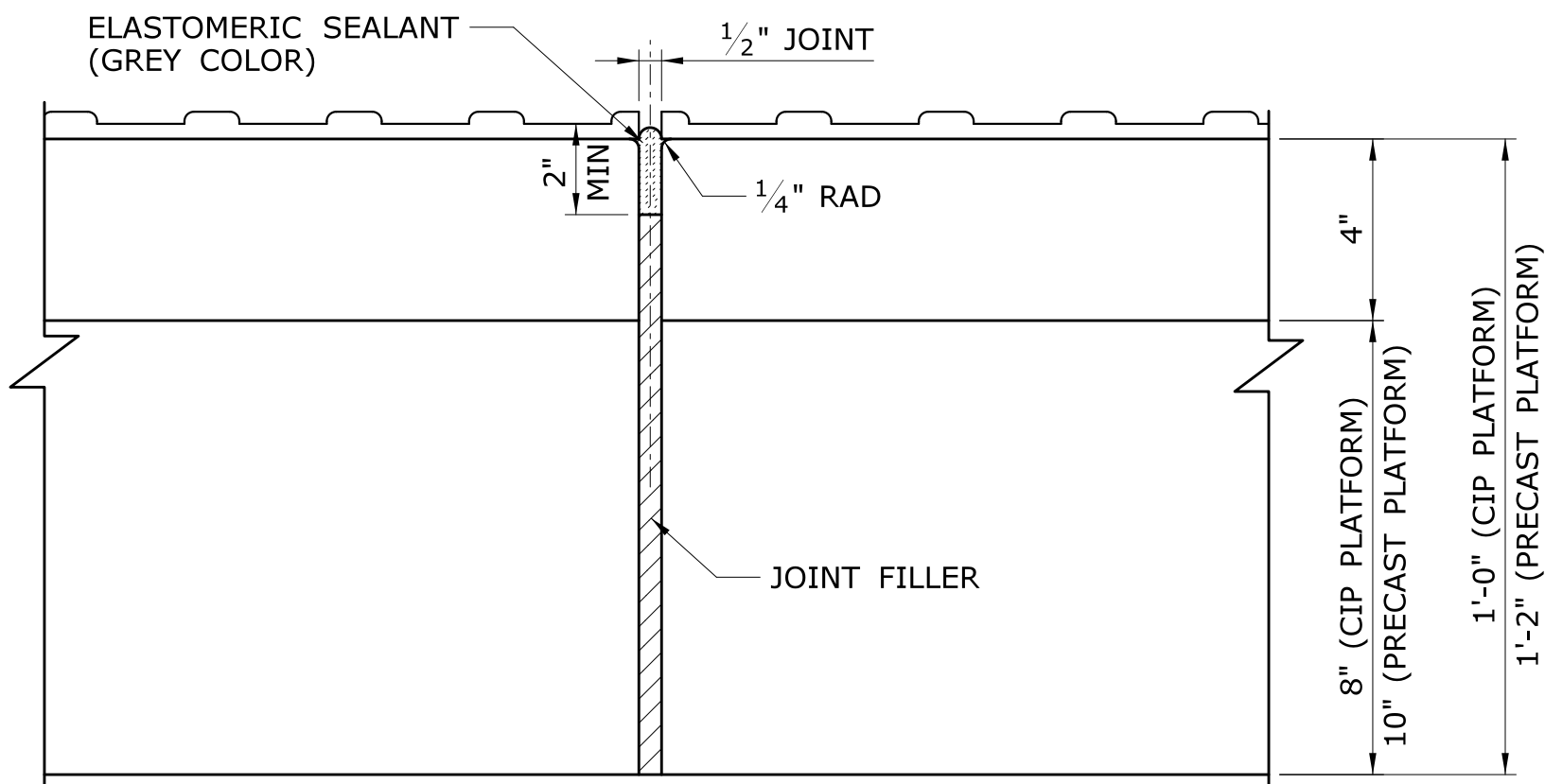
3
-
DETAIL
SCALE: 1" = 1"

CONT. 24" WIDE TACTILE SURFACE
INSTALLED PER MANUFACTURER'S
RECOMMENDATIONS.

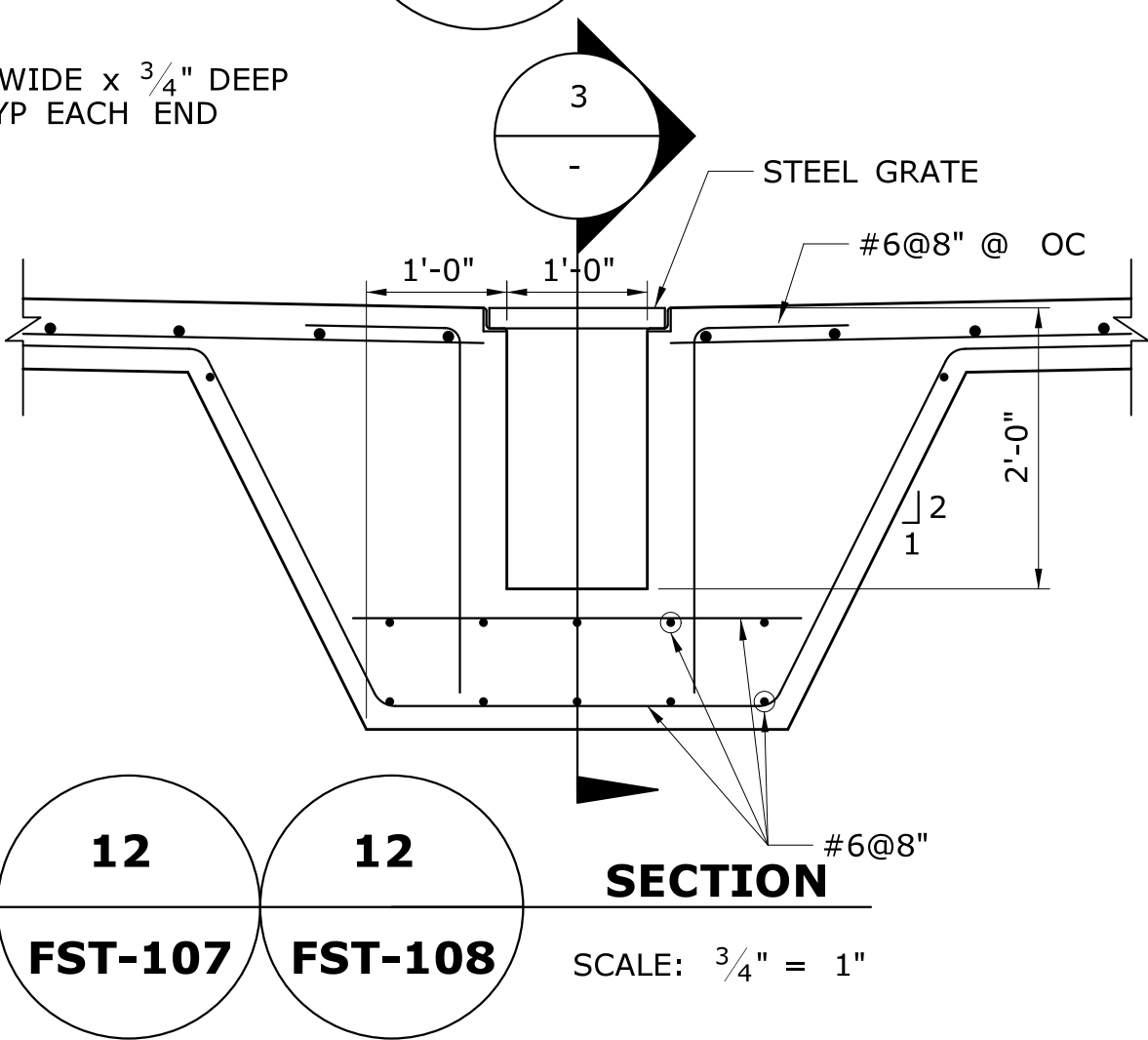
CONT. 3/8" WIDE x 3/4" DEEP
SAWCUT TYP EACH END



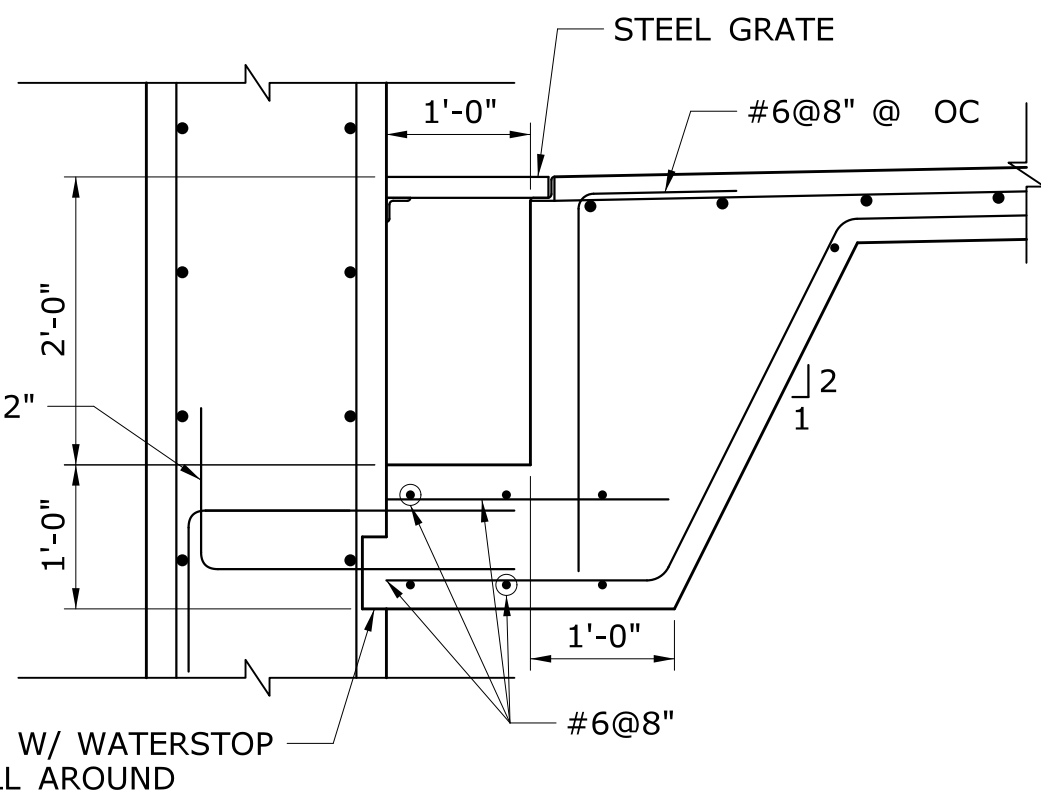
2
-
FST-200
2
-
FST-202
DETAIL
SCALE: 3" = 1'-0"



1
-
SECTION
SCALE: 3" = 1"



12
-
FST-107
12
-
FST-108
SECTION
SCALE: 3/4" = 1"



3
-
SECTION
SCALE: 3/4" = 1"

| ELEVATION TABLE (FT) | | | | |
|----------------------|-------|-------|-------|-------|
| COLUMN | PT. 1 | PT. 2 | PT. 3 | TSE |
| 1E | 65.79 | 64.08 | 64.19 | 61.84 |
| 2E | 65.69 | 63.98 | 64.09 | 67.07 |
| 3E | 65.59 | 63.88 | 63.99 | 65.64 |
| 4E | 65.49 | 63.78 | 63.89 | 64.87 |
| 5E | 65.39 | 63.68 | 63.79 | 63.44 |
| 14E | 64.48 | 62.77 | 62.88 | 61.86 |
| 15E | 64.38 | 62.67 | 62.78 | N/A |
| 16E | 64.28 | 62.57 | 62.68 | N/A |
| 17E | 64.18 | 62.47 | 62.58 | N/A |
| 18E | 64.08 | 62.37 | 62.48 | 61.26 |
| 19E | 63.98 | 62.27 | 62.38 | N/A |
| 20E | 63.88 | 62.17 | 62.28 | 61.26 |
| 21E | 63.78 | 62.07 | 62.18 | 61.16 |
| 22E | 63.68 | 61.97 | 62.08 | 61.06 |
| 23E | 63.58 | 61.87 | 61.98 | 62.30 |
| 24E | 63.48 | 61.77 | 61.88 | 63.53 |
| 25E | 63.38 | 61.67 | 61.78 | 64.76 |
| 26E | 63.28 | 61.57 | 61.68 | 60.65 |

| ELEVATION TABLE (FT) | | | | |
|----------------------|-------|-------|-------|-------|
| COLUMN | PT. 1 | PT. 2 | PT. 3 | TSE |
| 6W | 65.27 | 63.56 | 63.67 | 61.32 |
| 7W | 65.17 | 63.46 | 63.57 | 66.56 |
| 8W | 65.07 | 63.36 | 63.47 | 65.12 |
| 9W | 64.97 | 63.26 | 63.37 | 64.36 |
| 10W | 64.87 | 63.16 | 63.27 | 62.93 |
| 11W | 64.77 | 63.06 | 63.17 | 61.50 |
| 12W | 64.67 | 62.96 | 63.07 | 61.40 |
| 13W | 64.57 | 62.86 | 62.97 | 61.31 |
| 14W | 64.47 | 62.76 | 62.87 | 61.87 |
| 15W | 64.37 | 62.66 | 62.77 | N/A |
| 16W | 64.27 | 62.56 | 62.67 | N/A |
| 17W | 64.17 | 62.46 | 62.57 | N/A |
| 18W | 64.07 | 62.36 | 62.47 | 62.15 |
| 19W | 63.97 | 62.26 | 62.37 | 62.04 |
| 20W | 63.87 | 62.16 | 62.27 | 61.95 |
| 21W | 63.77 | 62.06 | 62.17 | 61.19 |
| 22W | 63.67 | 61.96 | 62.07 | 61.10 |
| 23W | 63.57 | 61.86 | 61.97 | N/A |
| 24W | 63.47 | 61.76 | 61.87 | 60.89 |
| 25W | 63.37 | 61.66 | 61.77 | 60.80 |
| 26W | 63.27 | 61.56 | 61.67 | 60.70 |
| 27W | 63.17 | 61.46 | 61.57 | 60.60 |
| 28W | 63.07 | 61.36 | 61.47 | 61.84 |
| 29W | 62.97 | 61.26 | 61.37 | 63.08 |
| 30W | 62.87 | 61.16 | 61.27 | 64.31 |
| 31W | 62.80 | 61.09 | 61.20 | 60.88 |

NOTE:
FOR LOCATIONS OF PT.1, PT.2 & PT. 3, SEE
DRAWING NO. FST-200.


- NOTES:
- FOR STRUCTURAL NOTES, SEE DRAWING NOS. FST-001, FST-002 AND FST-003.
 - FOR TYPICAL PLATFORM PLANS, SEE DRAWING NOS. FST-208 & FST-209.
 - FOR LOCATIONS OF PT. 1, PT. 2 AND PT. 3, SEE DRAWING NO. FST-200.
 - THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR FALL PROTECTION WHICH SHALL INCLUDE ROOF MOUNTING DETAILS AS PER MANUFACTURER.

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED
QUANTITIES OF WORK, SHOWN ON THESE
SHEETS IS BASED ON LIMITED
INVESTIGATIONS BY THE STATE AND IS
IN NO WAY WARRANTED TO INDICATE
THE CONDITIONS OF ACTUAL QUANTITIES
OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

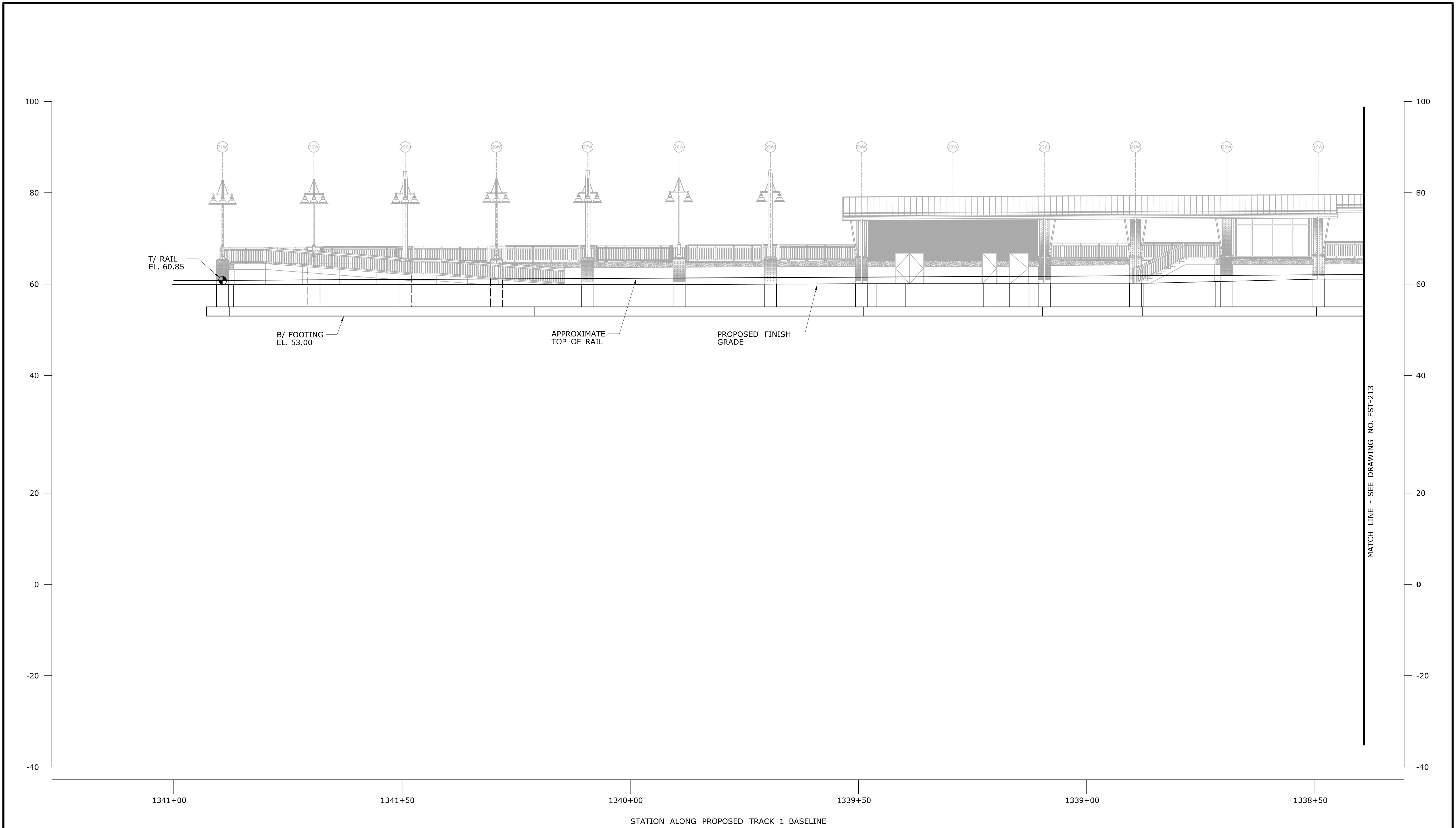

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_211.dgn

SIGNATURE/
BLOCK:

TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**PLATFORM SECTIONS
& DETAILS 12**
PROJECT NO.
170-3155
DRAWING NO.
FST-211
SHEET NO.
01.12.032



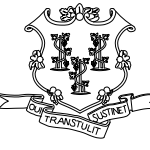
WEST PLATFORM PROFILE - NORTH HALF LOOKING EAST
SCALE: 1" = 10'

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.


Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED



STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_212.dgn



SIGNATURE/
BLOCK:



TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

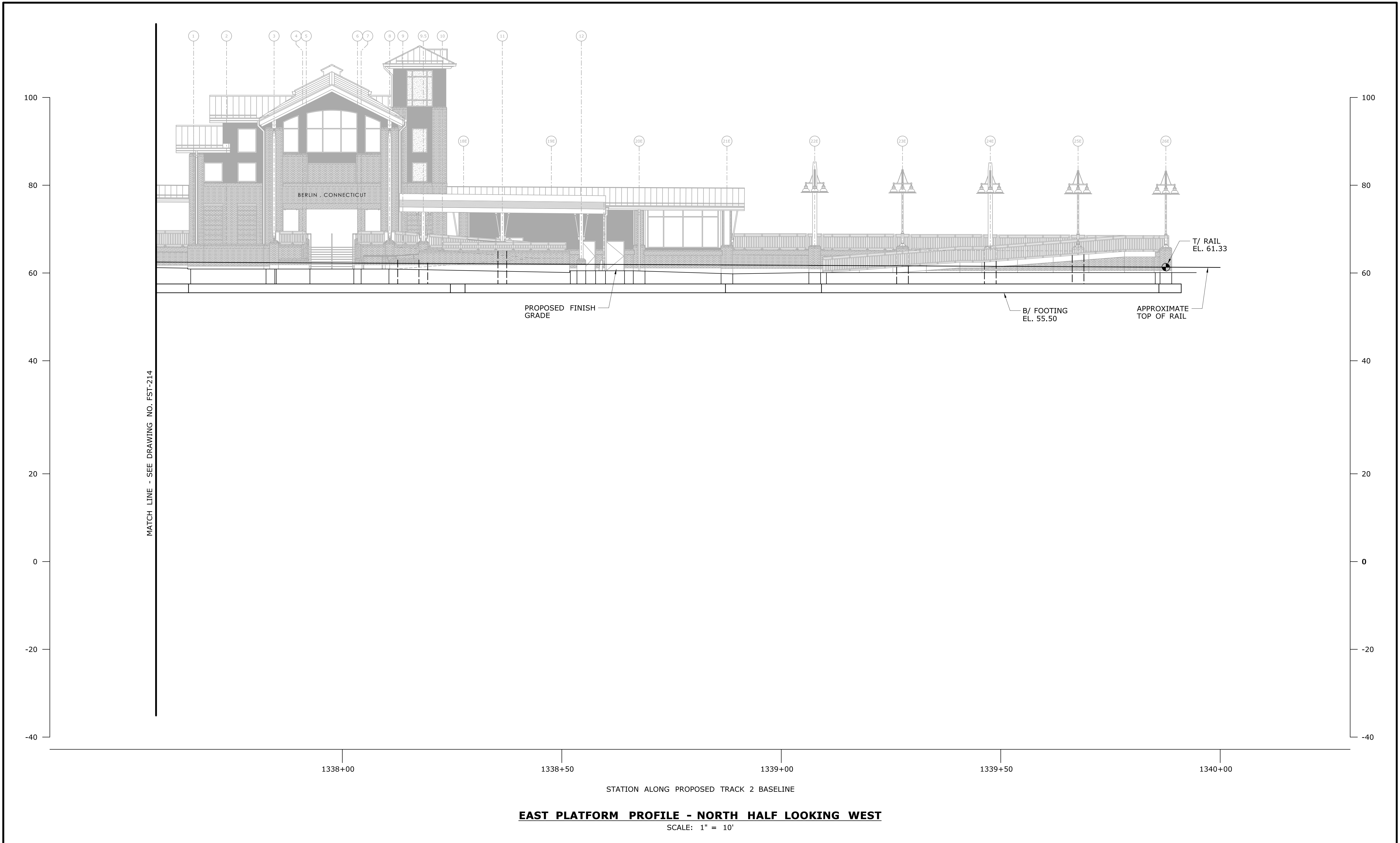
TOWN:
BERLIN

DRAWING TITLE:
**WEST PLATFORM
PROFILE - NORTH HALF**

PROJECT NO.
170-3155

DRAWING NO.
FST-212

SHEET NO.
01.12.033




| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.


Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED



STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_215.dgn



SIGNATURE/
BLOCK:



TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:

**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:

BERLIN

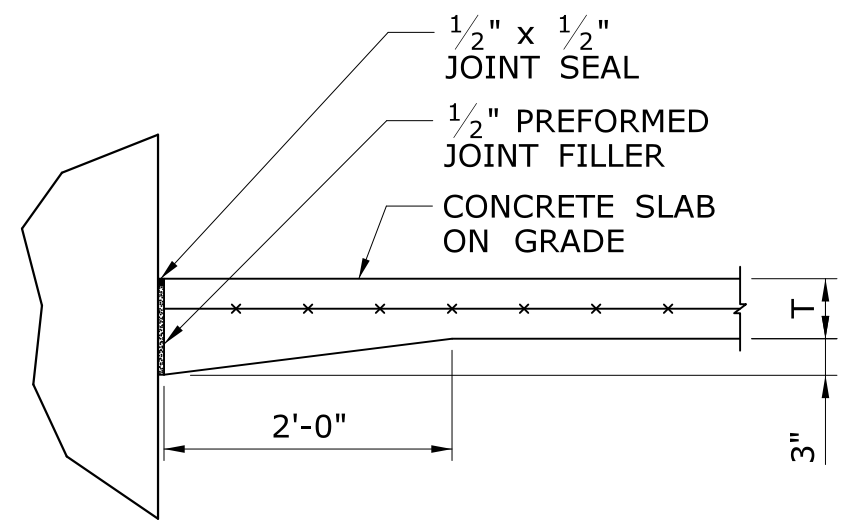
DRAWING TITLE:

**EAST PLATFORM
PROFILE - NORTH HALF**

PROJECT NO.
170-3155

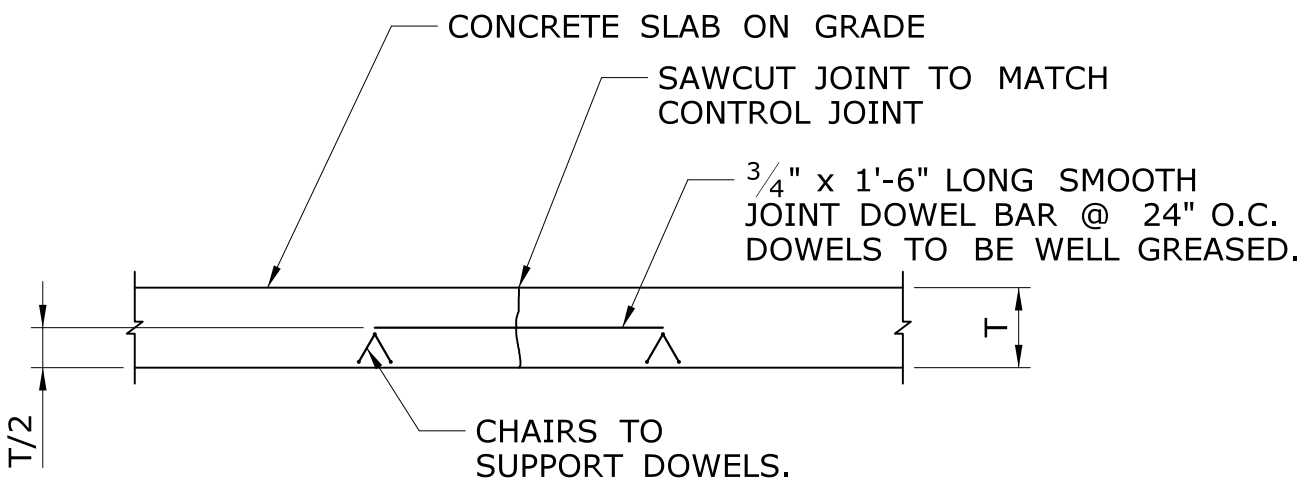
DRAWING NO.
FST-215

SHEET NO.
01.12.036



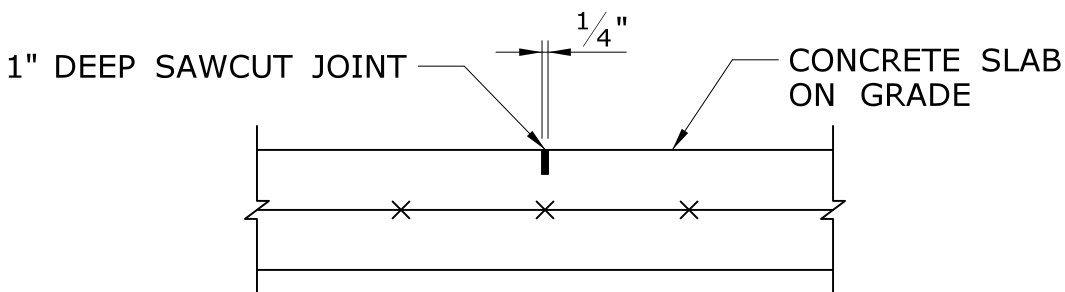
TYPICAL SLAB ON GRADE PERIMETER DETAIL

SCALE: 3/4" = 1'-0"



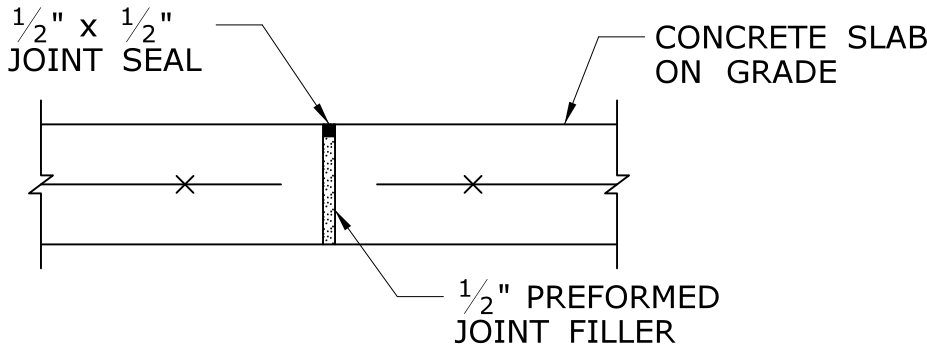
TYPICAL SLAB CONSTRUCTION JOINT

SCALE: 1" = 1'-0"



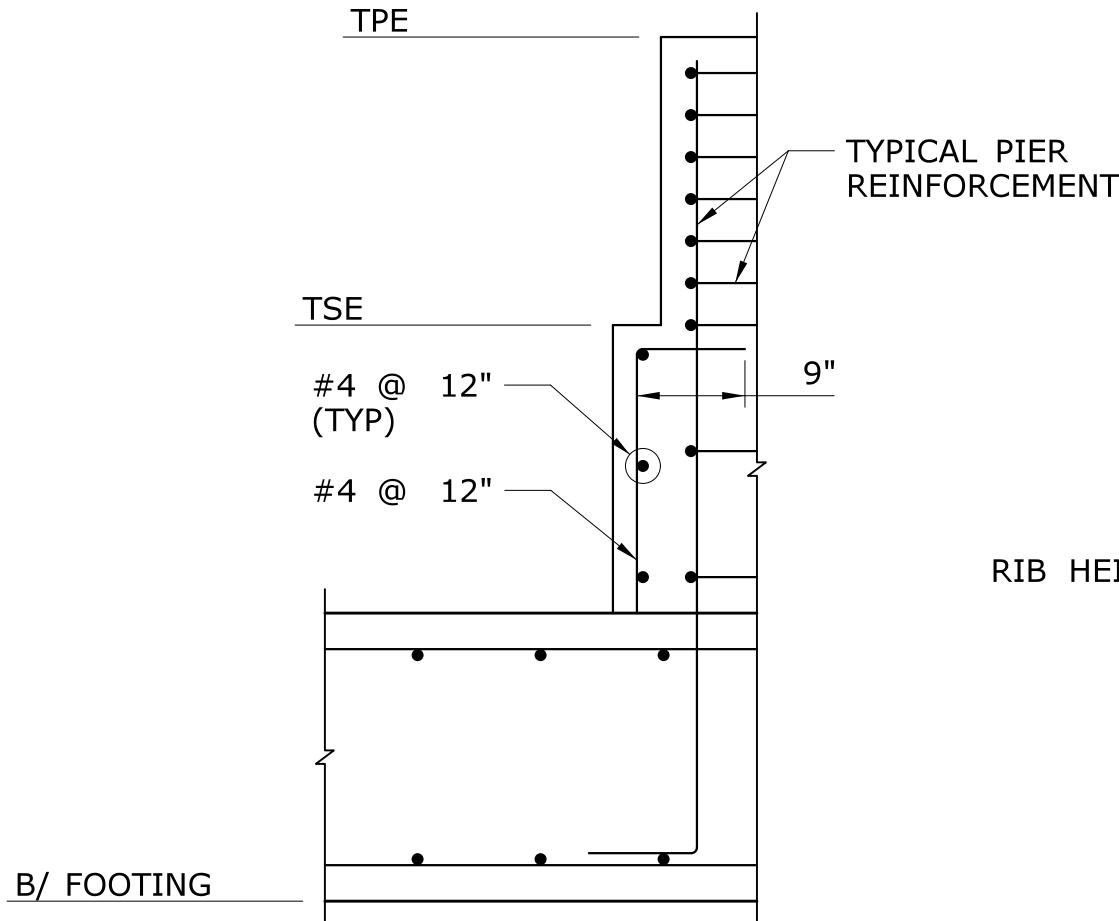
TYPICAL SLAB/MICROSILICA CONTROL JOINT DETAIL

SCALE: 3/4" = 1'-0"



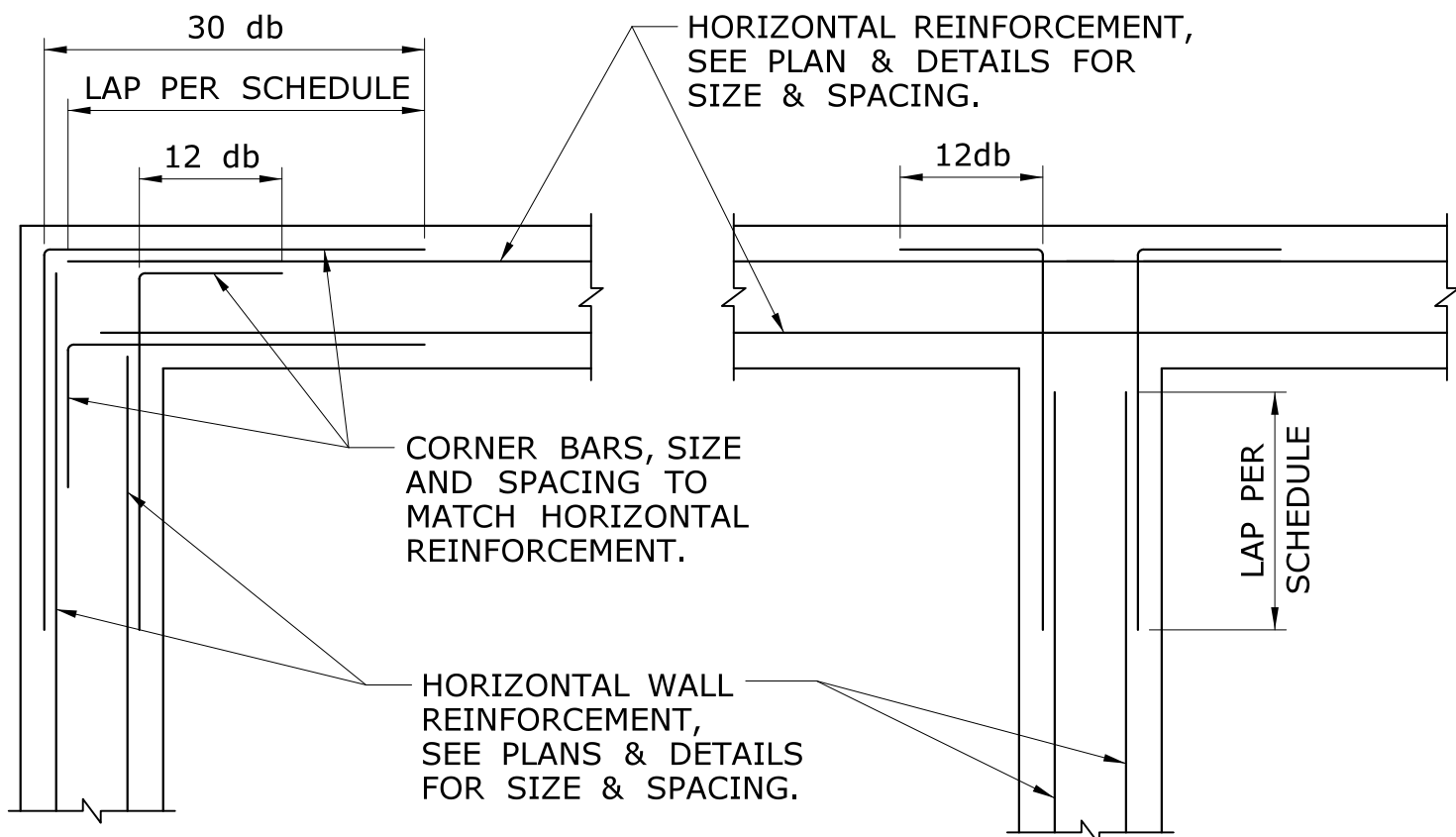
TYPICAL SLAB EXPANSION JOINT DETAIL

SCALE: 1 1/2" = 1'-0"



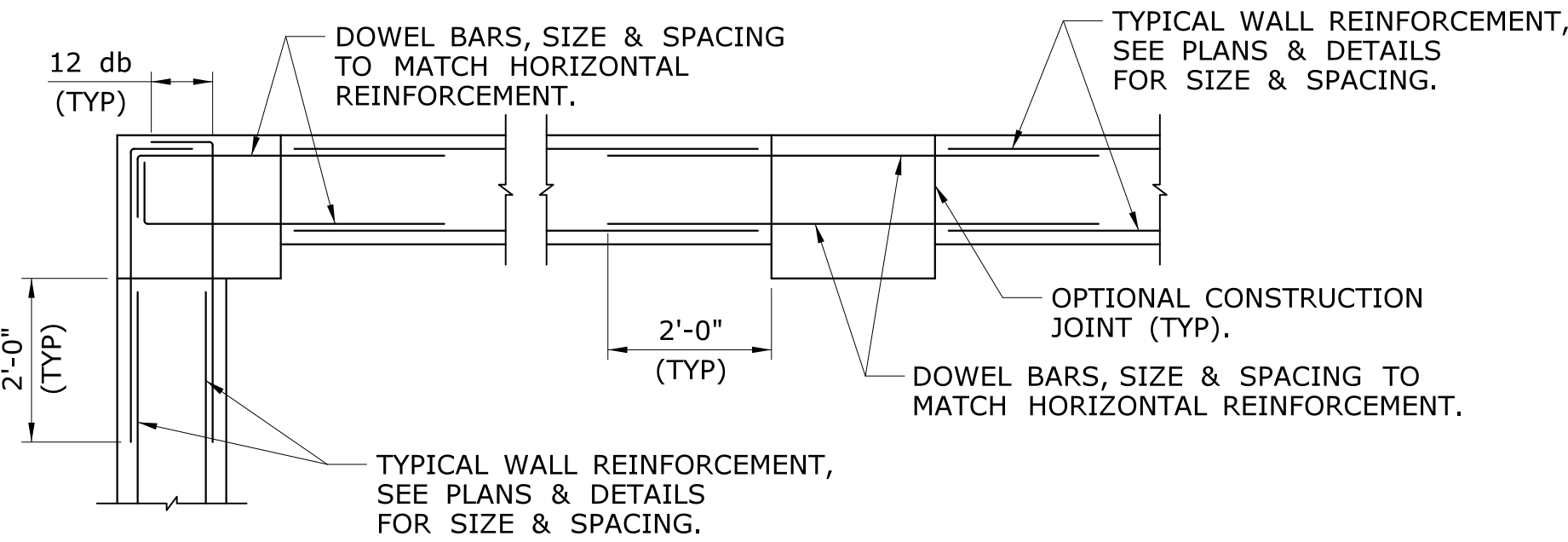
TYPICAL REINFORCEMENT AT PIER BRICK SHELF DETAIL

SCALE: 3/4" = 1'-0"



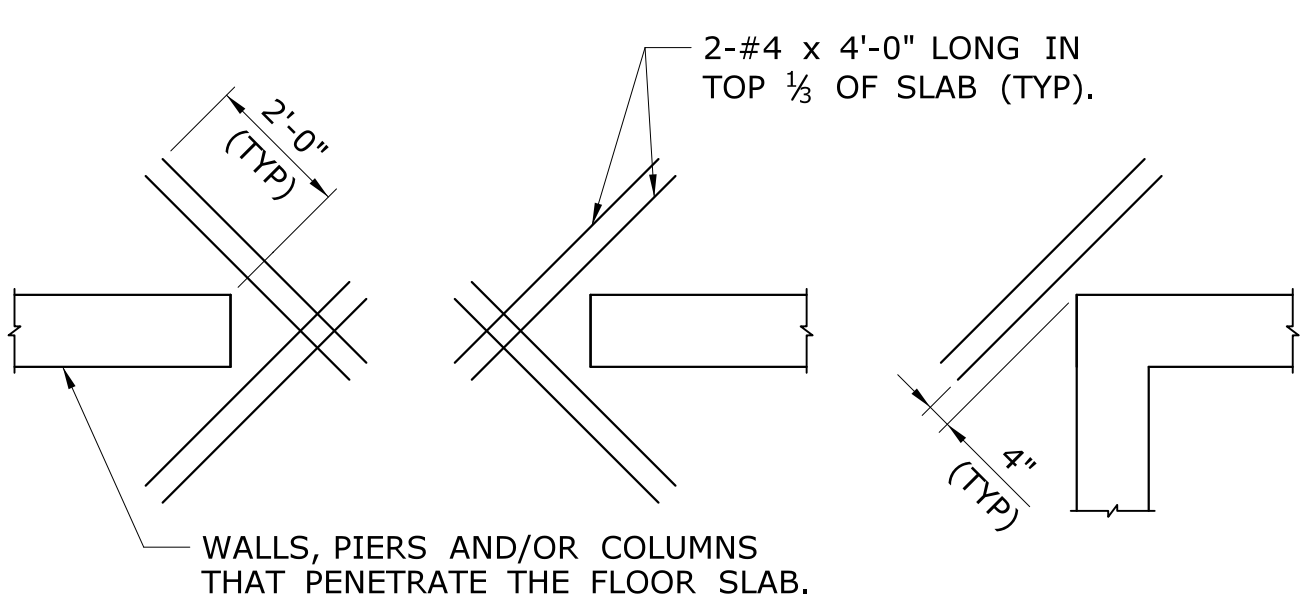
TYPICAL WALL INTERSECTION DETAILS

SCALE: 3/4" = 1'-0"



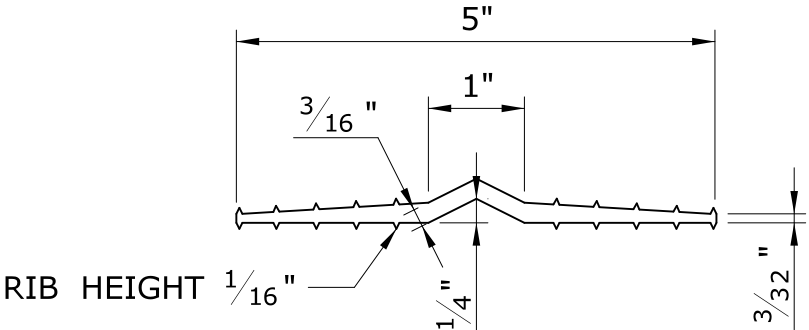
TYPICAL WALL & PIER INTERSECTION DETAILS

SCALE: 1/2" = 1'-0"



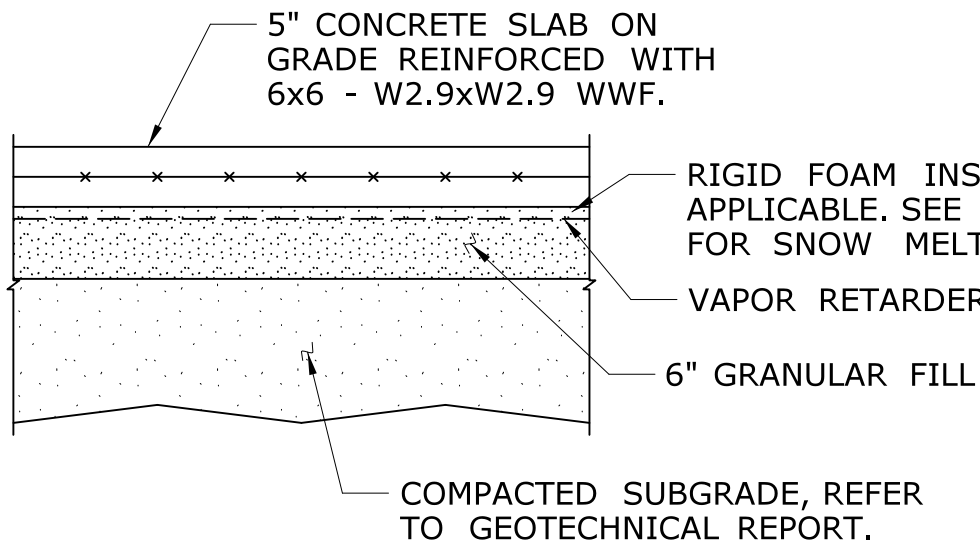
TYPICAL RE-ENTRANT DETAIL

SCALE: 3/8" = 1'-0"



5" WATERSTOP DETAIL

SCALE: 6" = 1'-0"

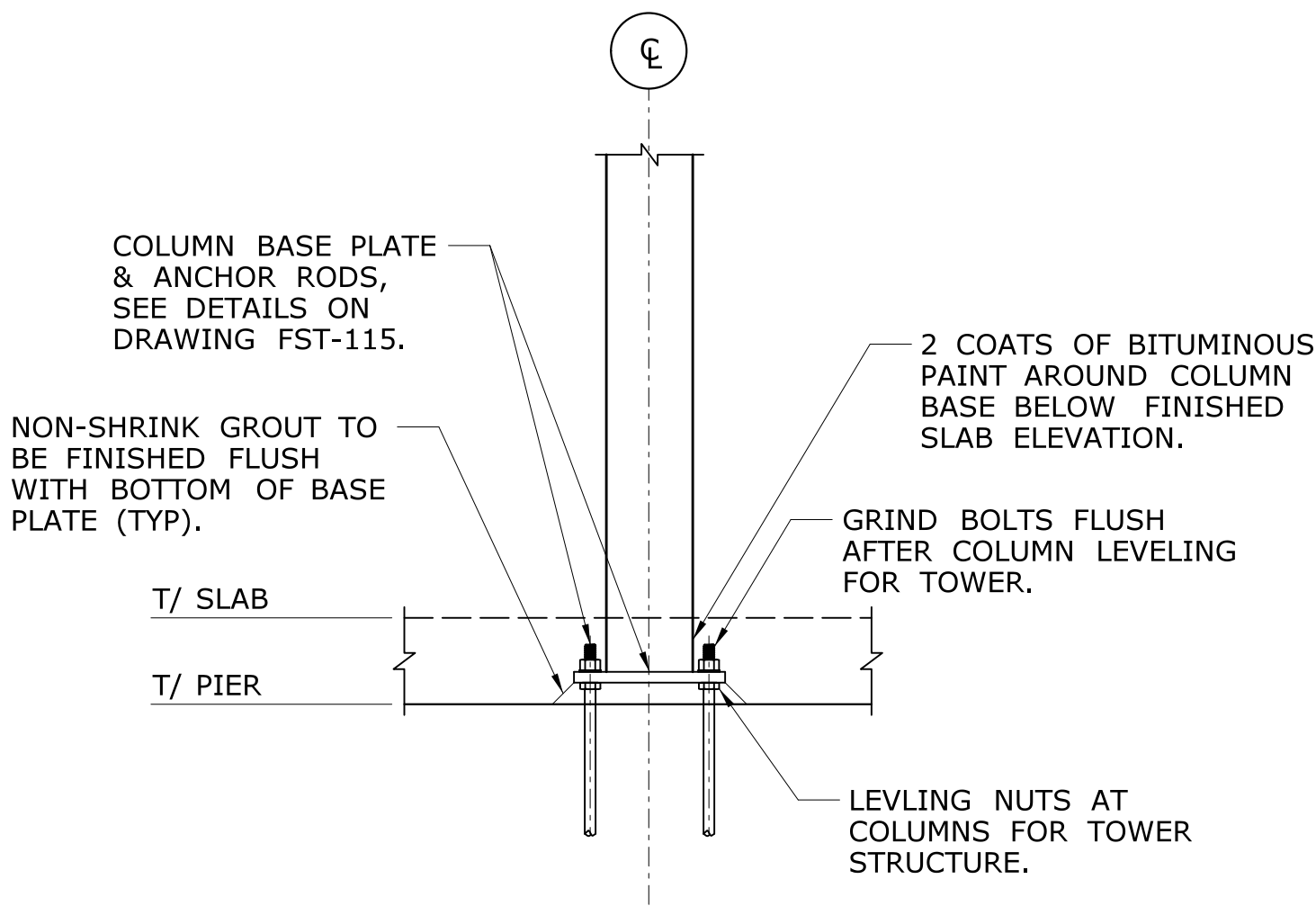


NOTE:

REFER TO GENERAL NOTE, GEOTECHNICAL REPORT AND SPECIFICATIONS FOR ADDITIONAL NOTES AND REQUIREMENT.

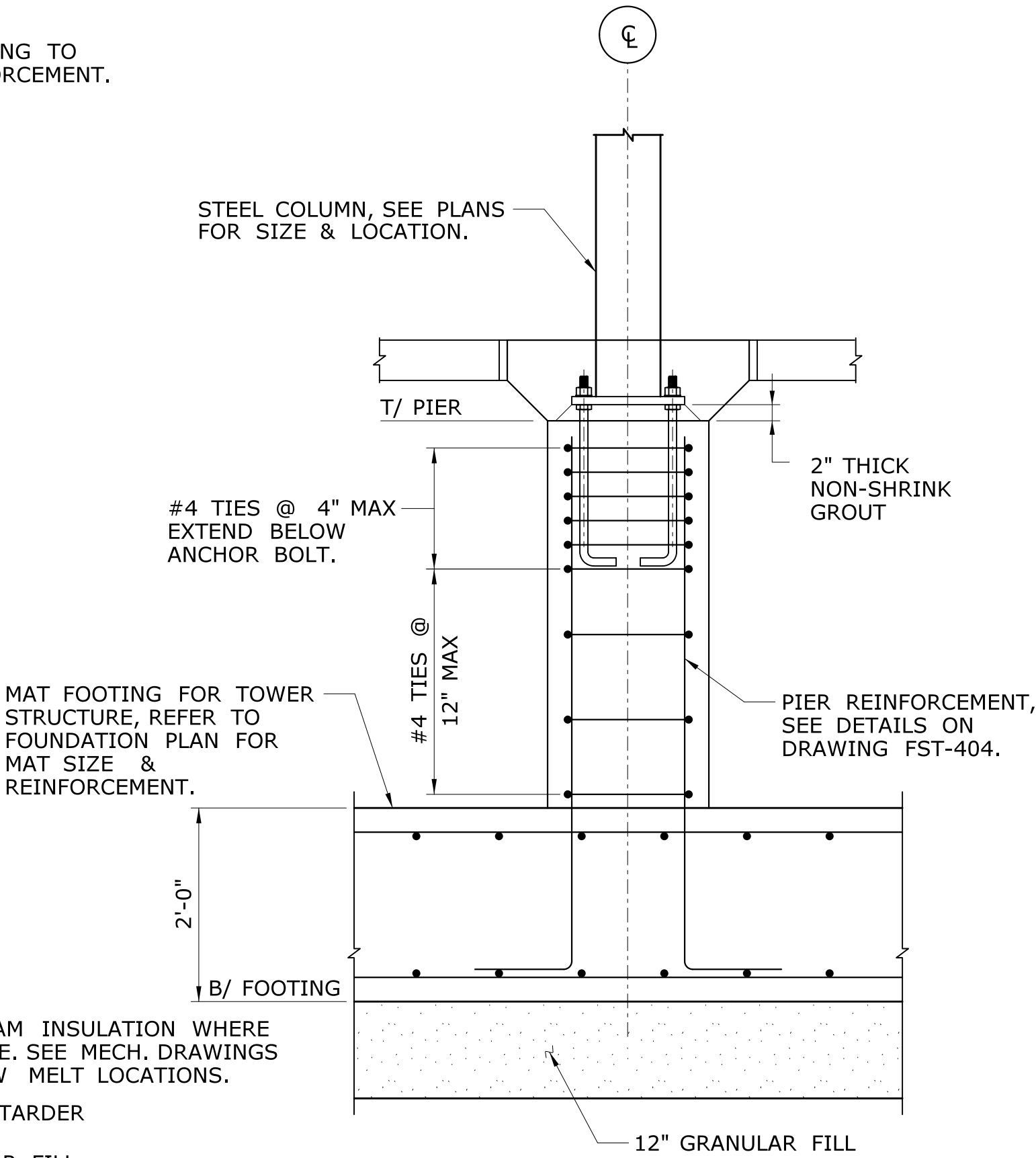
TYPICAL SLAB ON GRADE DETAIL

SCALE: 3/4" = 1'-0"



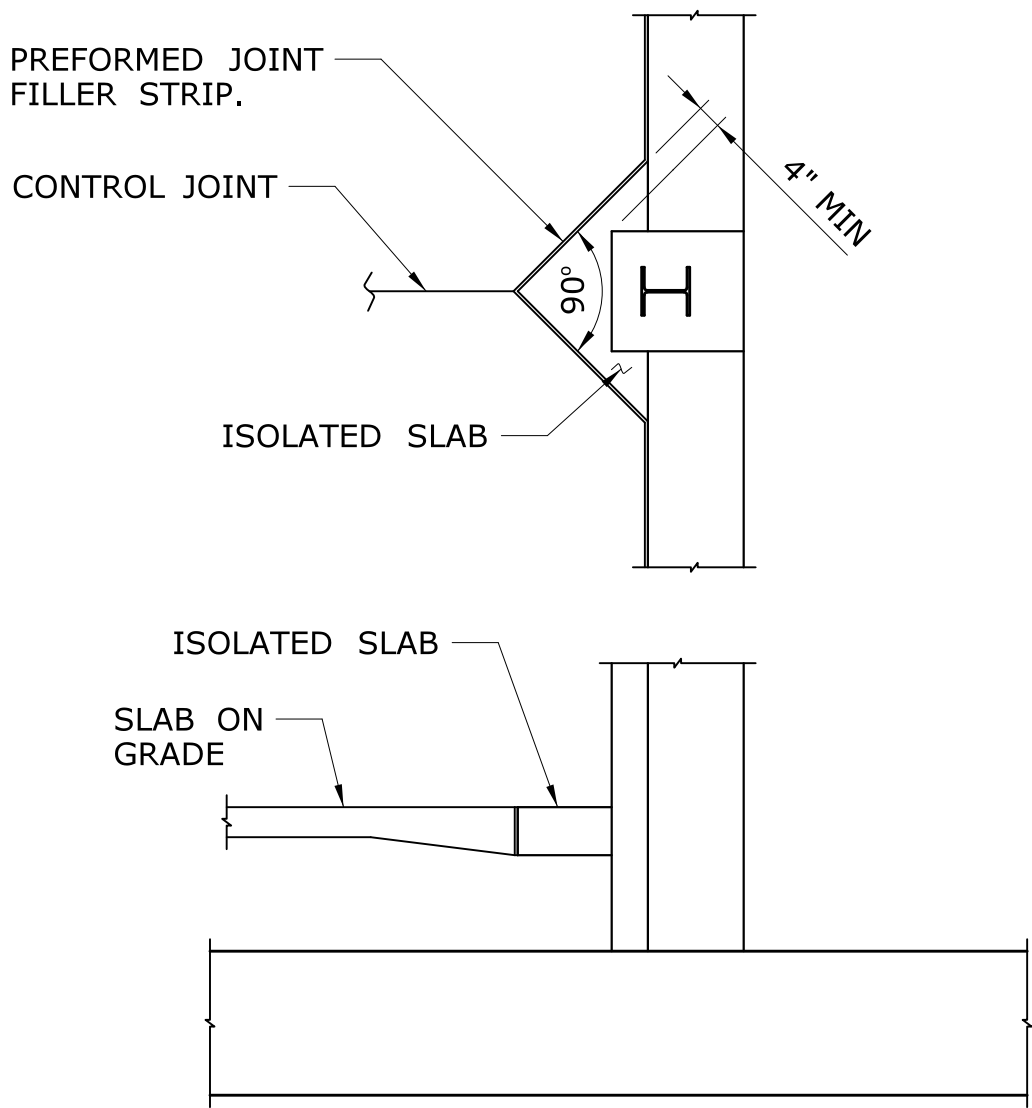
TYPICAL COLUMN BASE DETAIL

SCALE: 3/4" = 1'-0"



TYPICAL PIER DETAIL

SCALE: 3/4" = 1'-0"



TYPICAL ISOLATION JOINT AT PIER DETAIL

SCALE: 3/8" = 1'-0"

NOTE:

FOR T/SLAB EL, TWE, TSE AND B/FOOTING EL, SEE DRAWING NOS FST-107 AND FST-108.

REINFORCING STEEL DEVELOPMENT LENGTH SCHEDULE 4000PSI

| BAR SIZE | TOP BARS | OTHER BARS |
|----------|-----------------------|----------------------|
| #3 | 50 x BAR DIA = 1'-7" | 38 x BAR DIA = 1'-3" |
| #4 | 50 x BAR DIA = 2'-1" | 38 x BAR DIA = 1'-7" |
| #5 | 50 x BAR DIA = 2'-8" | 38 x BAR DIA = 2'-0" |
| #6 | 50 x BAR DIA = 3'-2" | 38 x BAR DIA = 2'-5" |
| #7 | 63 x BAR DIA = 4'-8" | 48 x BAR DIA = 3'-6" |
| #8 | 63 x BAR DIA = 5'-3" | 48 x BAR DIA = 4'-0" |
| #9 | 63 x BAR DIA = 5'-11" | 48 x BAR DIA = 4'-6" |
| #10 | 63 x BAR DIA = 6'-7" | 48 x BAR DIA = 5'-0" |

NOTES:

- TOP BARS INCLUDE FOOTING REINFORCEMENT WITH MORE THAN 12" OF CONCRETE CAST BELOW THE REBAR AND ALL HORIZONTAL WALL REINFORCEMENT WITHOUT EXCEPTION. REFER TO ACI 318-02 FOR MORE INFORMATION. OTHER BARS INCLUDE FOOTING REINFORCEMENT WITH LESS THAN 12" OF CONCRETE CAST BELOW THE REBAR.
- CONCRETE IS NORMAL WEIGHT AND REINFORMENT IS FY=60 KSI.
- PROVIDE APPROPRIATE FACTORS FOR LIGHTWEIGHT CONCRETE AND EPOXY COATED REINFORCEMENT PER ACI.
- MULTIPLY TABLE VALUES BY 1.3 FOR LAP SPLICE LENGTHS.
- MINIMUM CLEAR SPACING OF BARS SHALL NOT BE LESS THAN 3 BAR DIAMETERS. CLEAR COVER SHALL NOT BE LESS THE MOST RESTRICTIVE REQUIREMENTS OF 1 BAR DIAMETER, THE MINIMUM CLEAR COVER DIMENSION LISTED IN THE CONTRACT DOCUMENTS AND THE ACI CODE. REPORT ANY DISCREPANCIES TO THE ENGINEER OF RECORD.

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
J POPOLI
CHECKED BY:
H BUI
SCALE AS NOTED

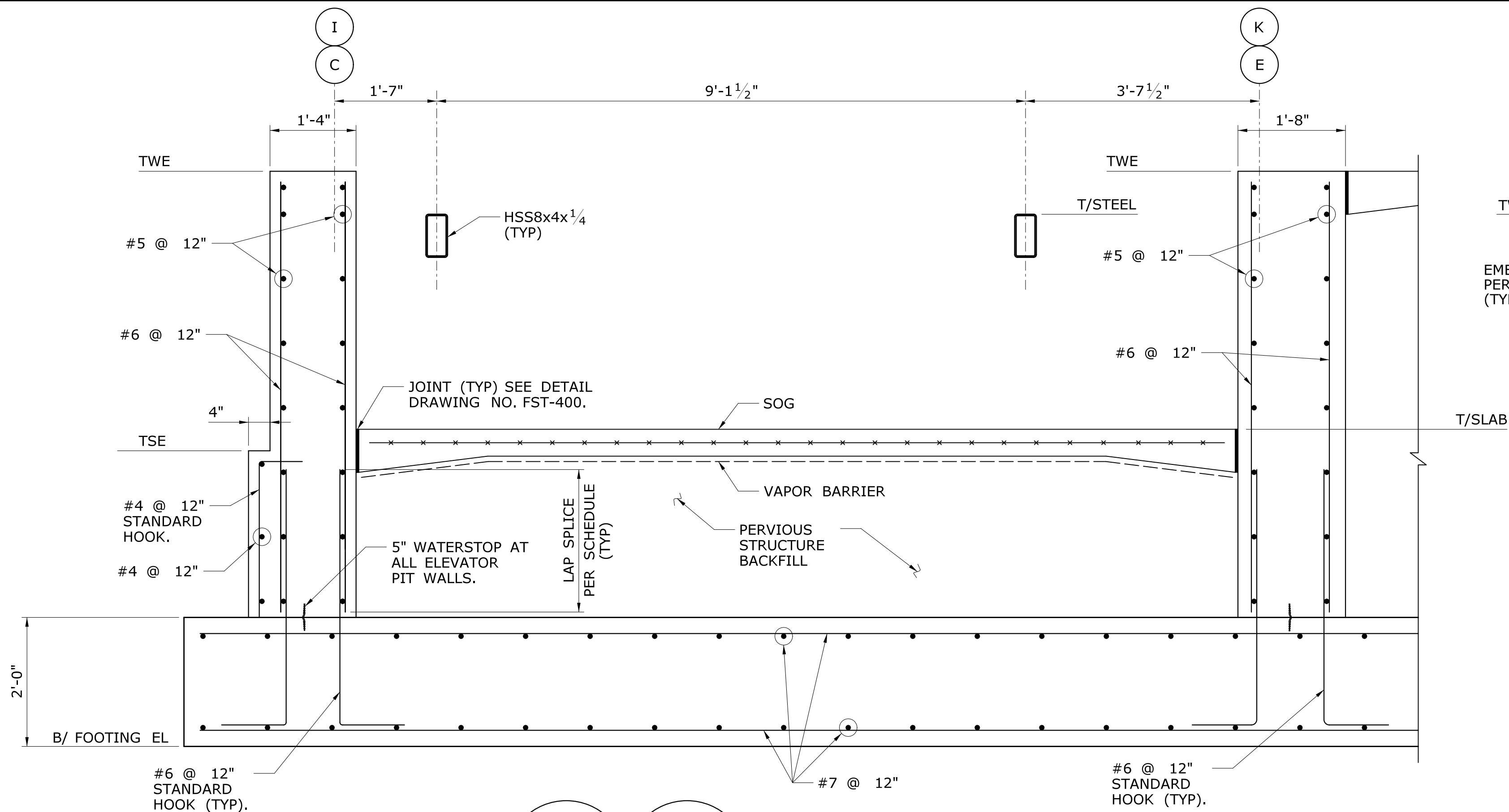
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_216.dgn

SIGNATURE/BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**TOWER FOUNDATION
DETAILS 1**

PROJECT NO.
170-3155
DRAWING NO.
FST-216
SHEET NO.
01.12.037



1
FST-107

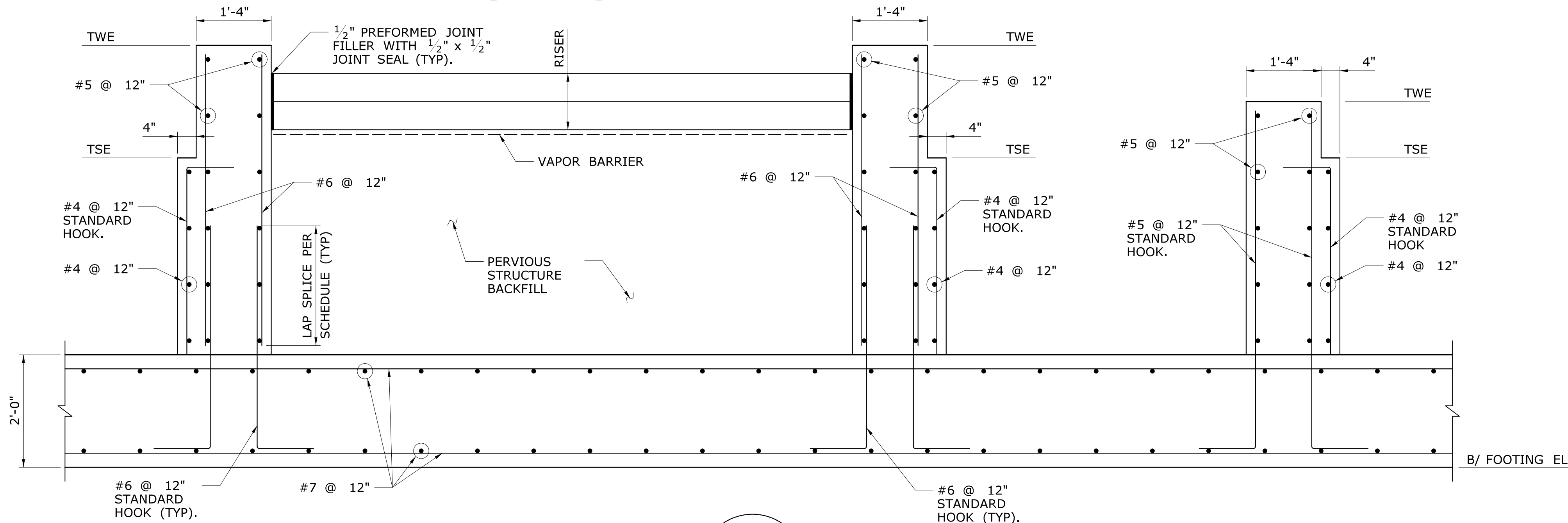
1
FST-108

SECTION THRU ELEVATOR

SCALE: $\frac{3}{4}" = 1'-0"$

NOTE:

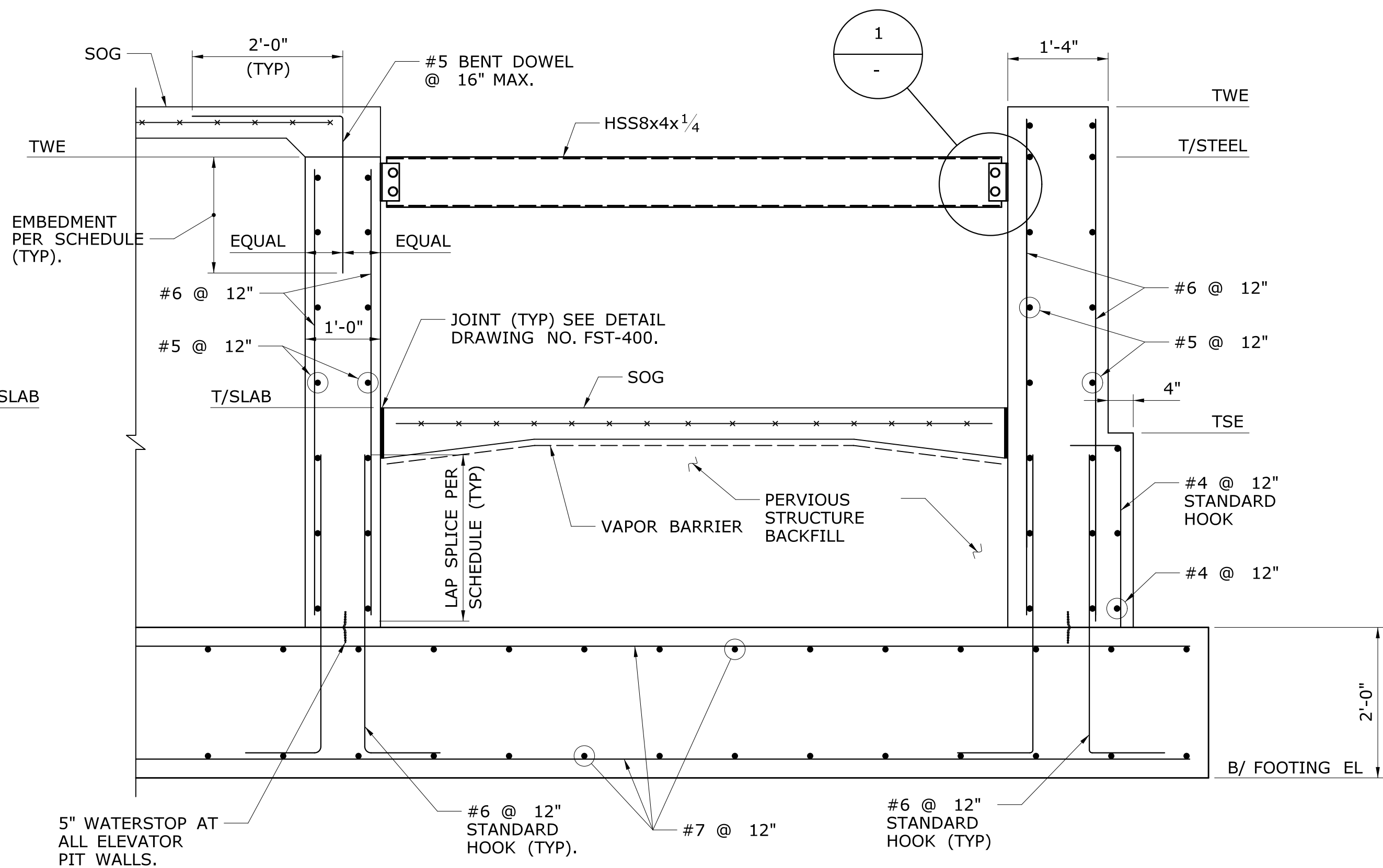
FOR T/STEEL EL, SEE
DRAWING NO. FST-107.



3
FST-108

SECTION

SCALE: $\frac{3}{4}" = 1'-0"$



2
FST-107

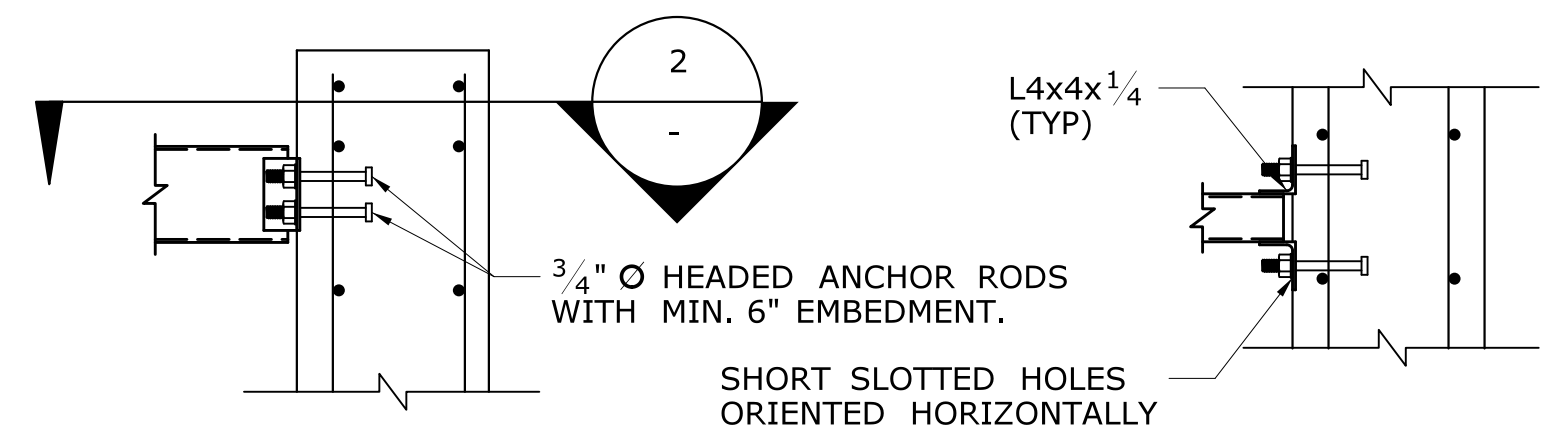
2
FST-108

SECTION THRU ELEVATOR

SCALE: $\frac{3}{4}" = 1'-0"$

NOTES:

1. SEE TOWER FOUNDATION PLANS ON DRAWING NOS. FST-107 AND FST-108.
2. FOR T/SLAB EL, TWE, TSE AND B/FOOTING EL, SEE DRAWING NOS. FST-107 AND FST-108.
3. FOR SOG DETAILS, SEE DRAWING NO. FST-400.
4. FOR EMBEDMENT LENGTH AND LAP SPLICE LENGTH, SEE DRAWING NO. FST-400.
5. 12" THICK LAYER OF GRANULAR FILL BELOW FOOTING NOT SHOWN FOR CLARITY.





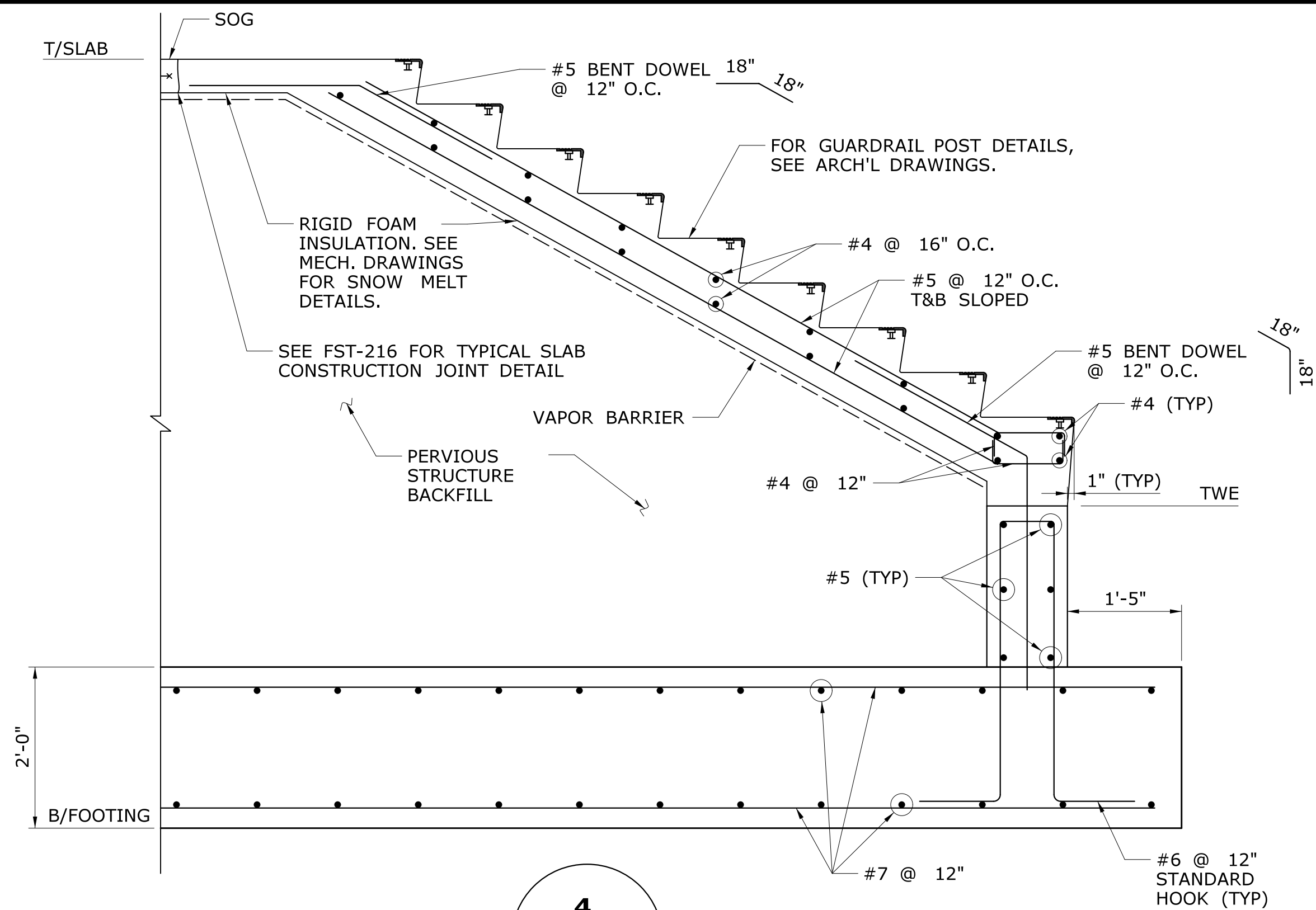
1
DETAIL

SCALE: $\frac{3}{4}" = 1'-0"$

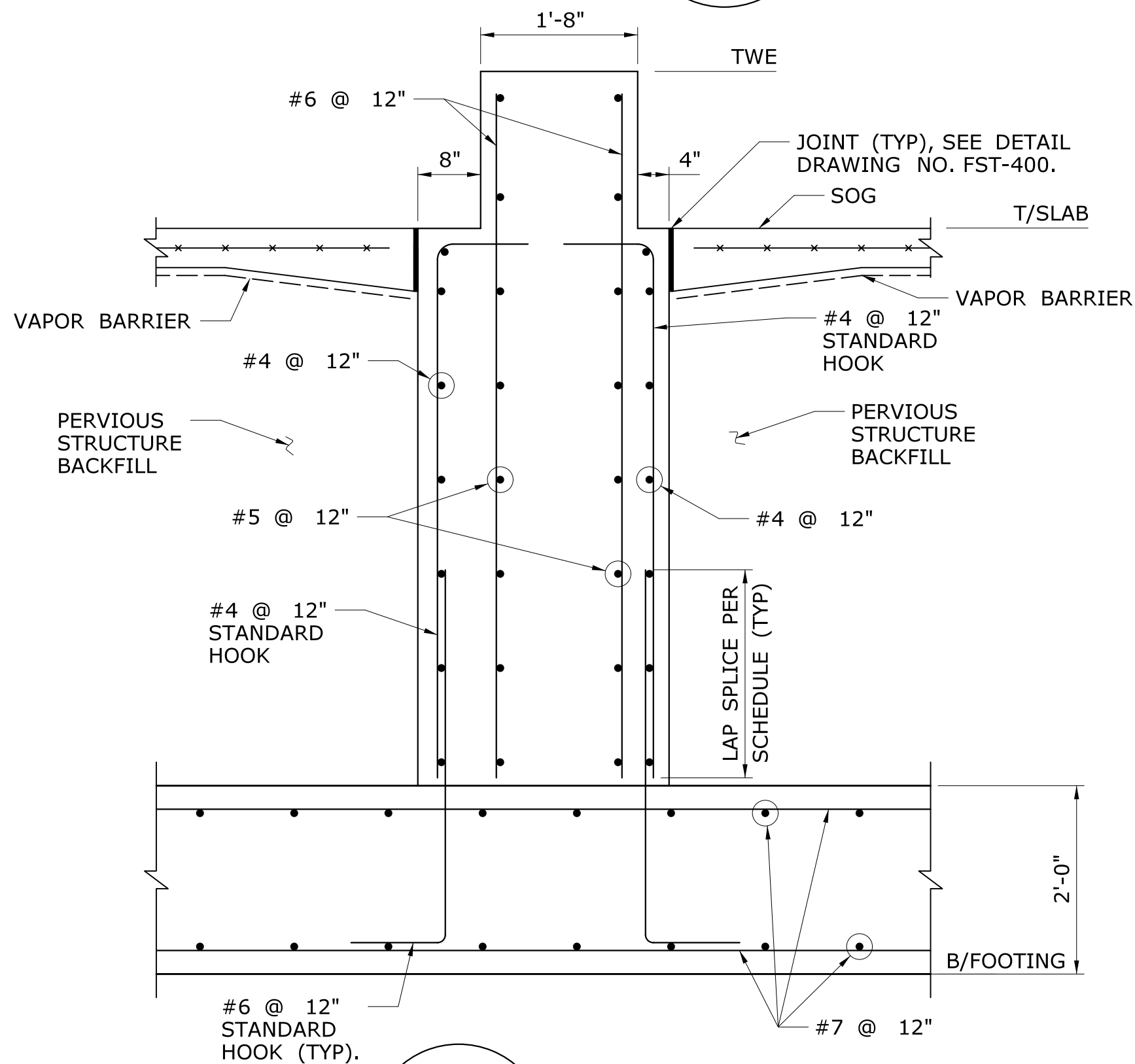
2
SECTION

SCALE: $\frac{3}{4}" = 1'-0"$

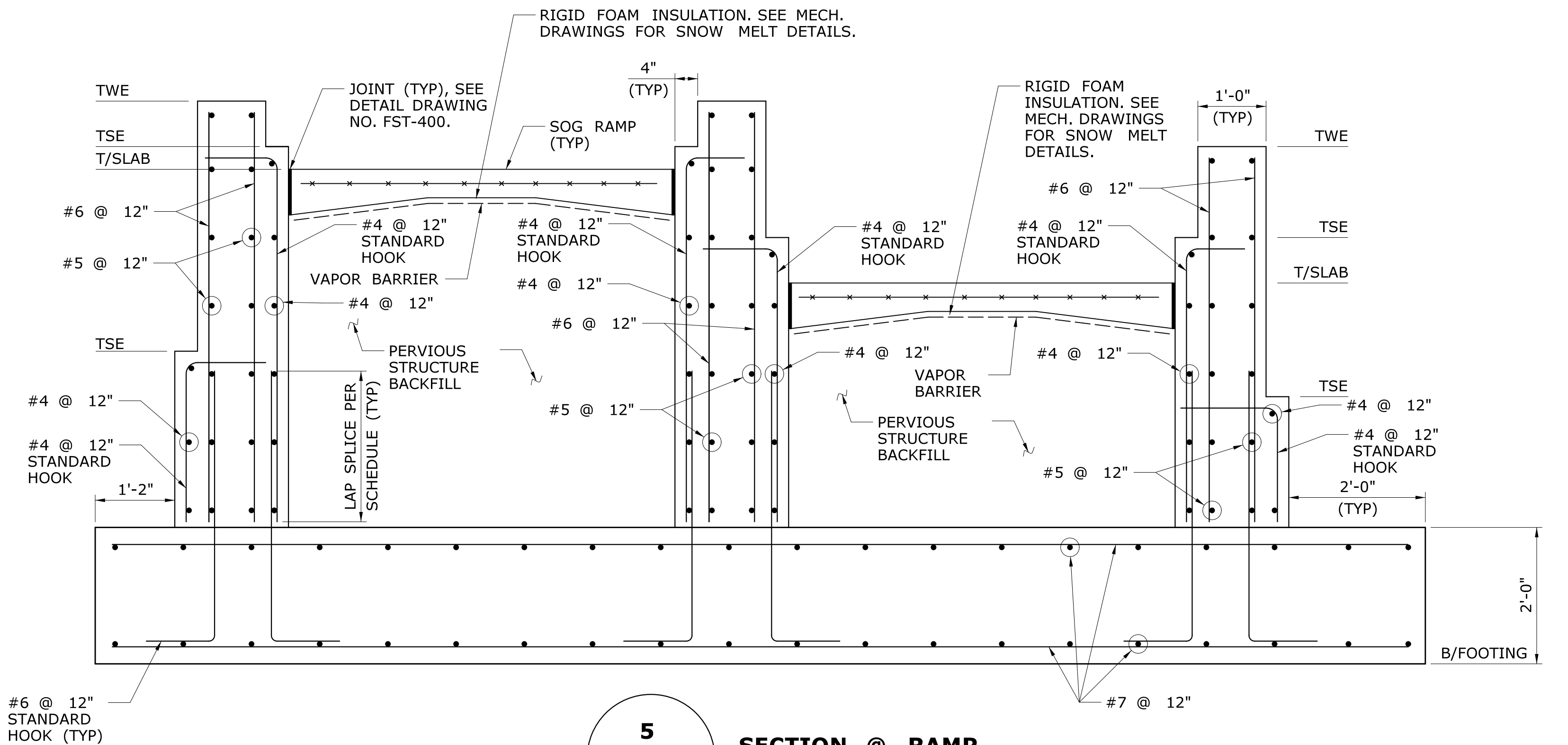
| | | | | | | | | | | | | | | | | | |
|------|------|----------------------|-----------|---|--|--------------------------------------|--|---|--|--|--|---|--|----------------------------|--|--------------------------------|--|
| | | | | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | | DESIGNER/DRAFTER: J POPOLI | |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION | |  TranSystems 530 PRESTON AVENUE MERIDEN, CT 06450 | | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | | TOWN: BERLIN | | PROJECT NO. 170-3155 | |
| | | | | CHECKED BY: H BUI | | DRAWING NO. FST-217 | | | | | | | | | | | |
| | | | | Plotted Date: 2/26/2014 | | SCALE AS NOTED | | Filename: ...\\FA_CGR_CPS_0170-2296_007...04_FST...217.dgn | | | | | | | | SHEET NO. 01.12.038 | |
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. | | | | | | | | | | | | | | |



4
SECTION
FST-108 SCALE: $\frac{3}{4}$ " = 1'-0"



6
SECTION
FST-108 SCALE: $\frac{3}{4}$ " = 1'-0"



5
SECTION @ RAMP
FST-108 SCALE: $\frac{3}{4}$ " = 1'-0"

| PIER # | INITIAL ELEVATION | FINAL ELEVATION | PIER # | INITIAL ELEVATION | FINAL ELEVATION |
|--------|-------------------|-----------------|--------|-------------------|-----------------|
| 1 | -0'-8" | +0'-0" | 7 | +0'-8" | +1'-4" |
| 1A | -0'-8" | +0'-0" | 8 | +0'-8" | +1'-4" |
| 2 | -0'-8" | +0'-0" | 9 | +0'-8" | +0'-8" |
| 2A | -0'-8" | +0'-0" | 10 | -0'-8" | +0'-0" |
| 3 | -0'-8" | +0'-0" | 11 | -0'-8" | +0'-0" |
| 3A | -0'-8" | +0'-0" | 11A | +0'-8" | +1'-4" |
| 4 | +0'-8" | +1'-4" | 12 | -0'-8" | +0'-0" |
| 5 | -0'-8" | +0'-0" | 13 | -0'-8" | +0'-0" |
| 6 | -0'-8" | +0'-0" | 14 | -0'-8" | +0'-0" |

NOTES:

- SEE TOWER FOUNDATION PLANS ON DRAWING NOS. FST-107 AND FST-108.
- FOR T/SLAB EL, TWE, TSE AND B/FOOTING EL, SEE DRAWING NOS. FST-107 AND FST-108.
- FOR SOG DETAILS, SEE DRAWING NO. FST-400.
- FOR EMBEDMENT LENGTH AND LAP SPLICE LENGTH, SEE DRAWING NO. FST-400.
- 12" THICK LAYER OF GRANULAR FILL BELOW FOOTING NOT SHOWN FOR CLARITY.
- THE INITIAL PIER ELEVATIONS GIVEN ON FST-107 & FST-108 ARE TO BE MADE PRIOR TO COLUMN INSTALLATION. FINAL PIER ELVATIONS GIVEN IN THE TABLE ON FST-218 ARE TO BE MADE CONCURRENTLY WITH SLAB ON GRADE CONSTRUCTION.

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
J POPOLI
CHECKED BY:
H BUI
SCALE AS NOTED

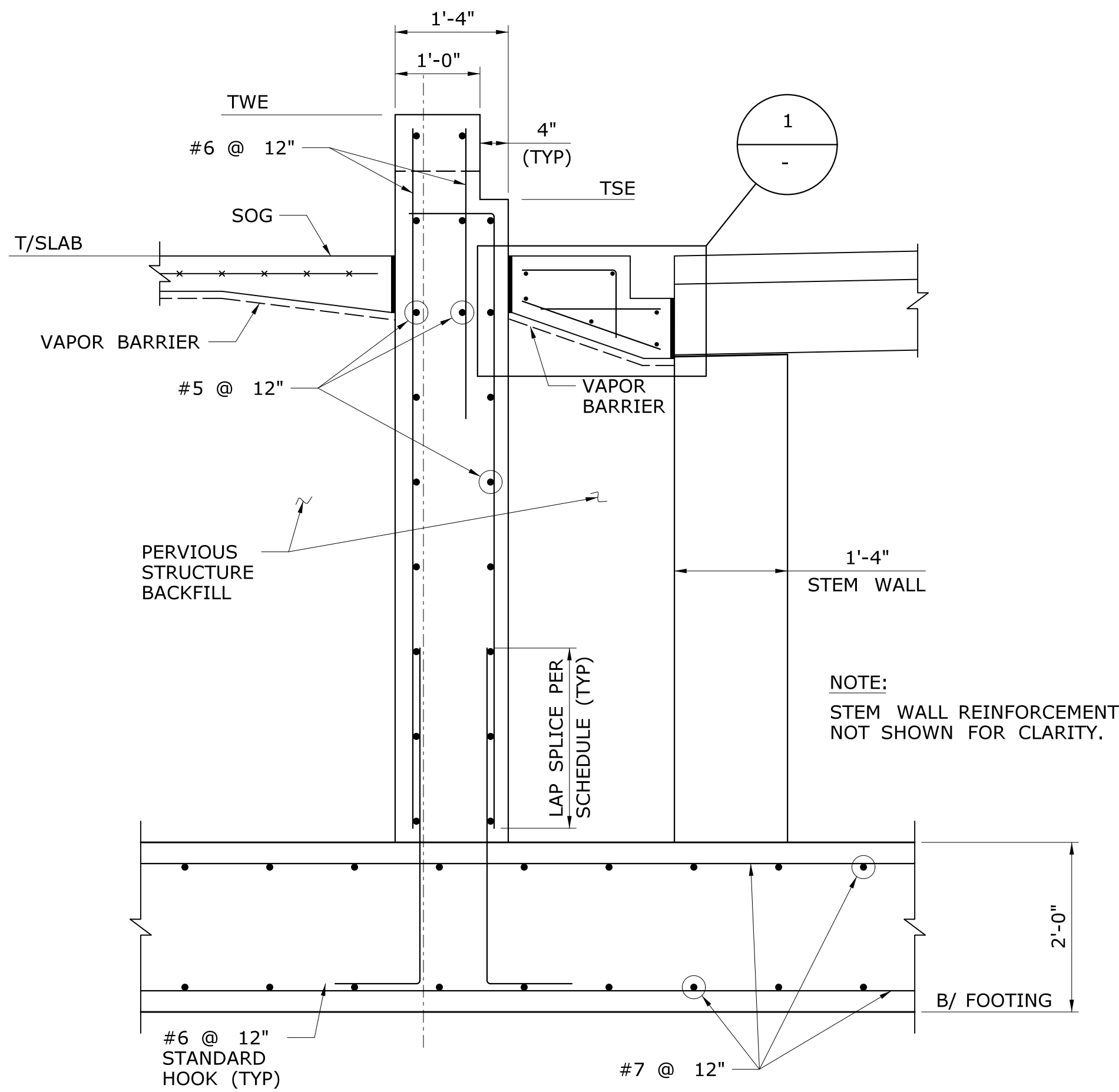
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...FA_CGR_CPS_0170-2296_007_04_FST_218.dgn

SIGNATURE/BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

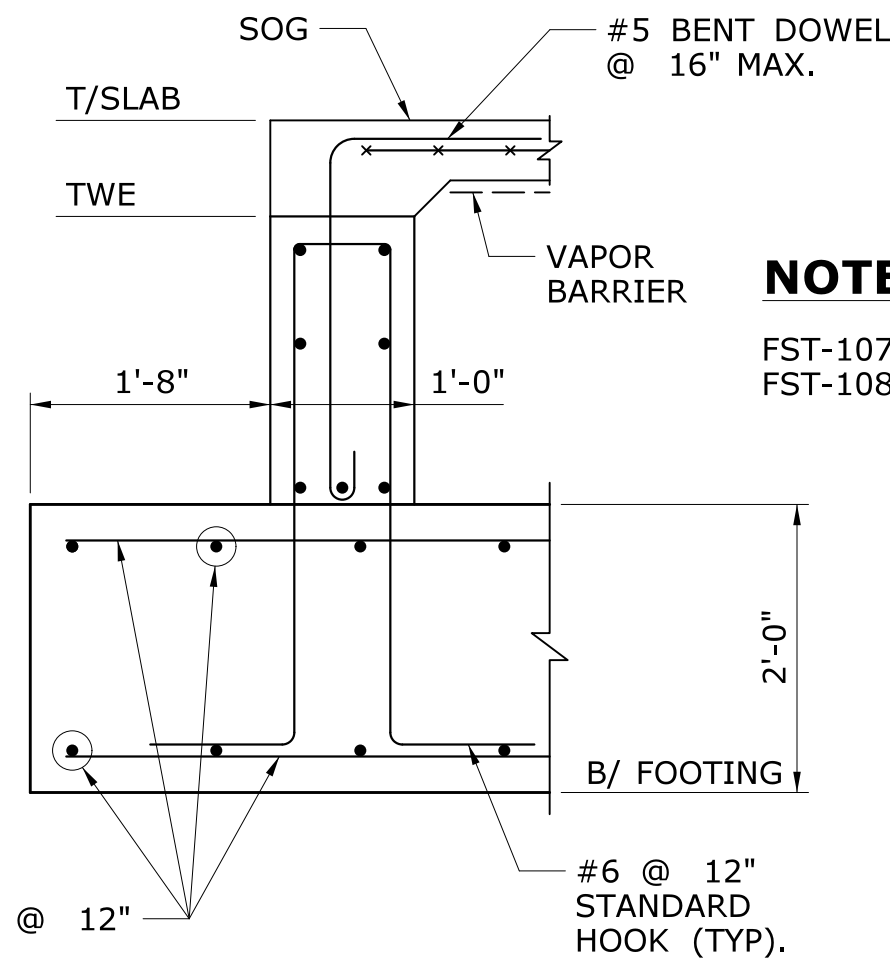
PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**TOWER FOUNDATION
DETAILS 3**

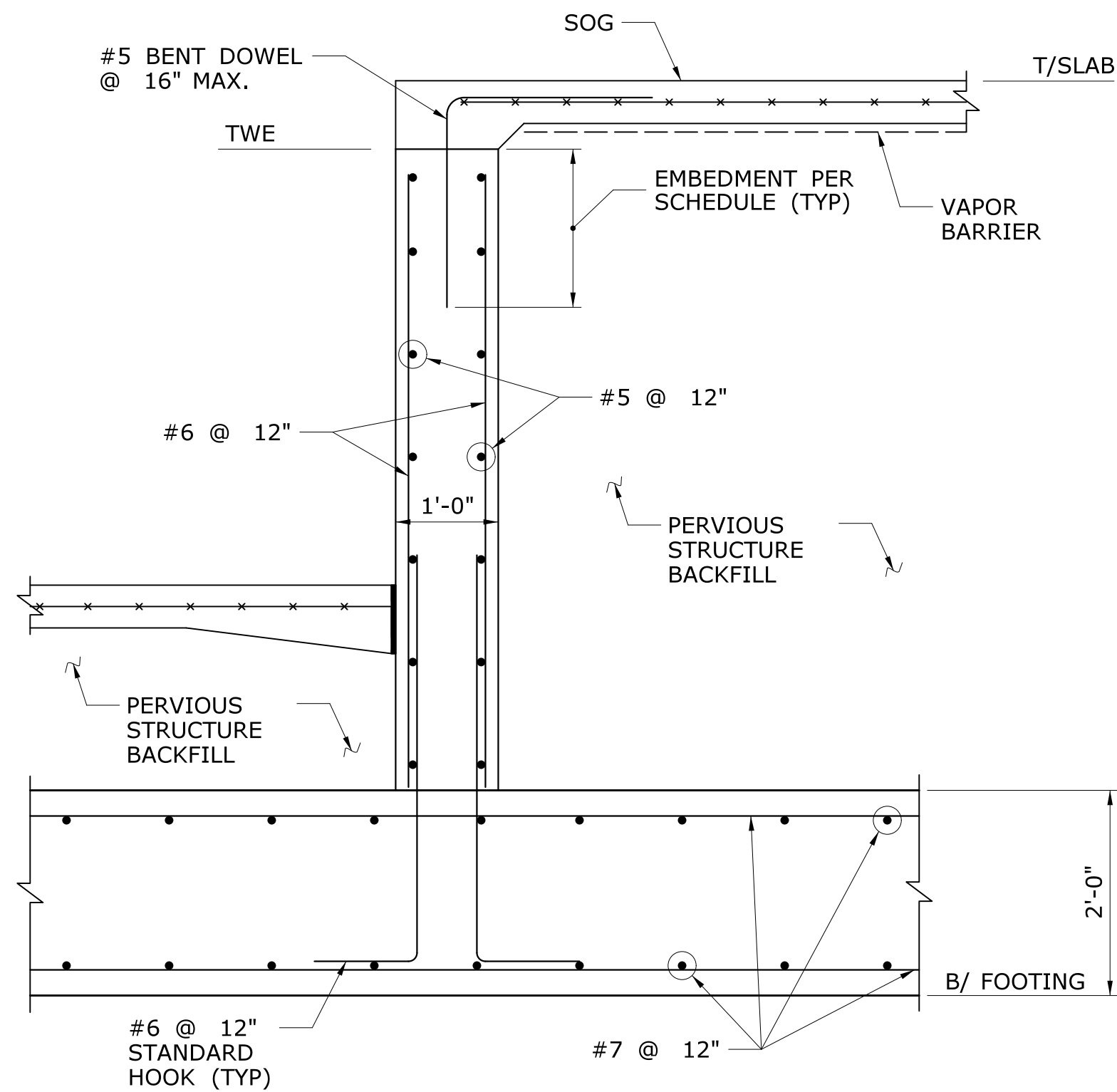
PROJECT NO.
170-3155
DRAWING NO.
FST-218
SHEET NO.
01.12.039



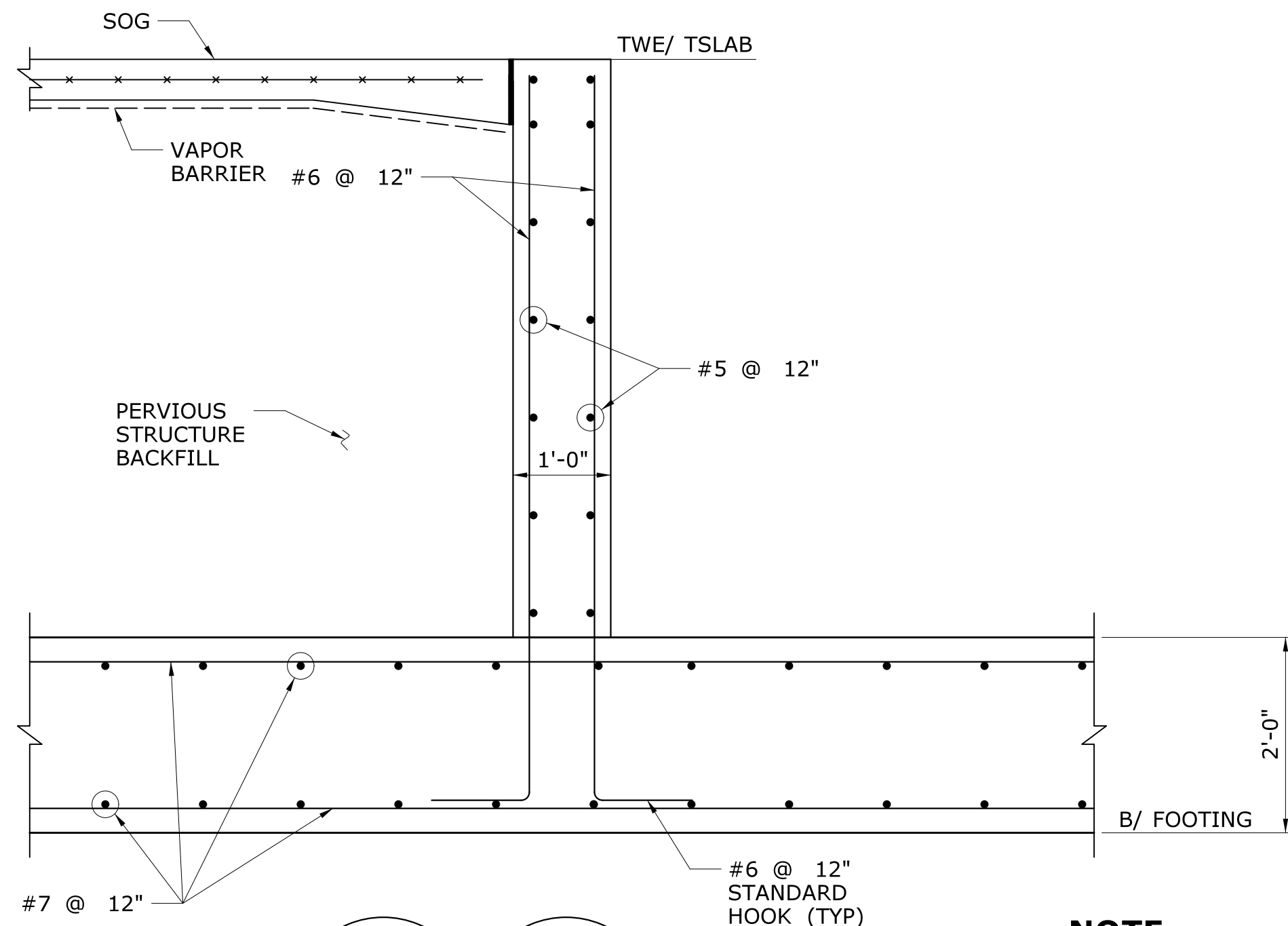
7 **7** **SECTION**
FST-107 **FST-108** SCALE: $\frac{3}{4}" = 1'-0"$



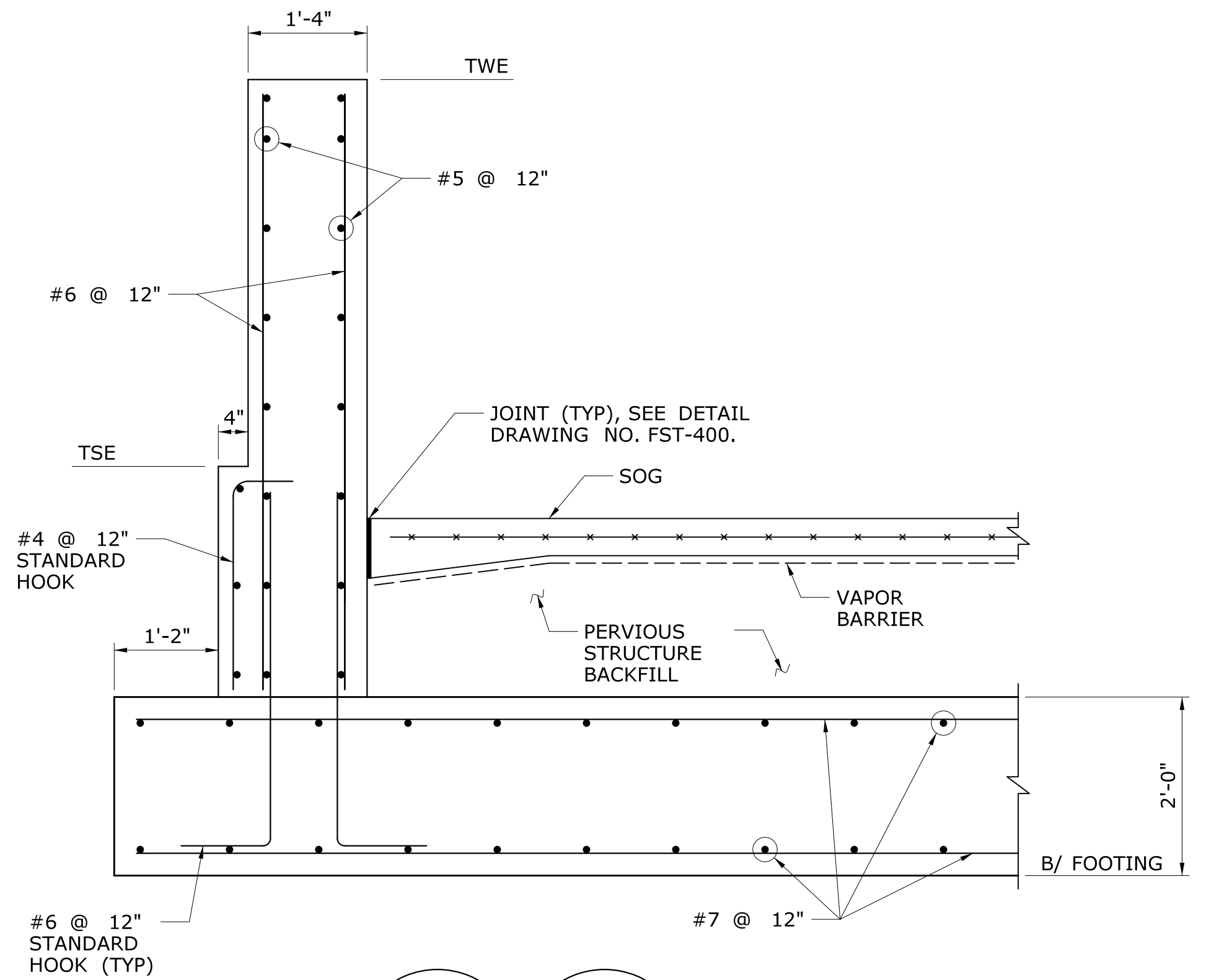
10 **10** **SECTION**
FST-107 **FST-108** SCALE: $\frac{3}{4}" = 1'-0"$



8 **8** **SECTION**
FST-107 **FST-108** SCALE: $\frac{3}{4}" = 1'-0"$



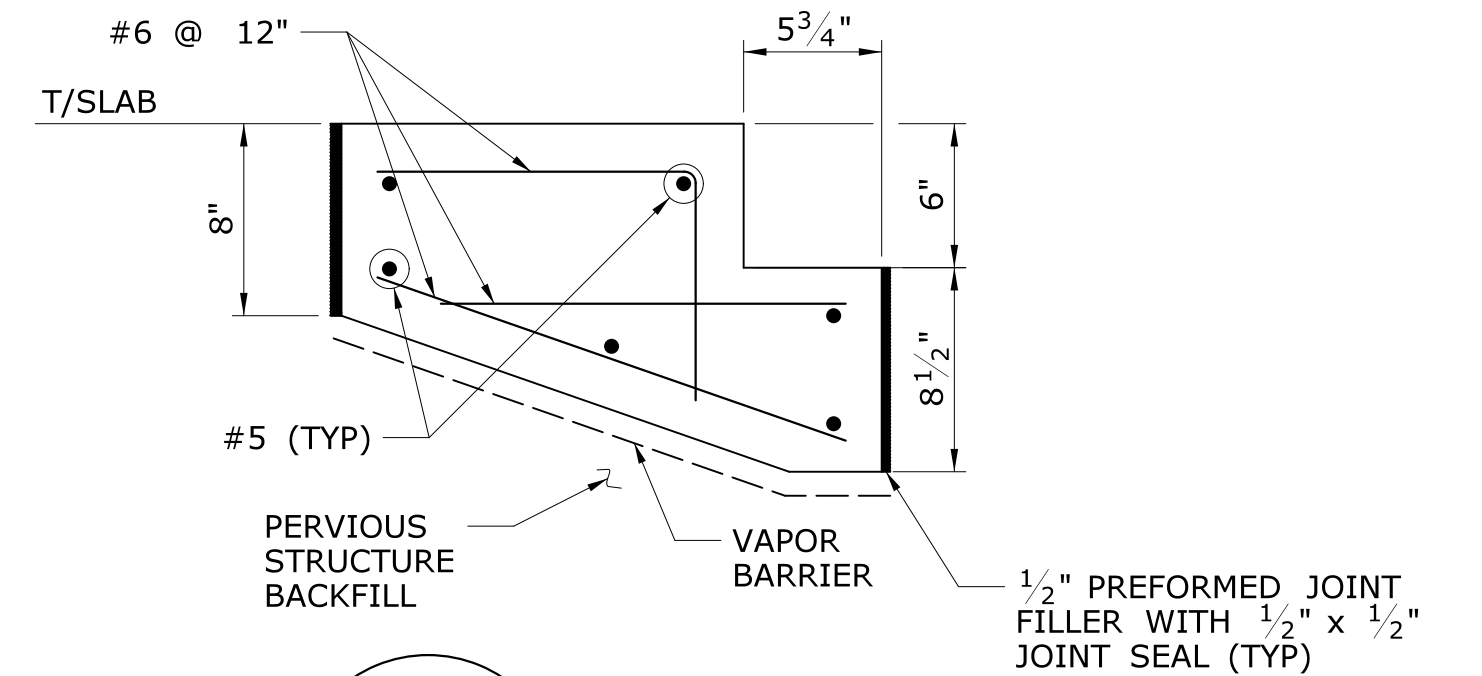
11 **11** **SECTION**
FST-107 **FST-108** SCALE: $\frac{3}{4}" = 1'-0"$



9 **9** **SECTION**
FST-107 **FST-108** SCALE: $\frac{3}{4}" = 1'-0"$

NOTES:

- SEE TOWER FOUNDATION PLANS ON DRAWING NOS. FST-107 AND FST-108.
- FOR T/SLAB EL, TWE, TSE AND B/FOOTING EL, SEE DRAWING NOS. FST-107 AND FST-108.
- FOR SOG DETAILS, SEE DRAWING NO. FST-400.
- FOR EMBEDMENT LENGTH AND LAP SPLICE LENGTH, SEE DRAWING NO. FST-400.
- 12" THICK LAYER OF GRANULAR FILL BELOW FOOTING NOT SHOWN FOR CLARITY.



1 **DETAIL**
SCALE: $1\frac{1}{2}" = 1'-0"$

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
J POPOLI
CHECKED BY:
H BUI
SCALE AS NOTED

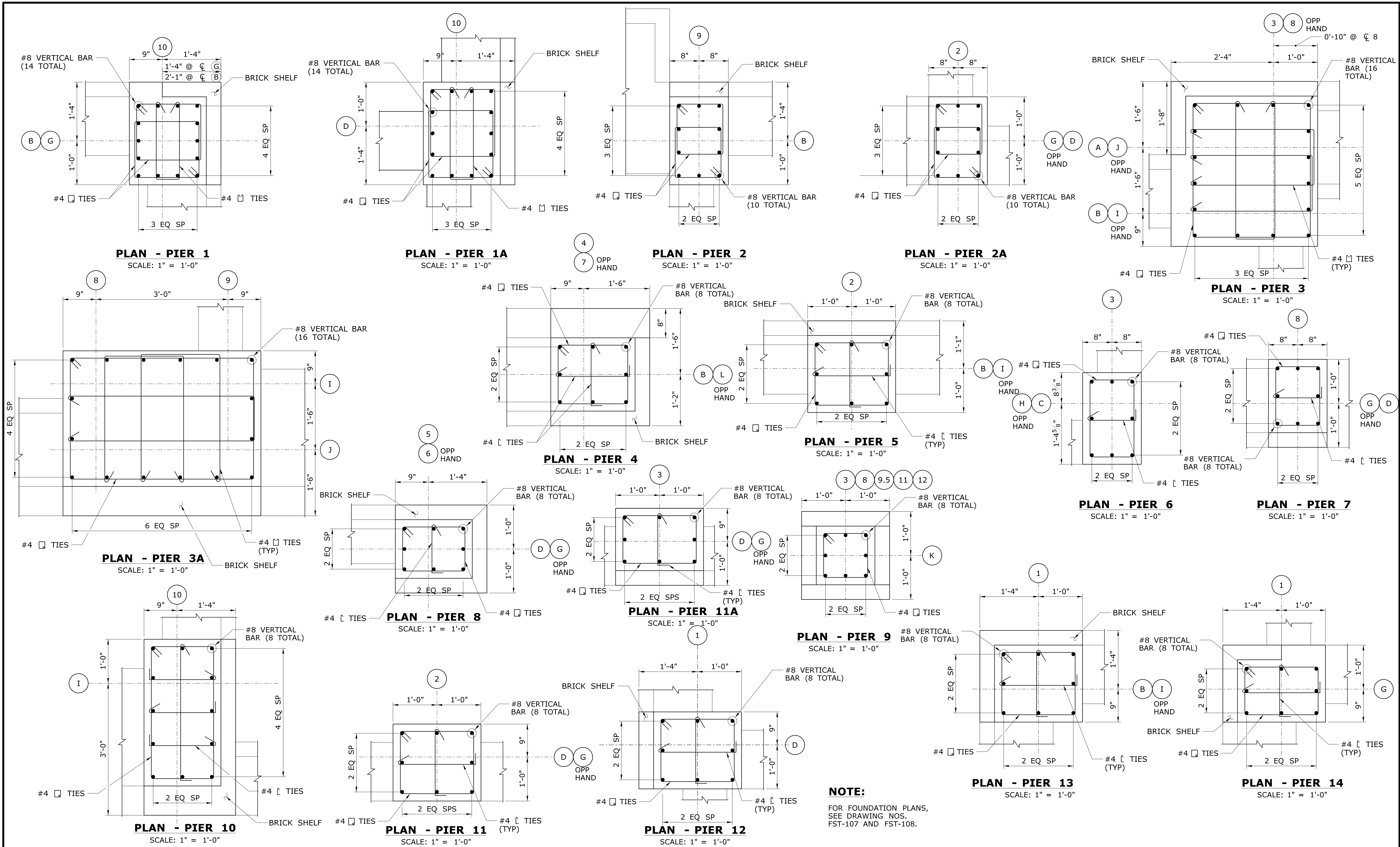
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_219.dgn




SIGNATURE/
BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

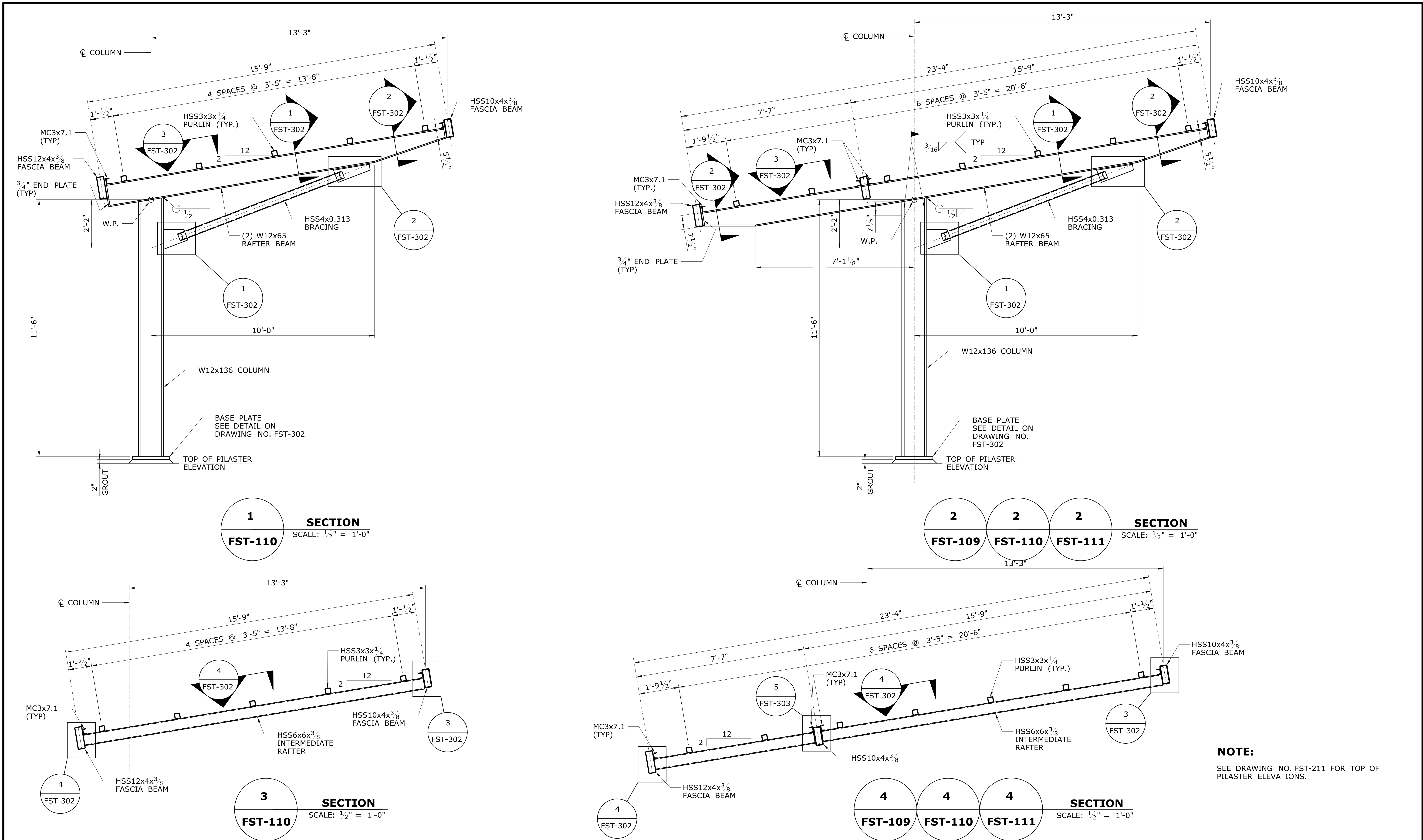
PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**TOWER FOUNDATION
DETAILS 4**

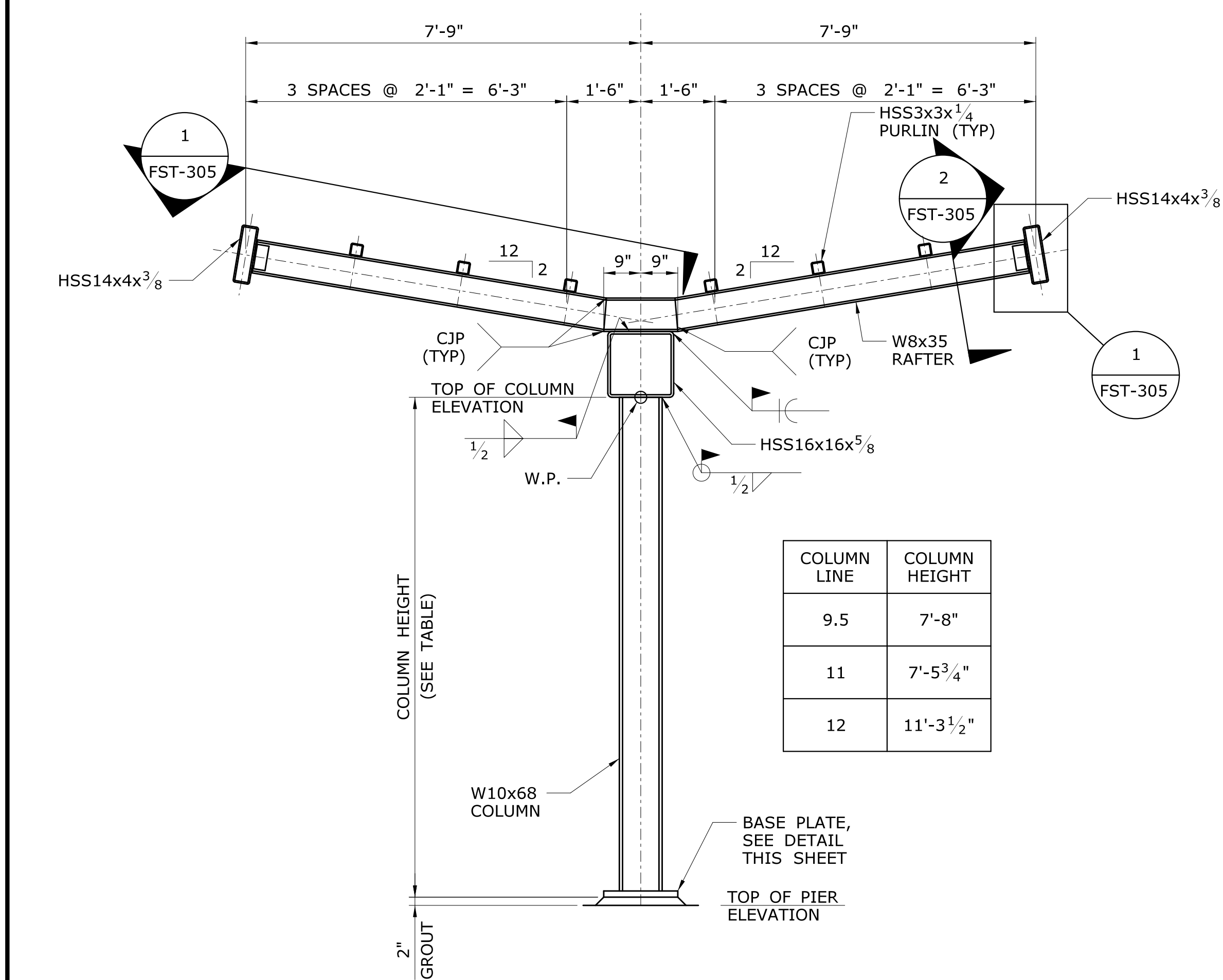
PROJECT NO.
170-3155
DRAWING NO.
FST-219
SHEET NO.
01.12.040



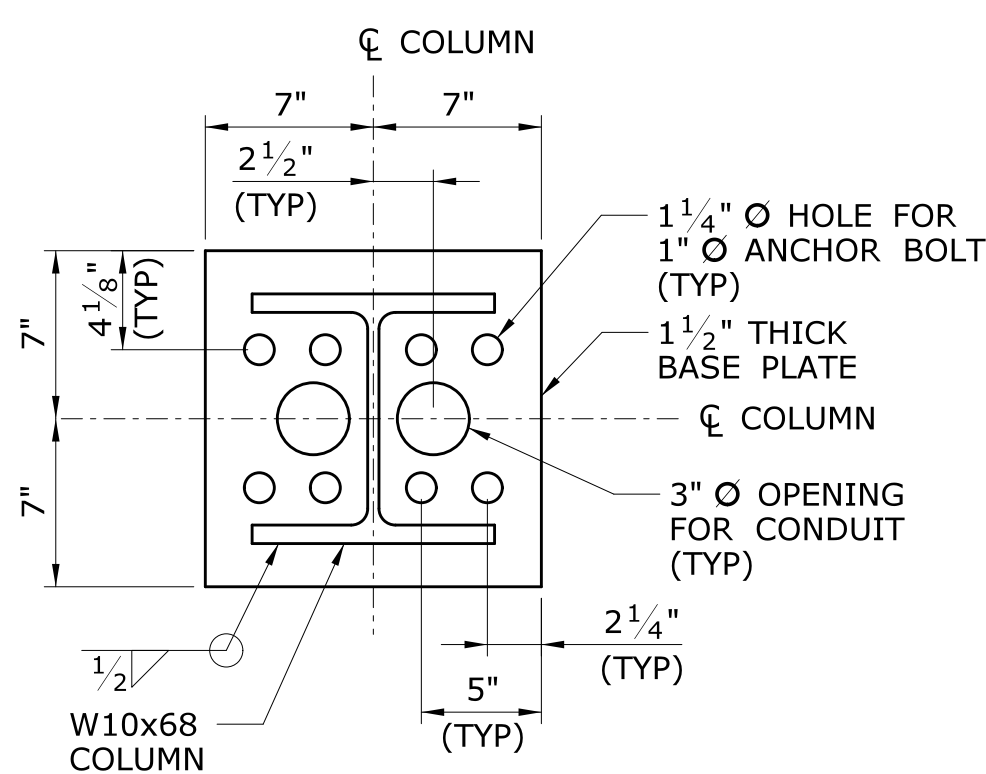
| | | | | | | | | | | |
|------|------|----------------------|---|-------------------------|---|---|---|---|---|--|
| | | | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | | DESIGNER/DRAFTER: J POPOLI CHECKED BY: H BUI SCALE AS NOTED |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_220.dgn |   530 PRESTON AVENUE MERIDEN, CT 06450 | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | TOWN: BERLIN DRAWING TITLE: TOWER FOUNDATION DETAILS 5 | PROJECT NO. 170-3155 DRAWING NO. FST-220 SHEET NO. 01.12.041 |
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. | Plotted Date: 2/26/2014 | | | | | | |



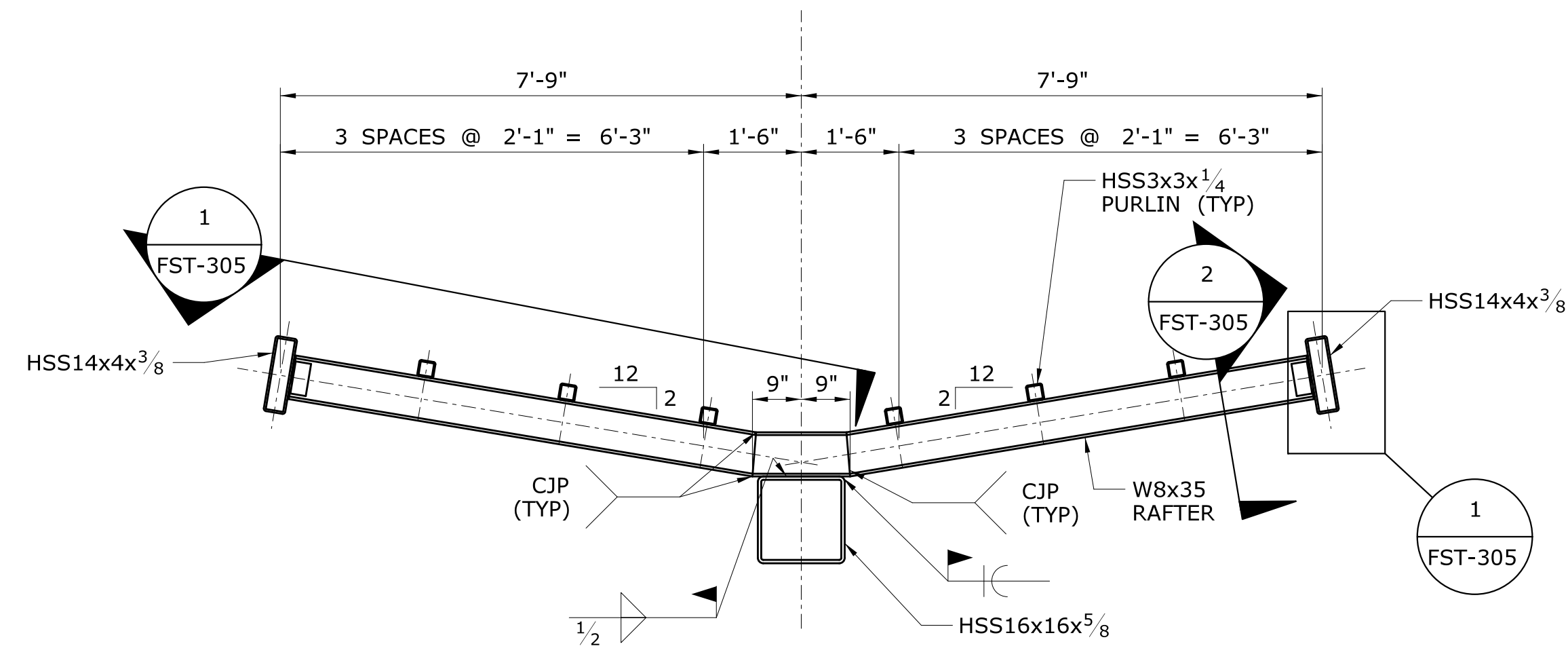
| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|



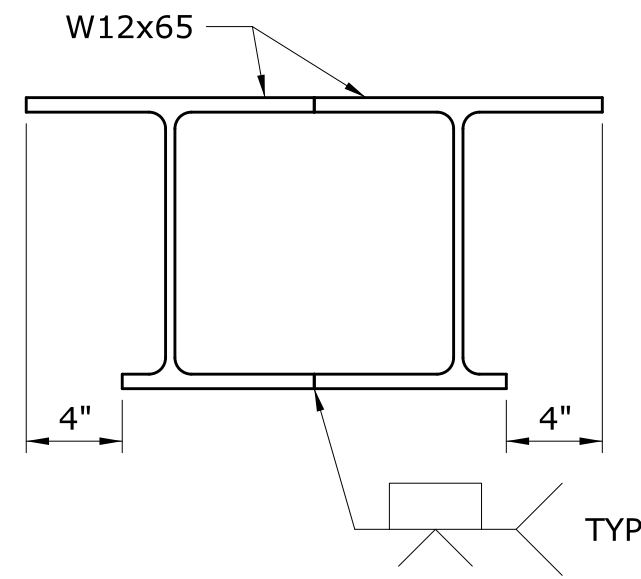
1
FST-111
SECTION
SCALE: 1/2" = 1'-0"



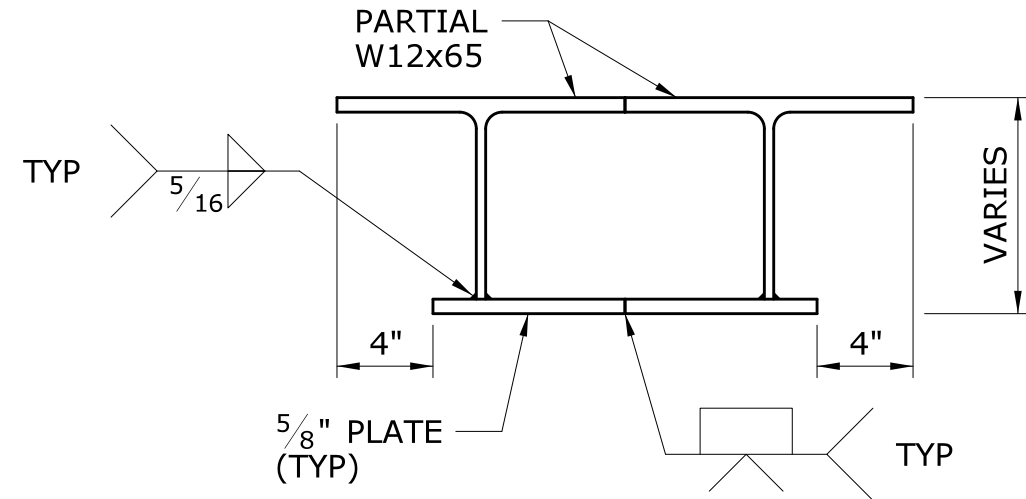
BASE PLATE DETAIL
SCALE: 1 1/2" = 1'-0"



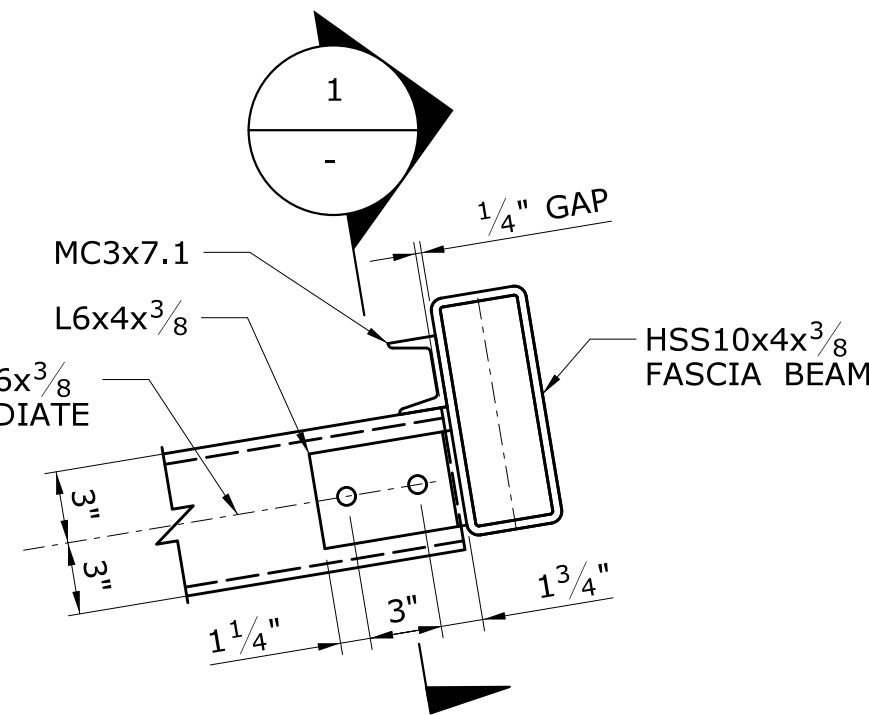
2
FST-111
SECTION
SCALE: 1/2" = 1'-0"



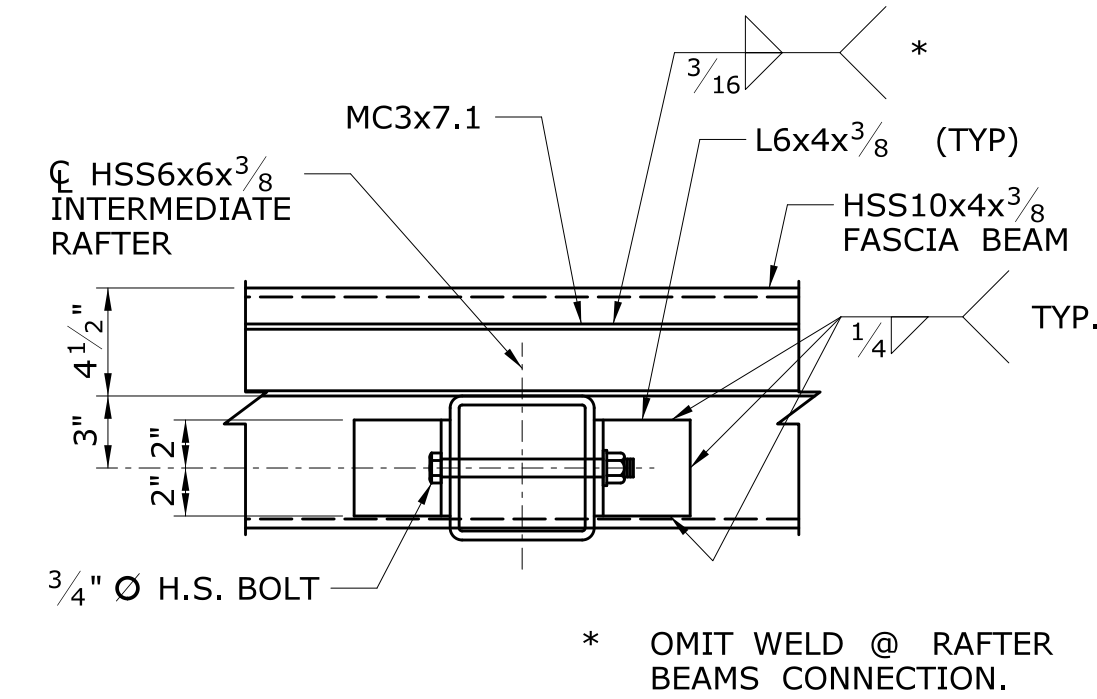
1
SECTION
FST-300 SCALE: 1 1/2" = 1'-0"



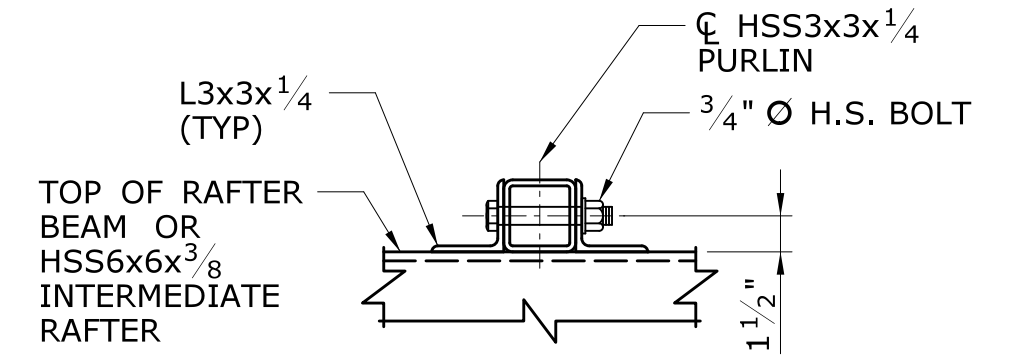
2
SECTION
FST-300 SCALE: 1 1/2" = 1'-0"



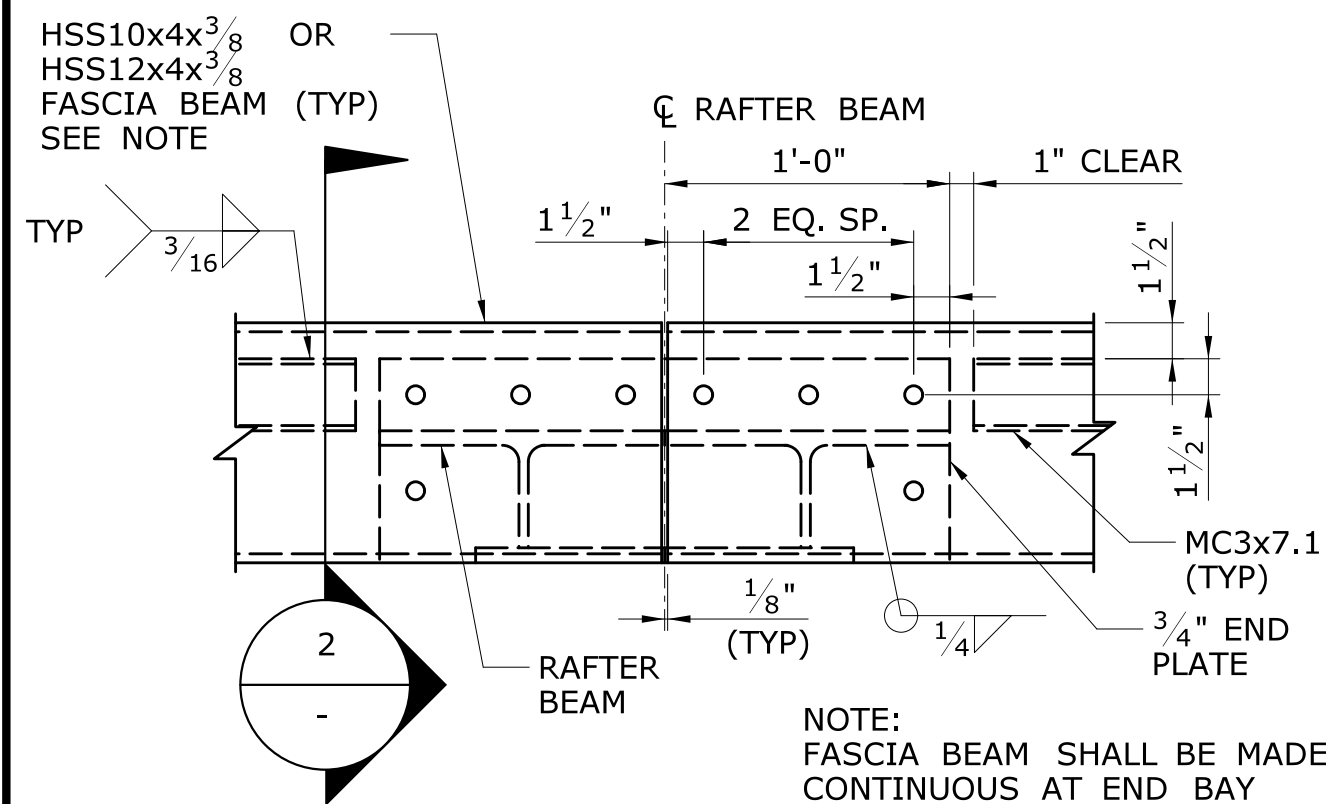
3
DETAIL
FST-300 SCALE: 1 1/2" = 1'-0"



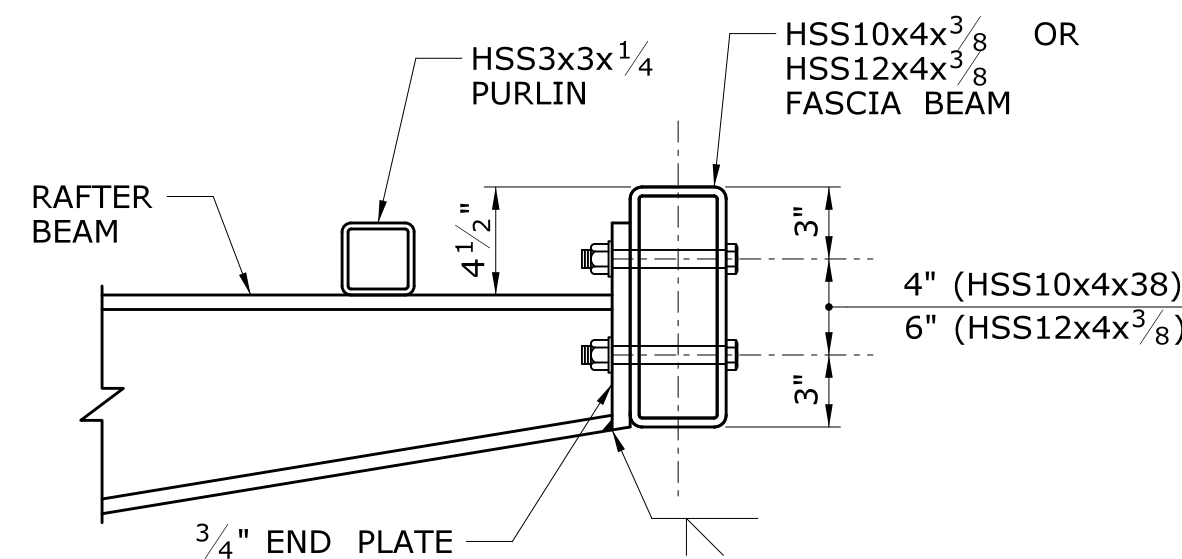
1
SECTION
SCALE: 1 1/2" = 1'-0"



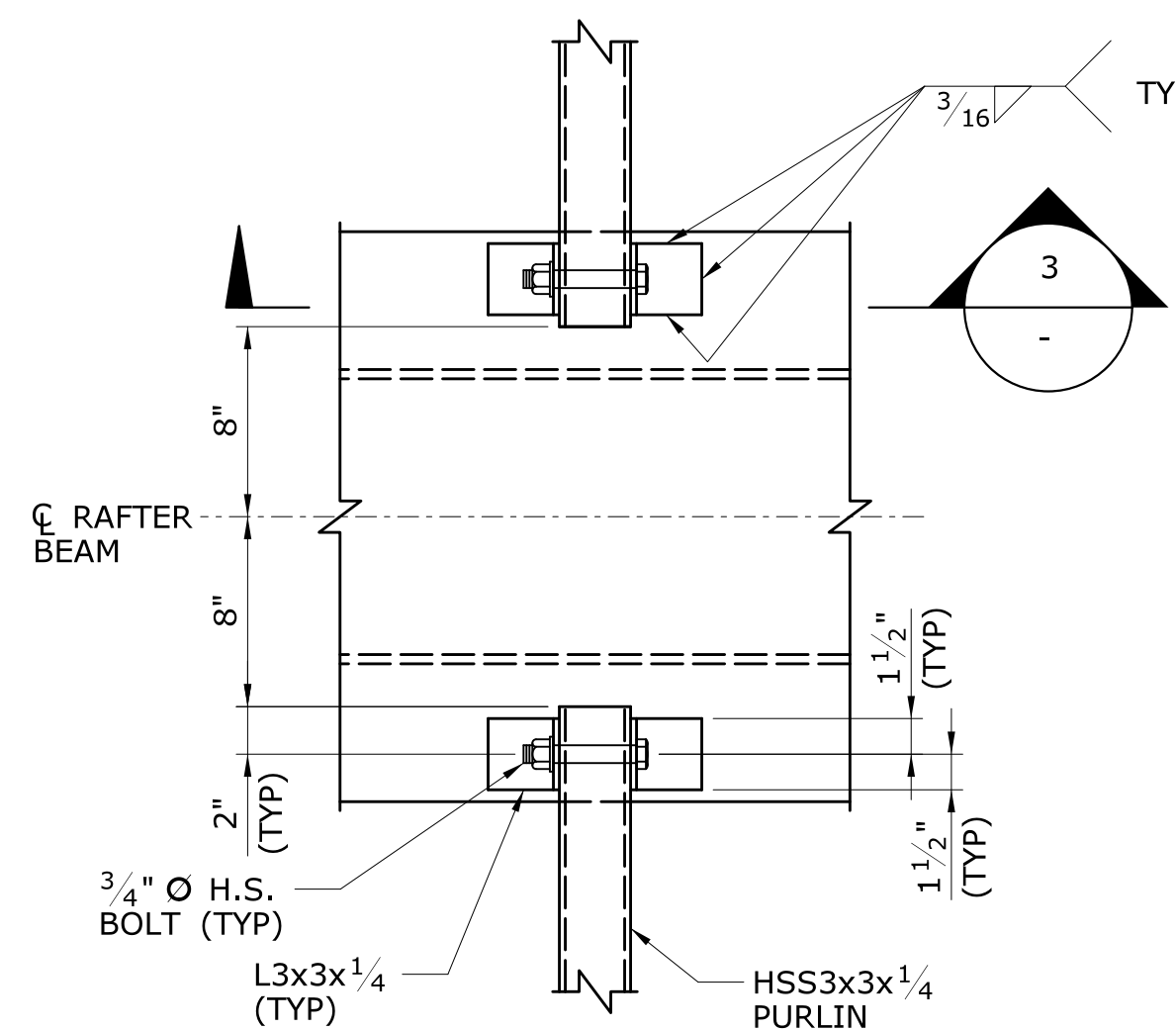
3
SECTION
FST-305 SCALE: 1 1/2" = 1'-0"



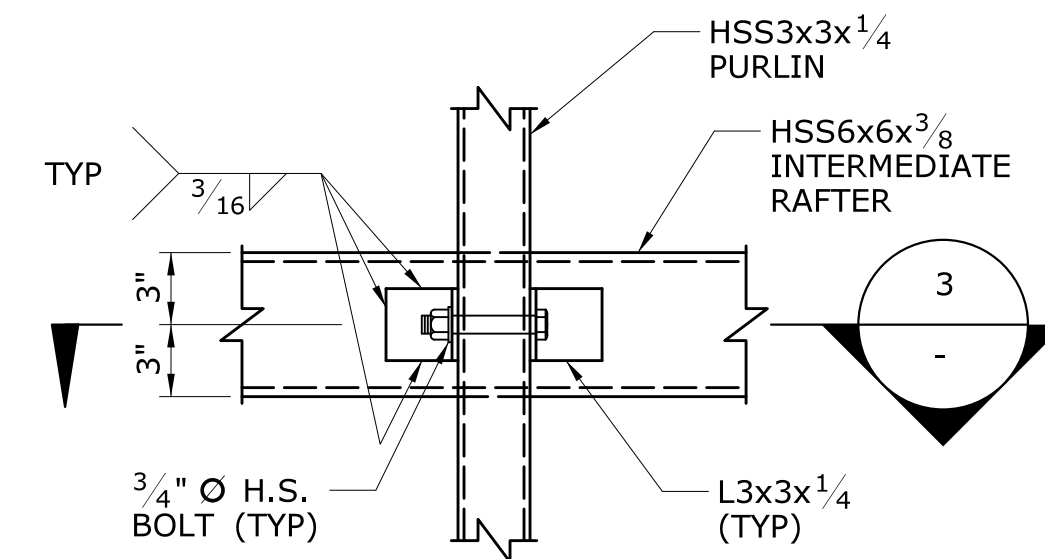
TYPICAL FASCIA BEAM CONNECTION
SCALE: 1 1/2" = 1'-0"



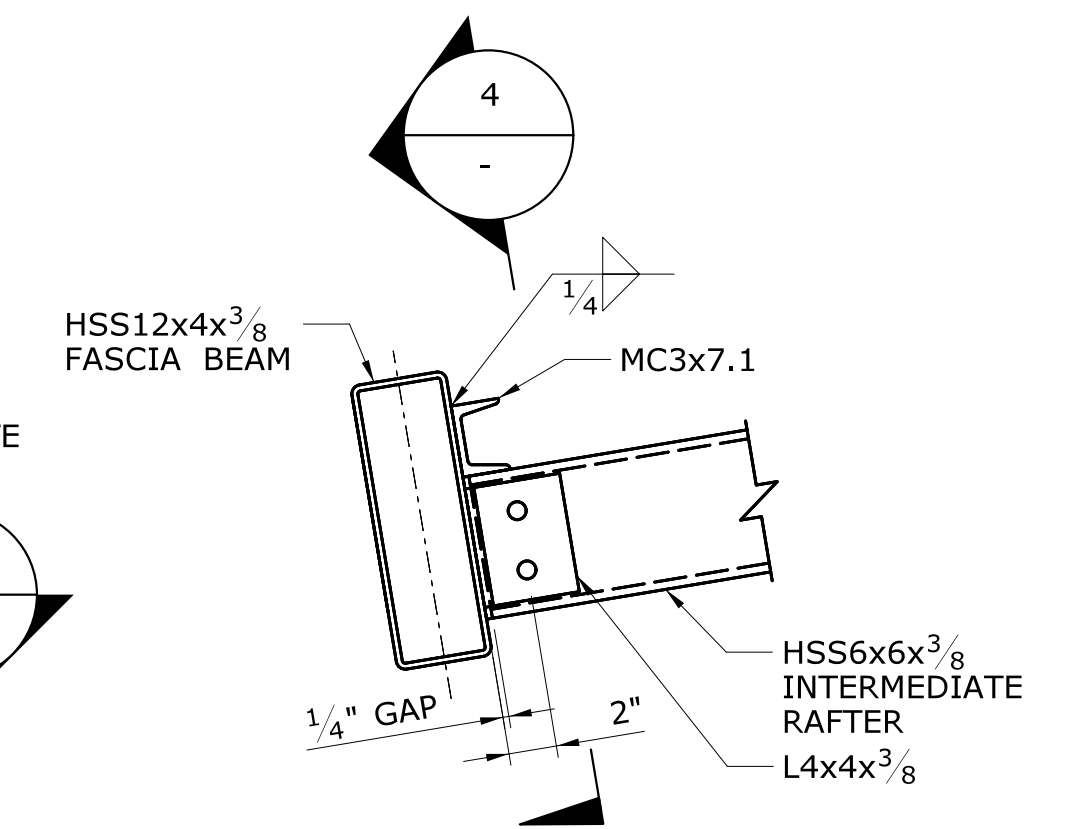
2
SECTION
SCALE: 1 1/2" = 1'-0"



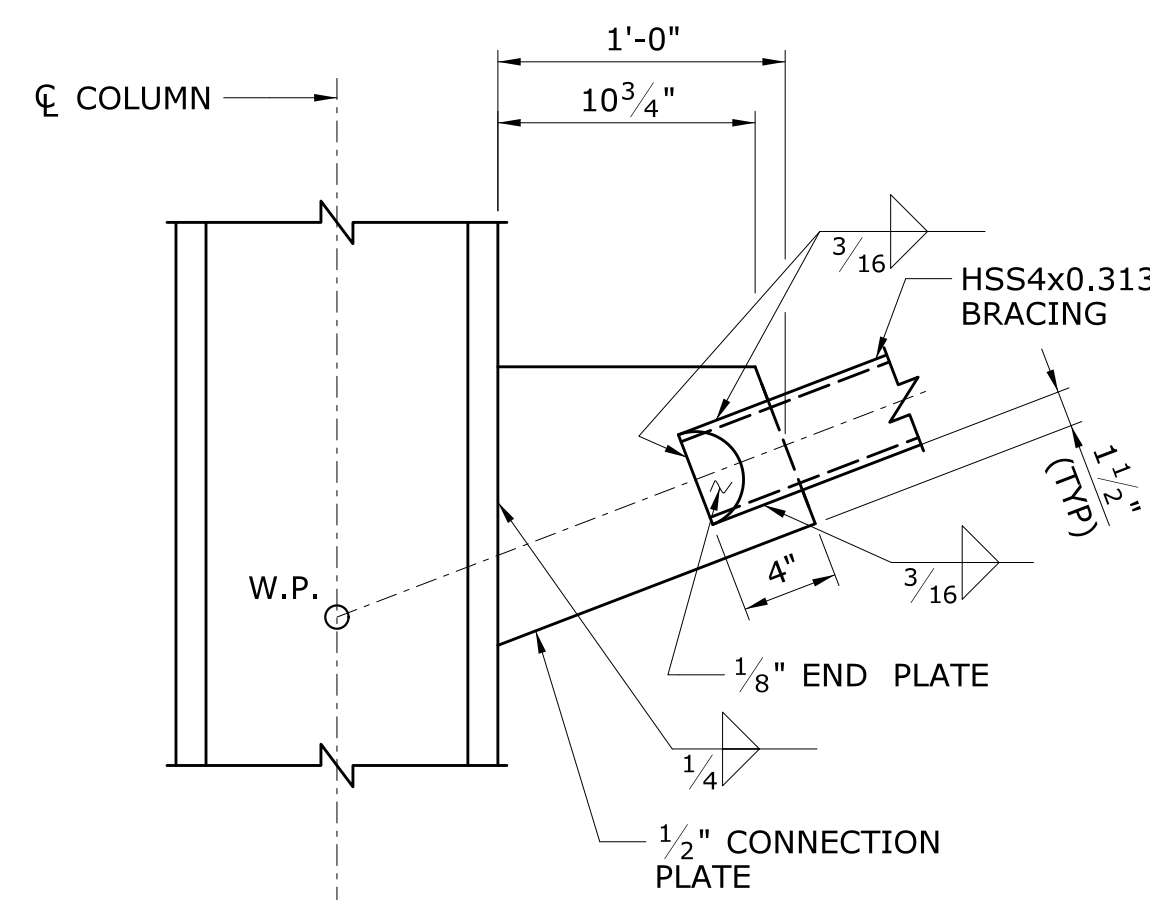
3
VIEW
FST-300 SCALE: 1 1/2" = 1'-0"



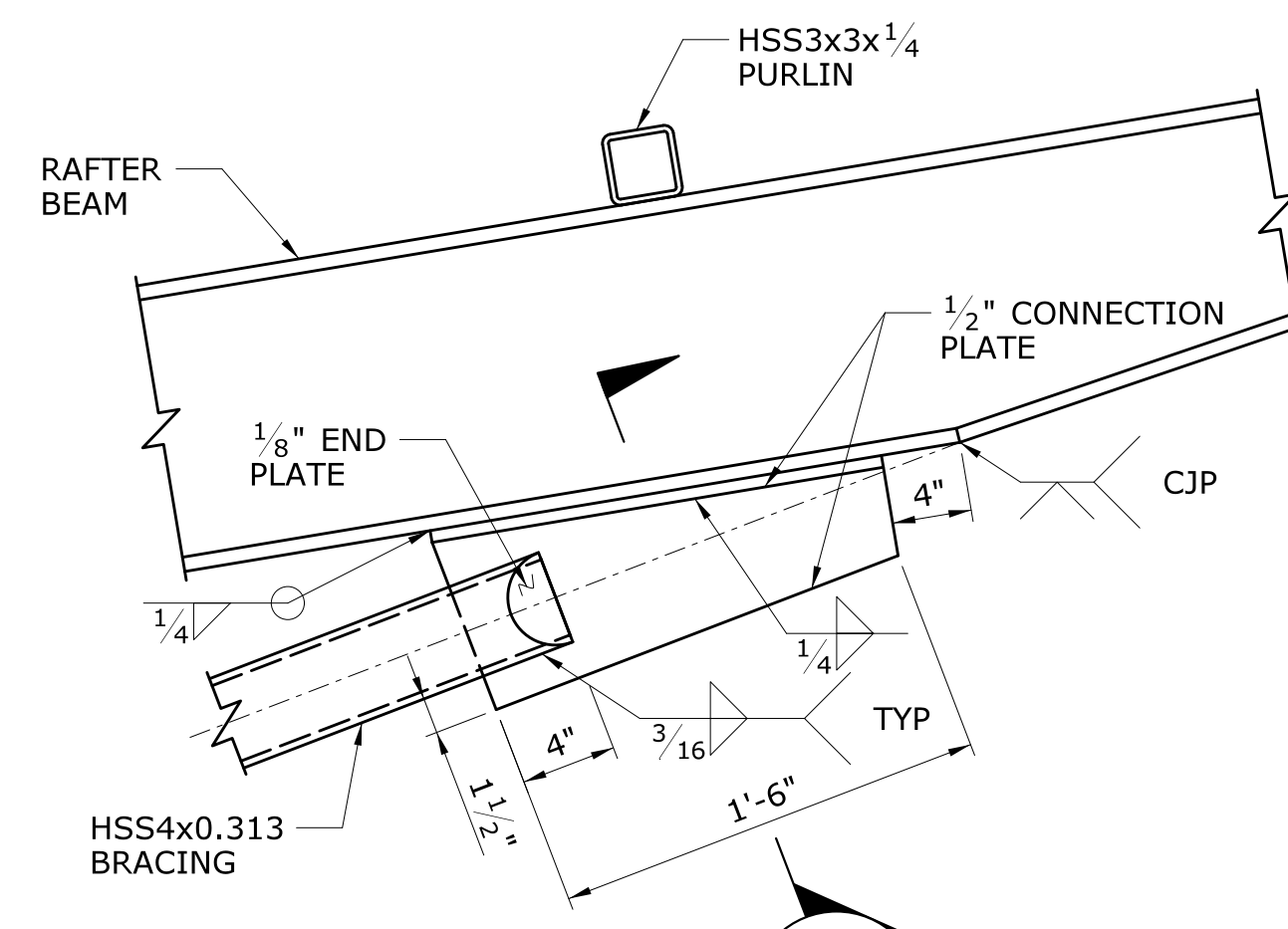
4
VIEW
FST-300 SCALE: 1 1/2" = 1'-0"



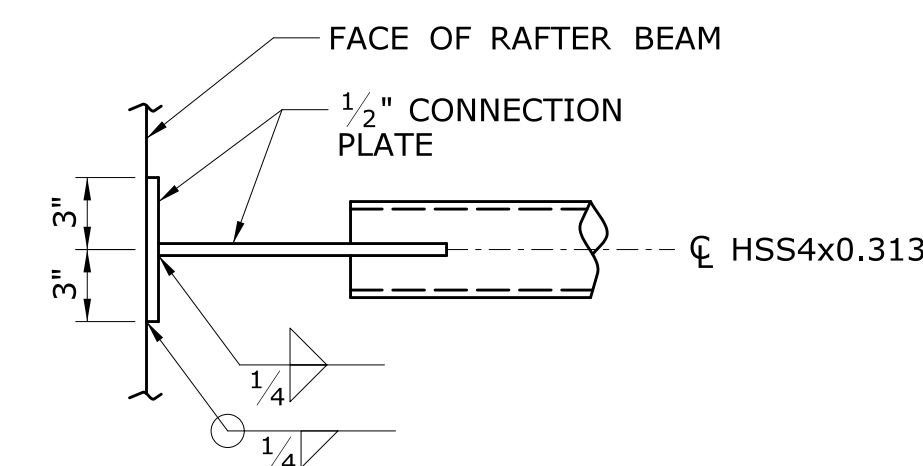
4
DETAIL
FST-300 SCALE: 1 1/2" = 1'-0"



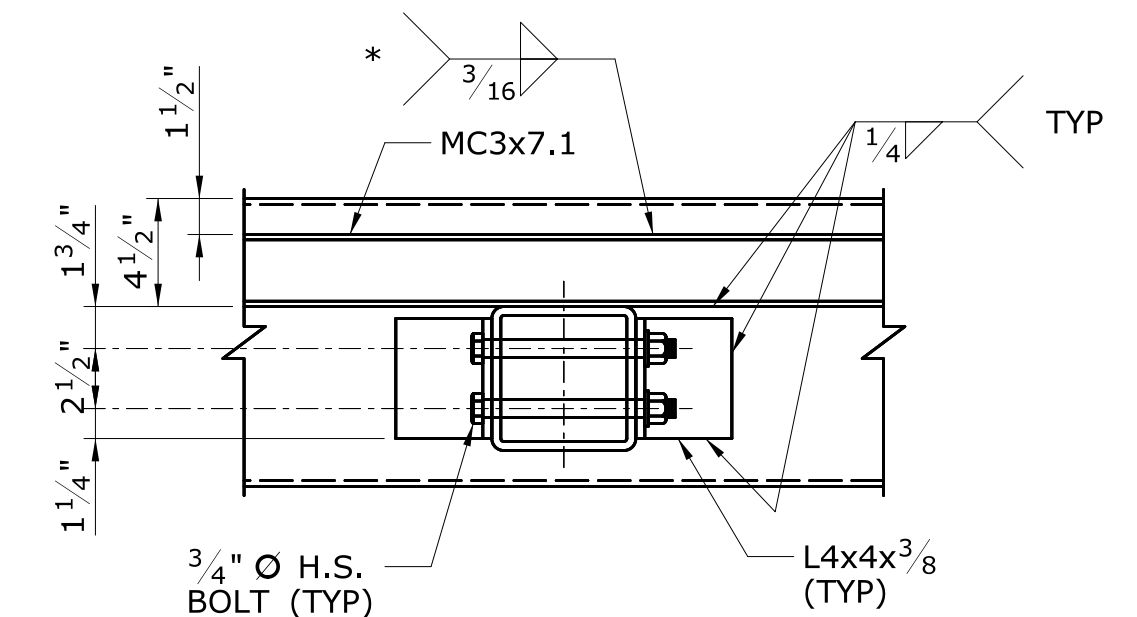
1
DETAIL
FST-300 SCALE: 1 1/2" = 1'-0"



2
DETAIL
FST-300 SCALE: 1 1/2" = 1'-0"



4
DETAIL
SCALE: 1 1/2" = 1'-0"



4
SECTION
SCALE: 1 1/2" = 1'-0"

| | | | |
|------|------|----------------------|-----------|
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
| - | - | - | - |
| - | - | - | - |
| - | - | - | - |
| - | - | - | - |
| - | - | - | - |
| - | - | - | - |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

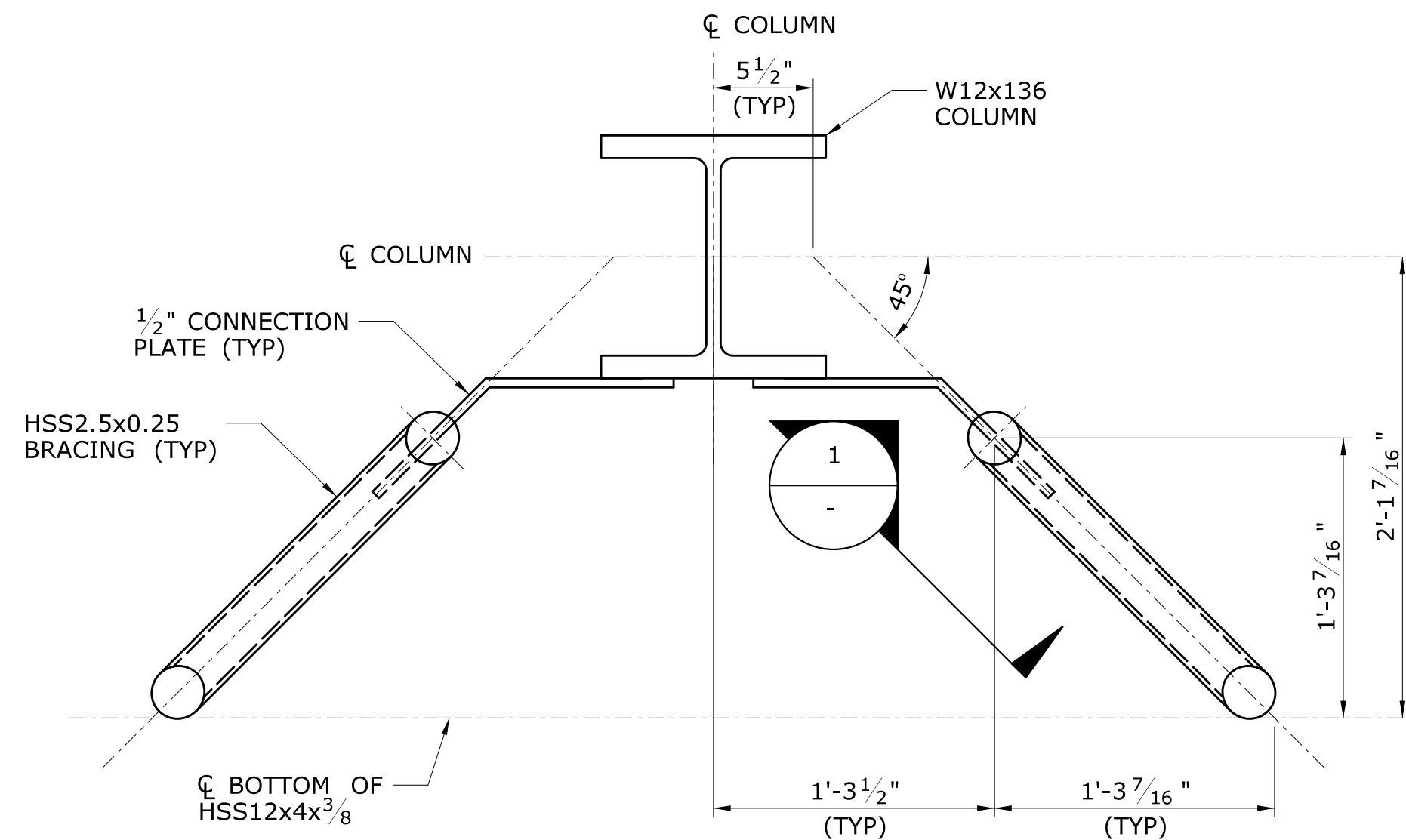
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_302.dgn

SIGNATURE/BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

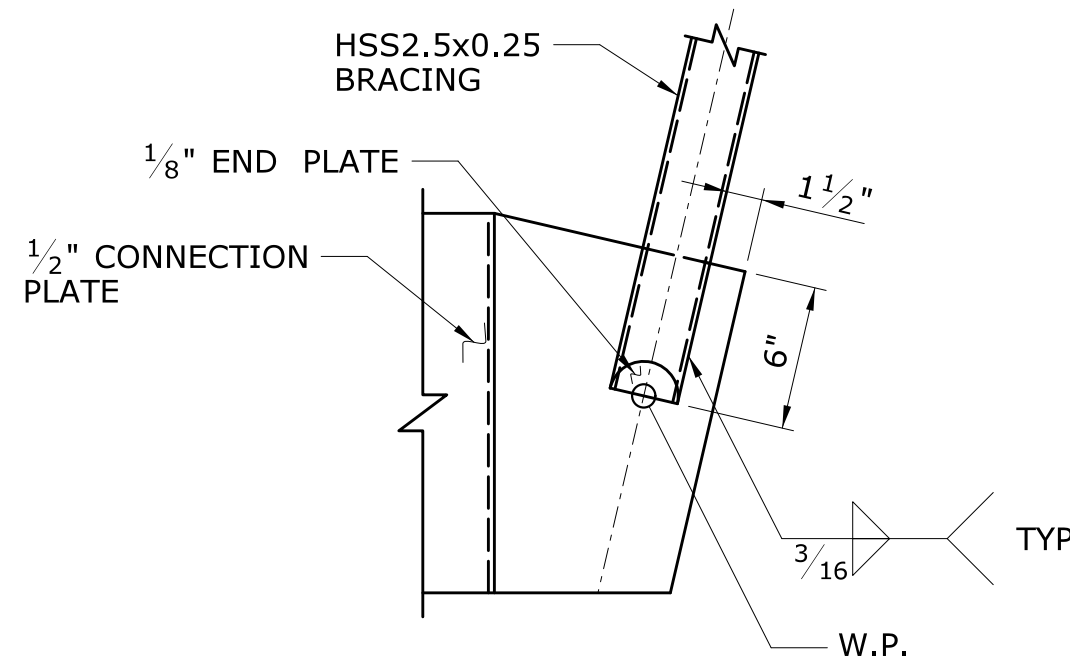
PROJECT TITLE:
NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR

TOWN:
BERLIN
DRAWING TITLE:
CANOPY SECTIONS
& DETAILS 3

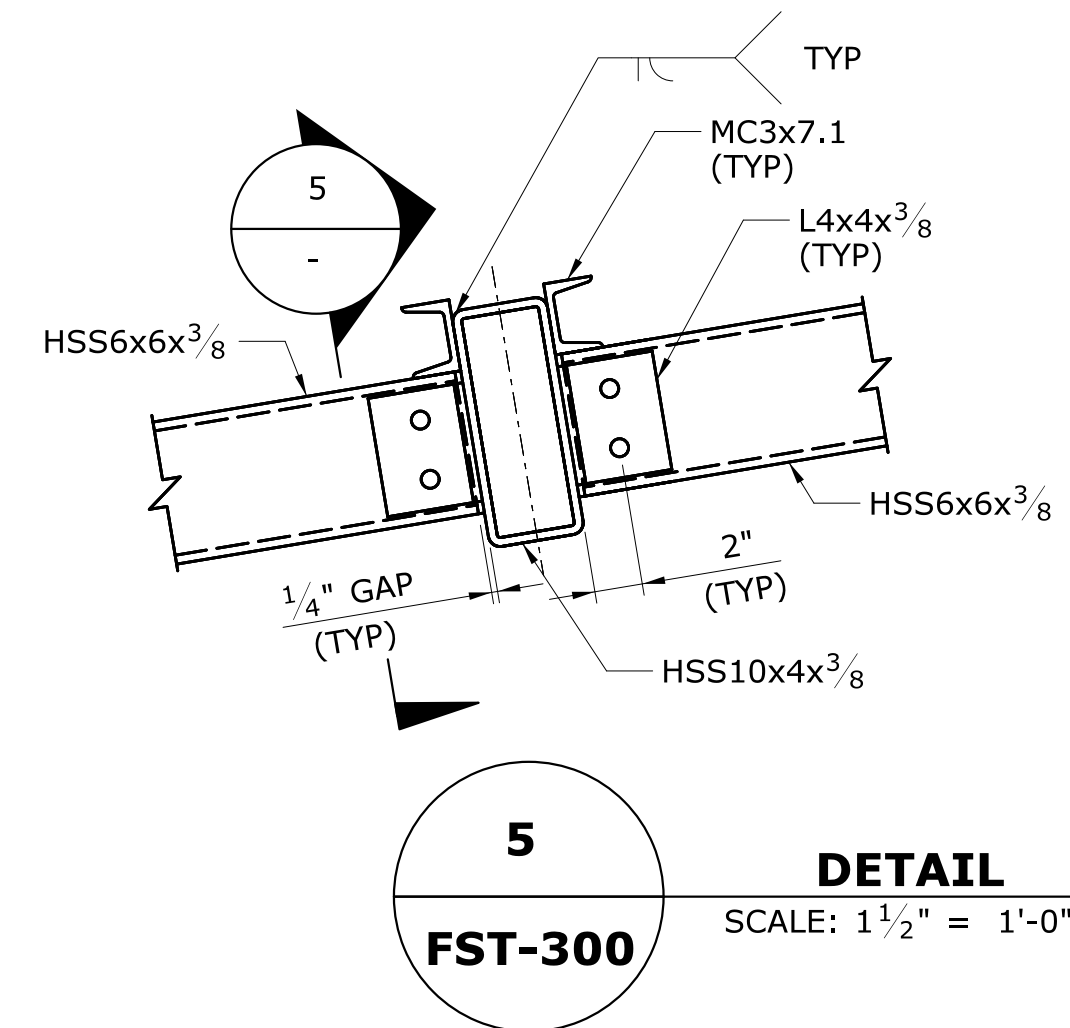
PROJECT NO.
170-3155
DRAWING NO.
FST-302
SHEET NO.
01.12.044



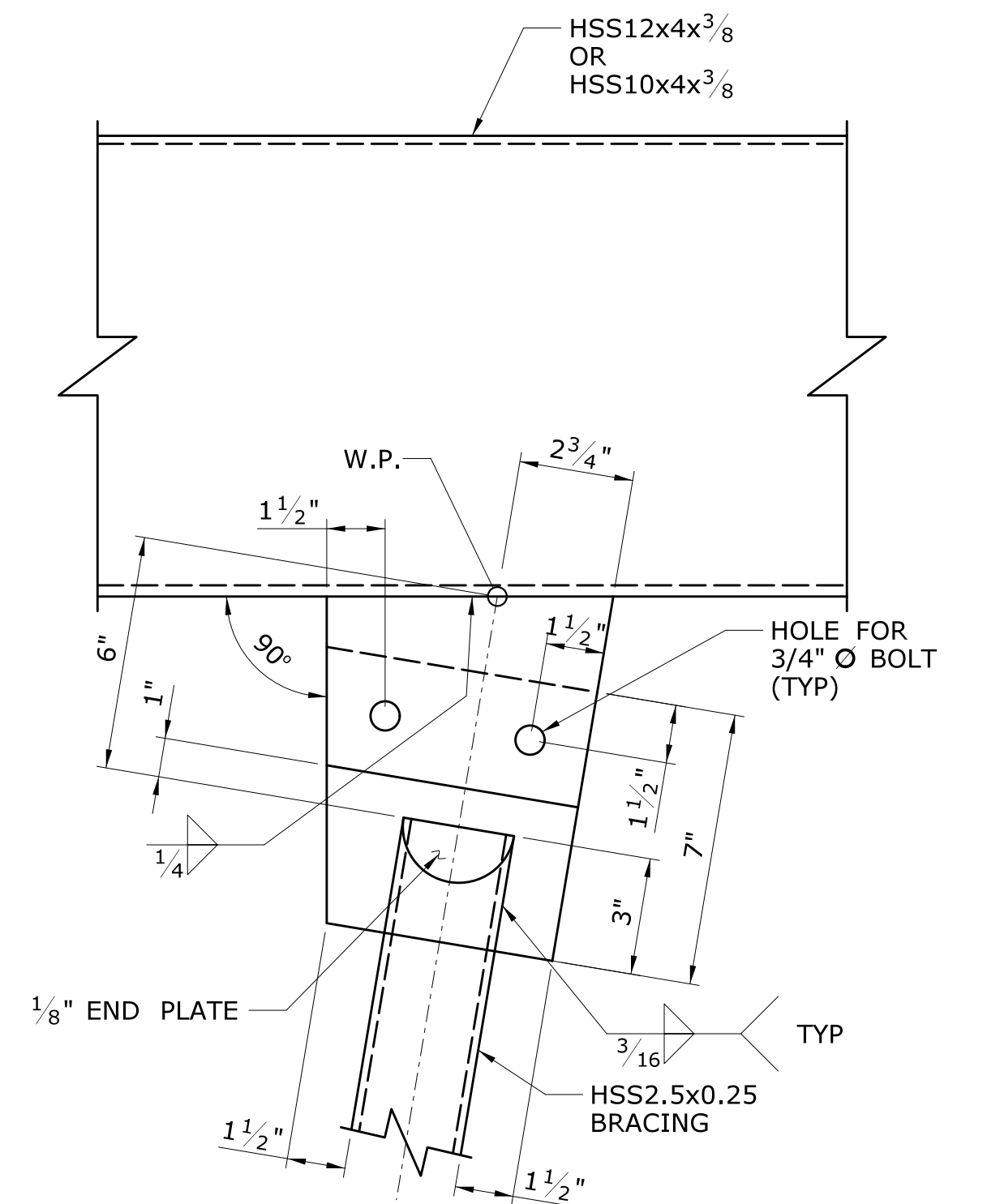
PLAN - CANOPY BRACING
SCALE: 1 1/2" = 1'-0"



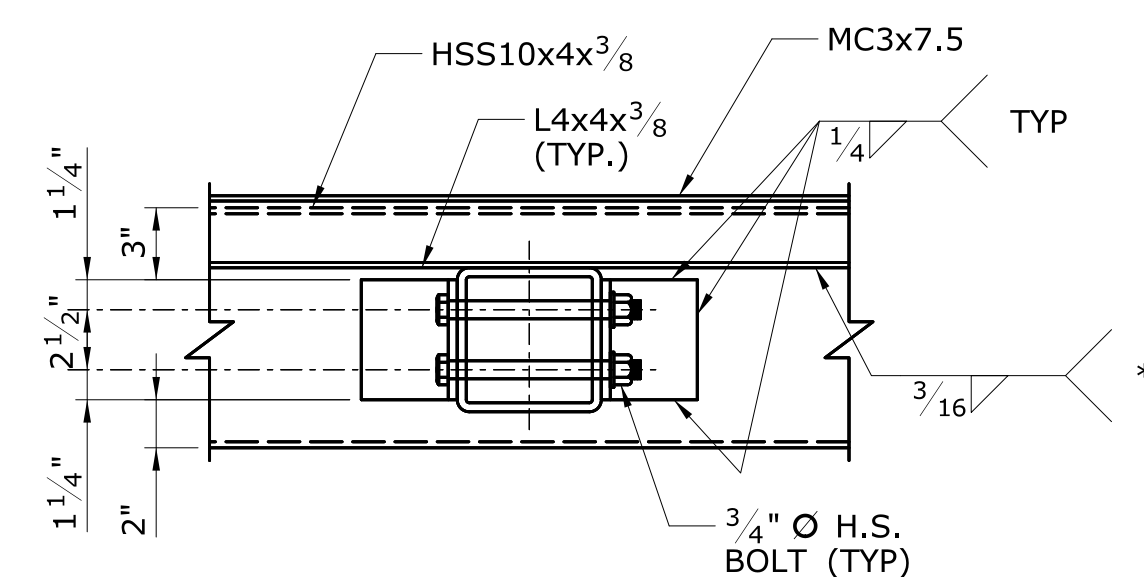
SECTION
SCALE: 1 1/2" = 1'-0"



DETAIL
SCALE: 1 1/2" = 1'-0"

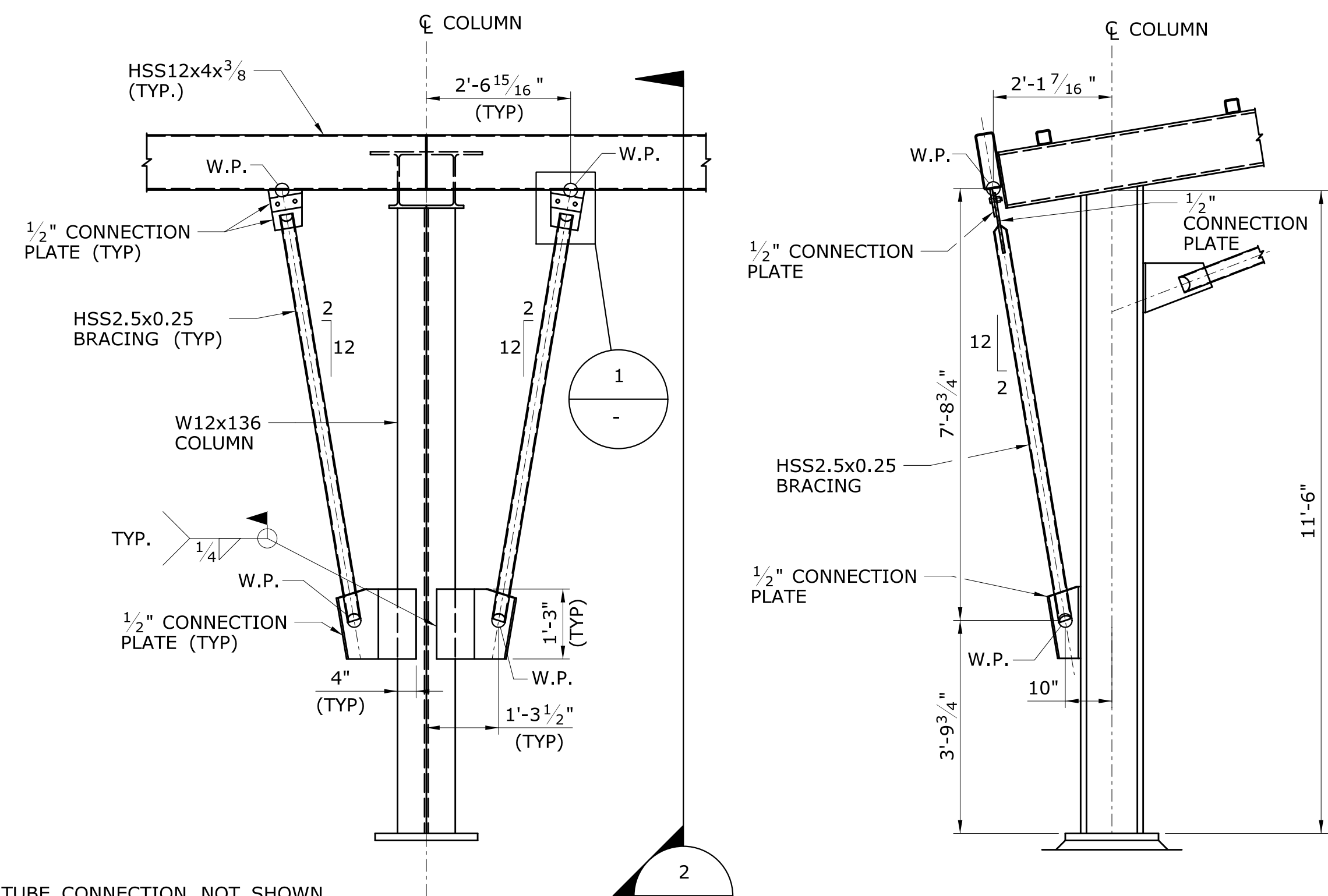


DETAIL
SCALE: 3" = 1'-0"

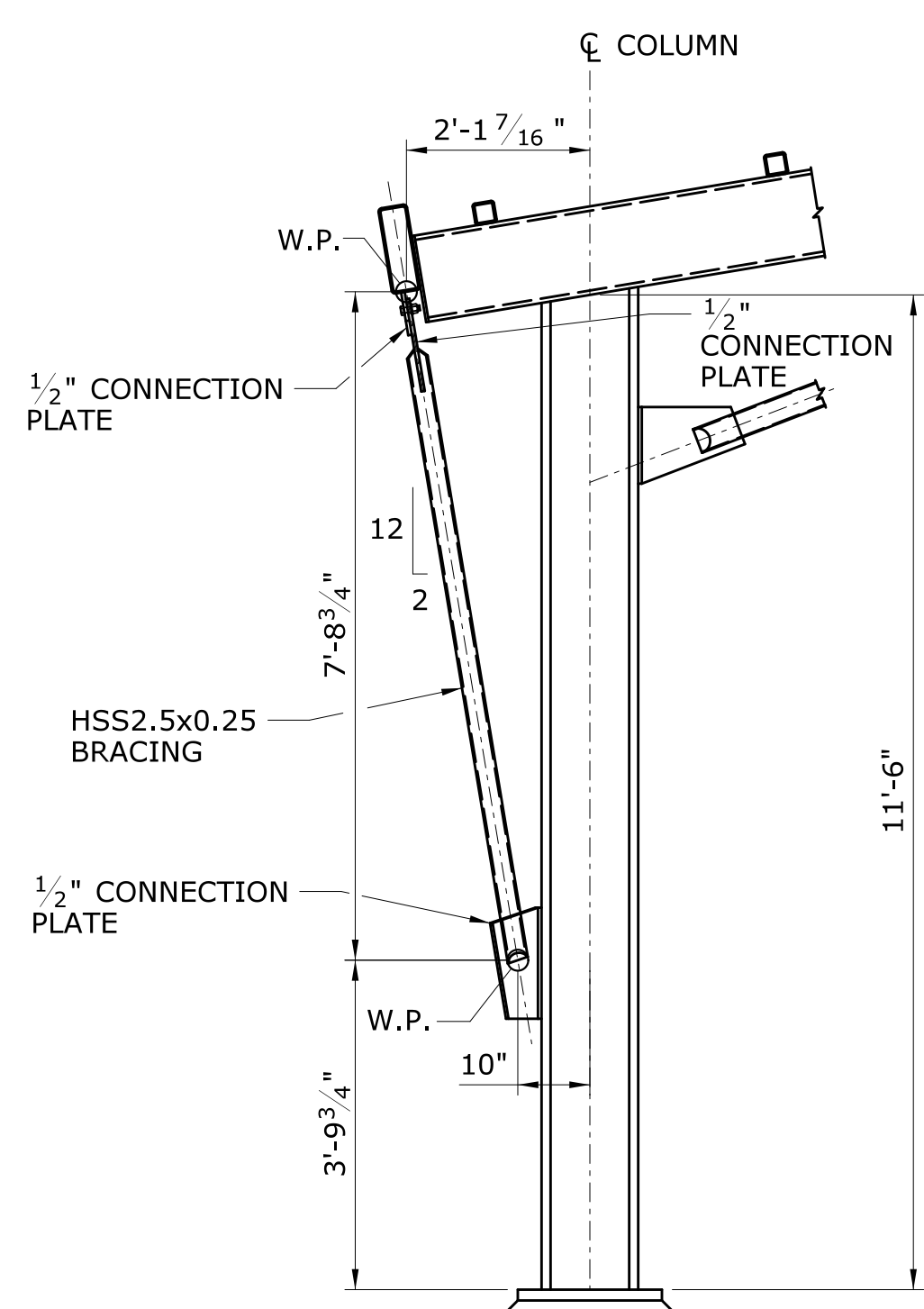


SECTION
SCALE: 1 1/2" = 1'-0"

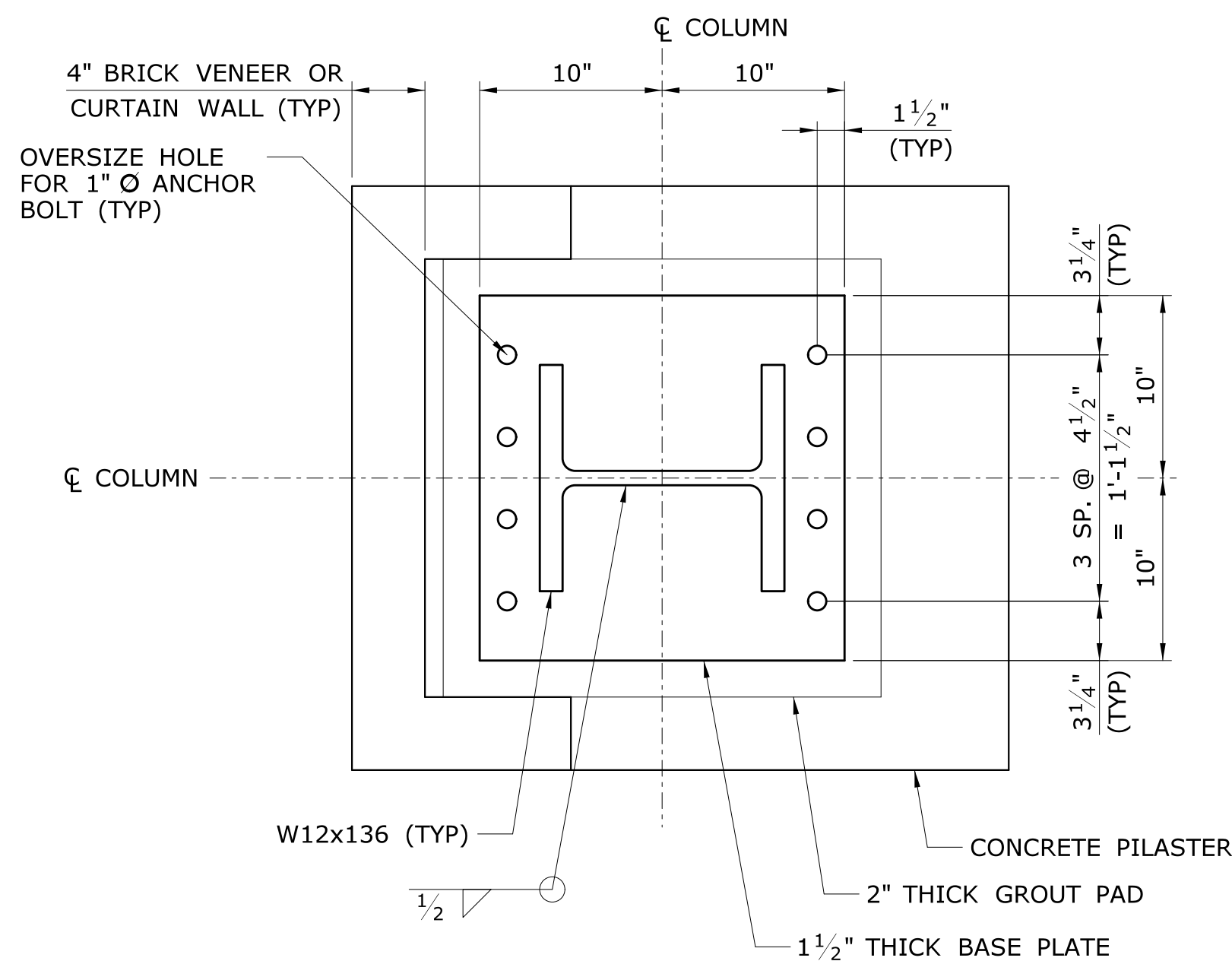
* OMIT WELD AT RAFTER BEAM CONNECTIONS.



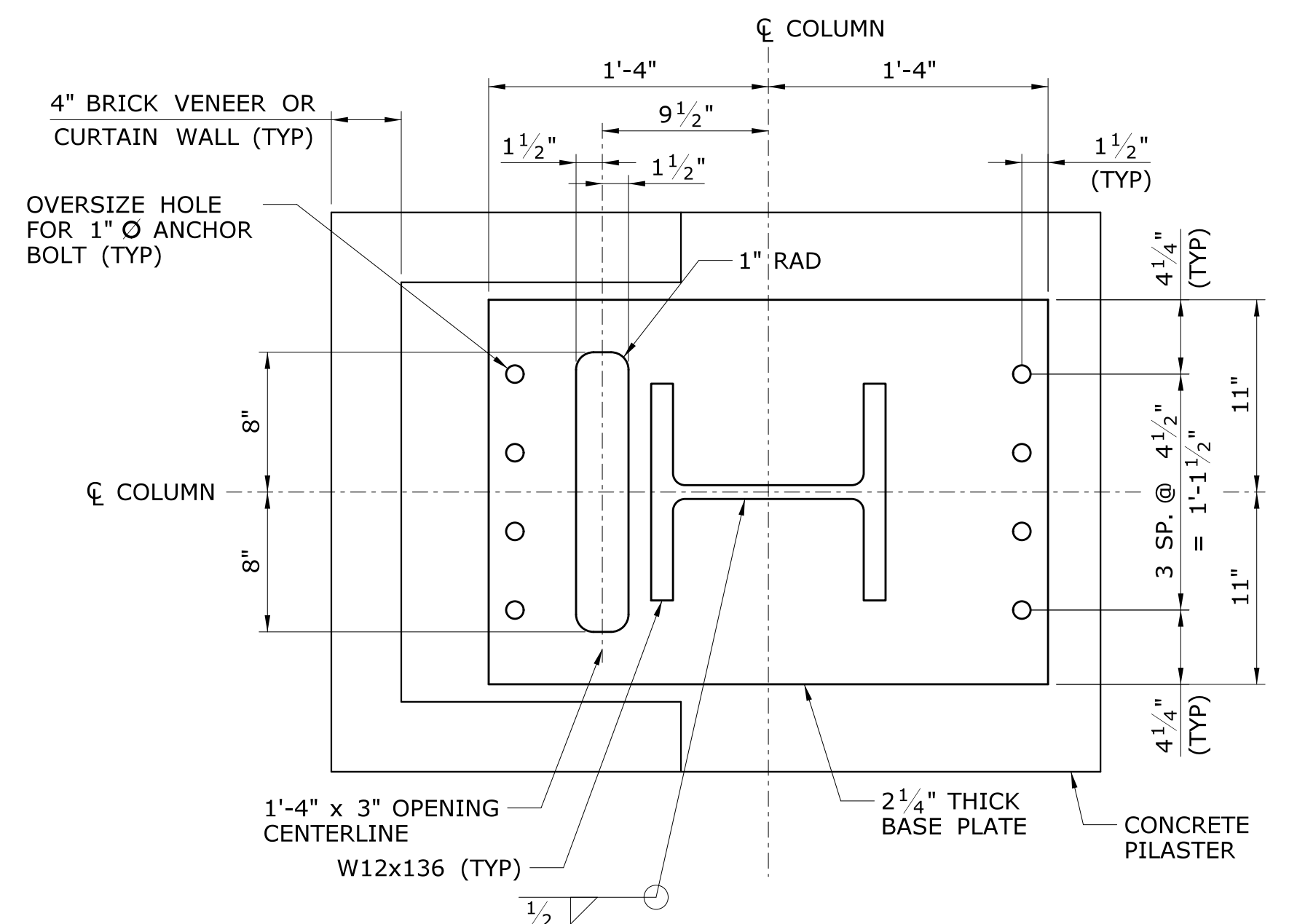
ELEVATION - CANOPY BRACING
SCALE: 1 1/2" = 1'-0"



SECTION
SCALE: 1 1/2" = 1'-0"



BASE PLATE DETAIL
SCALE: 1 1/2" = 1'-0"



BASE PLATE DETAIL @ UTILITY COLUMN
SCALE: 1 1/2" = 1'-0"

| | | | |
|------|------|----------------------|-----------|
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

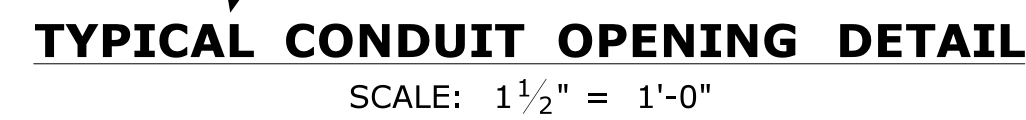
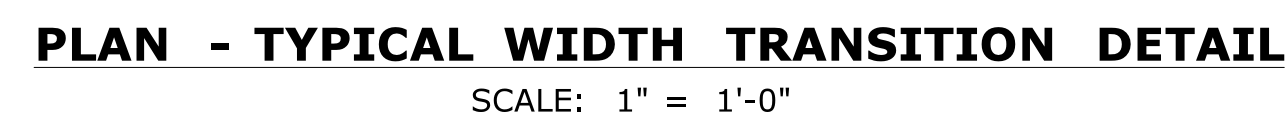
DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

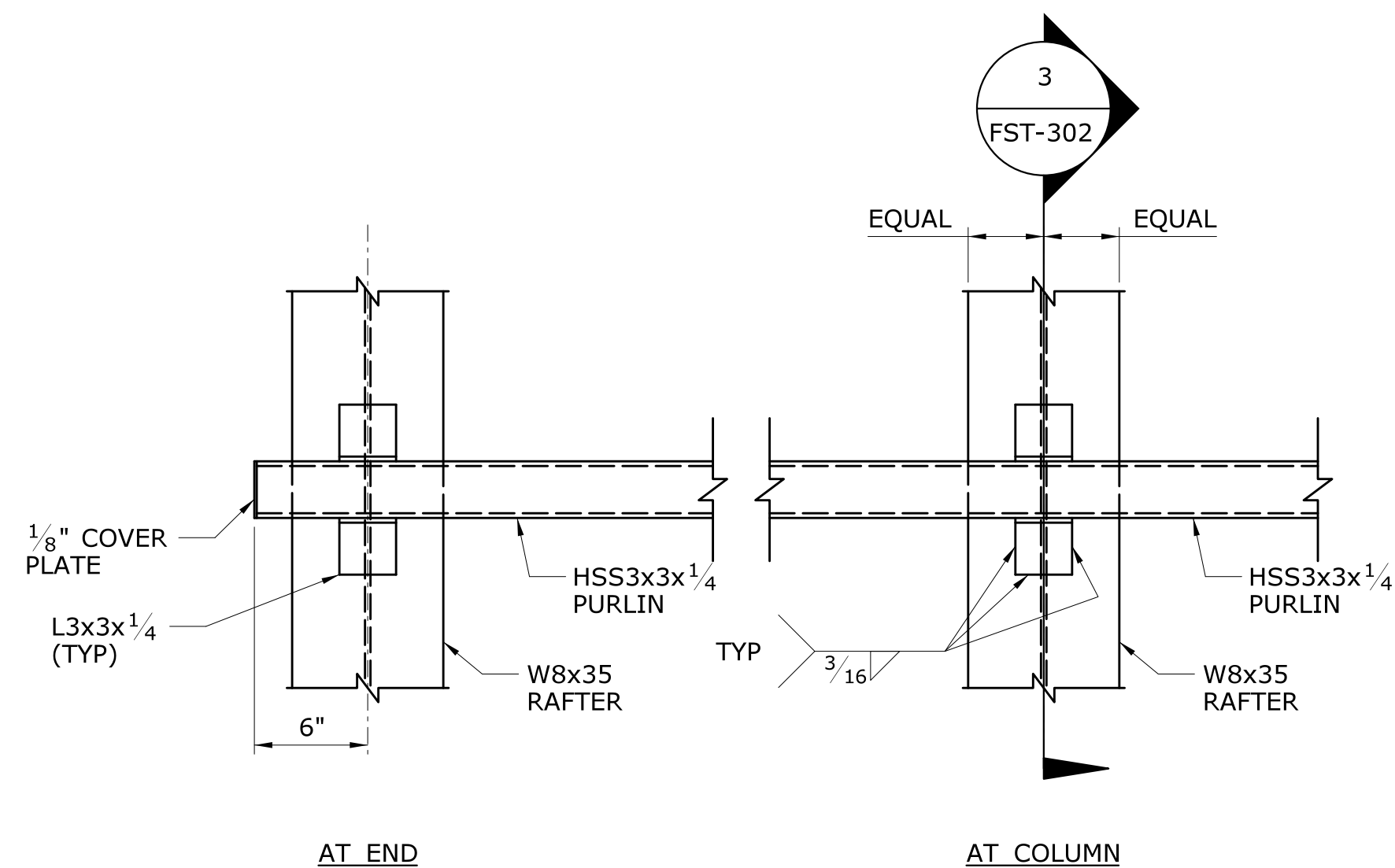
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...FA_CGR_CPS_0170-2296_007_04_FST_303.dgn

SIGNATURE/BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

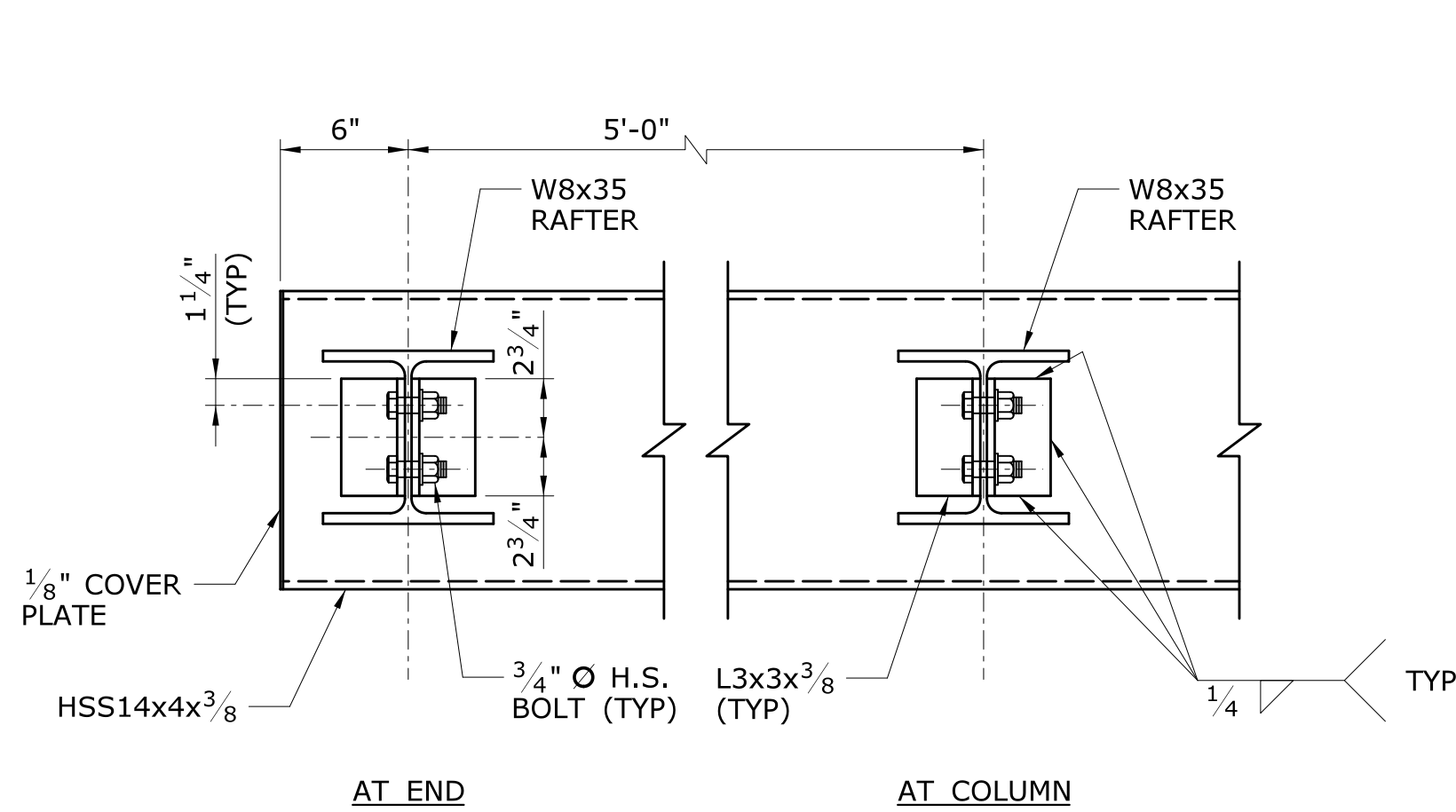
PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**CANOPY SECTIONS
& DETAILS 4**
PROJECT NO.
170-3155
DRAWING NO.
FST-303
SHEET NO.
01.12.045

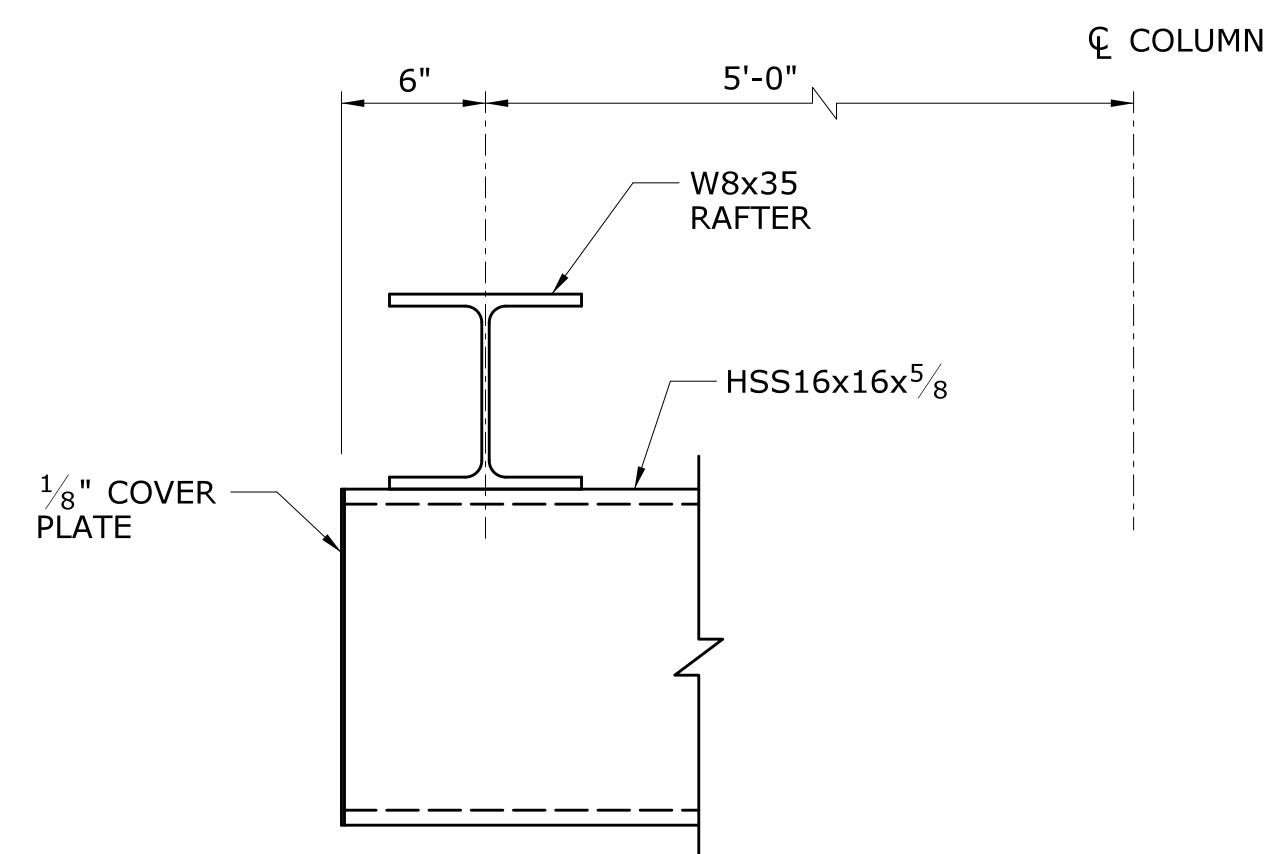
[illegible]



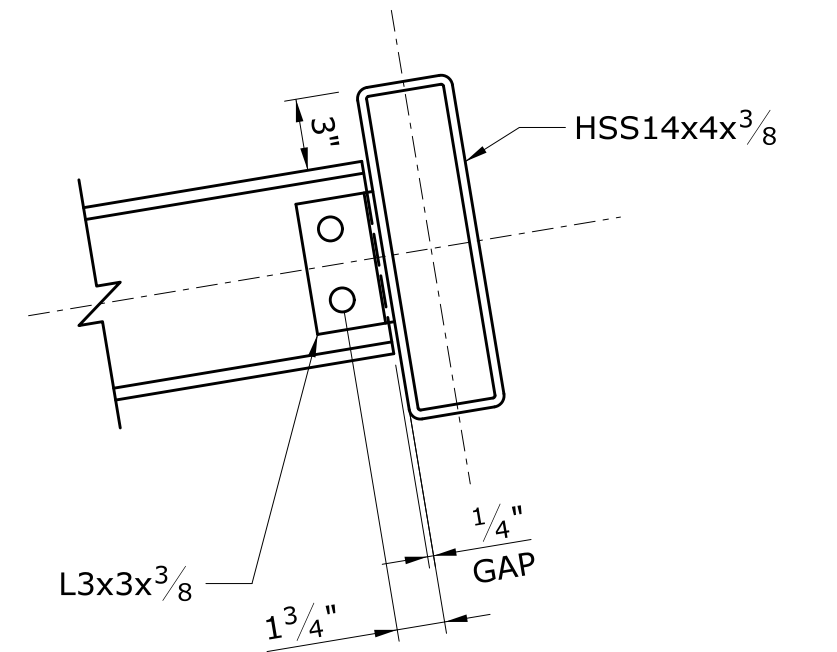
1
FST-301
SECTION
SCALE: 1 1/2" = 1'-0"



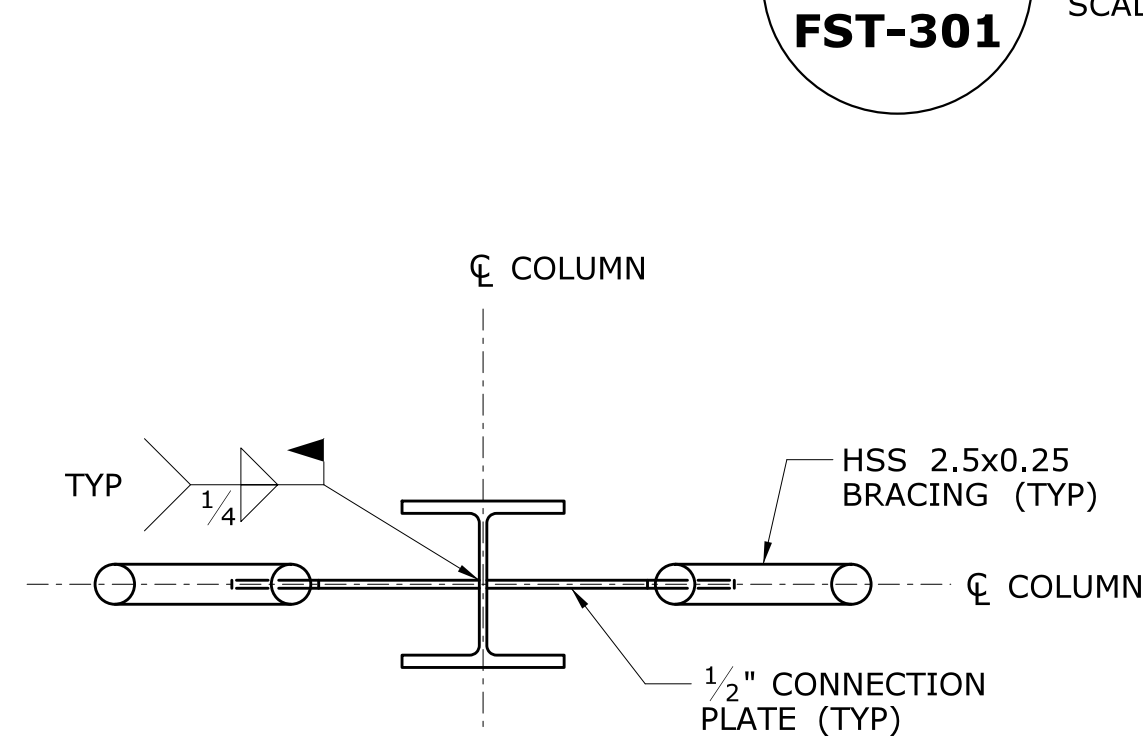
2
FST-301
SECTION
SCALE: 1 1/2" = 1'-0"



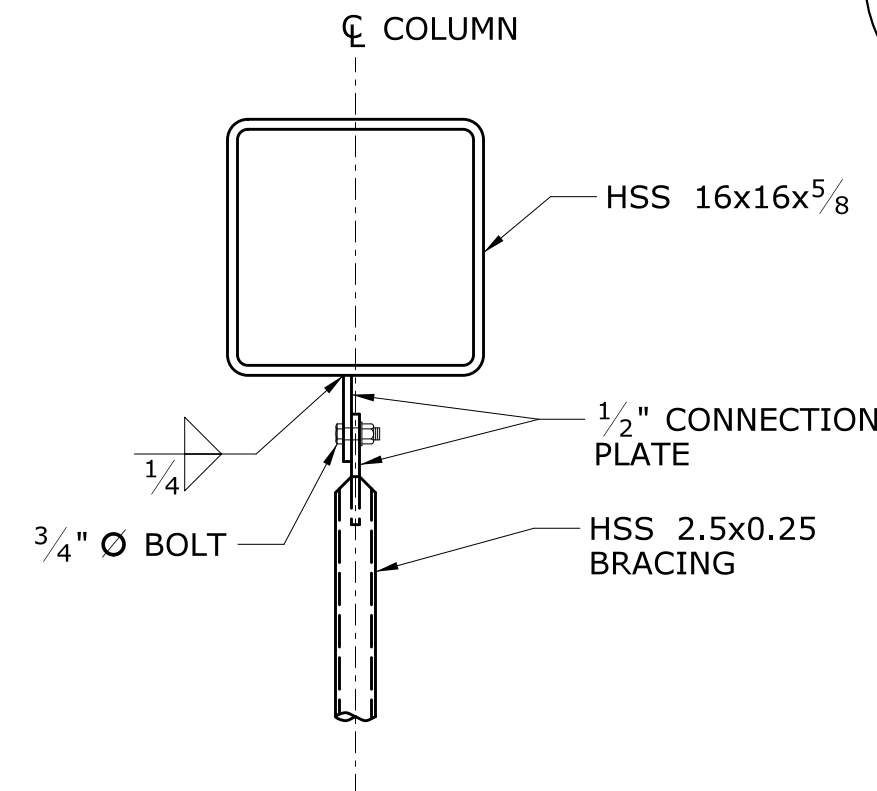
3
FST-111
SECTION
SCALE: 1 1/2" = 1'-0"



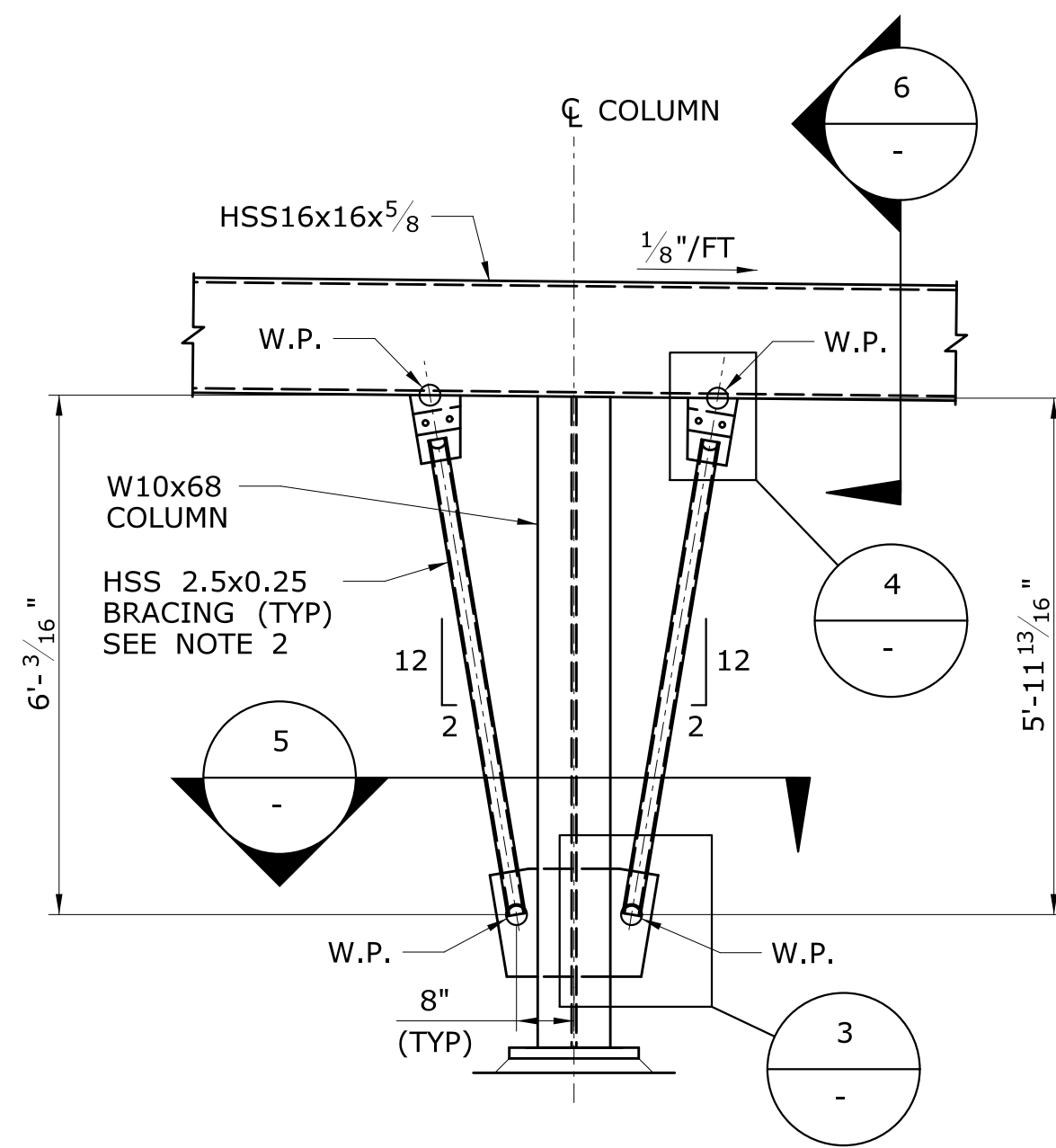
1
FST-301
DETAIL
SCALE: 1 1/2" = 1'-0"



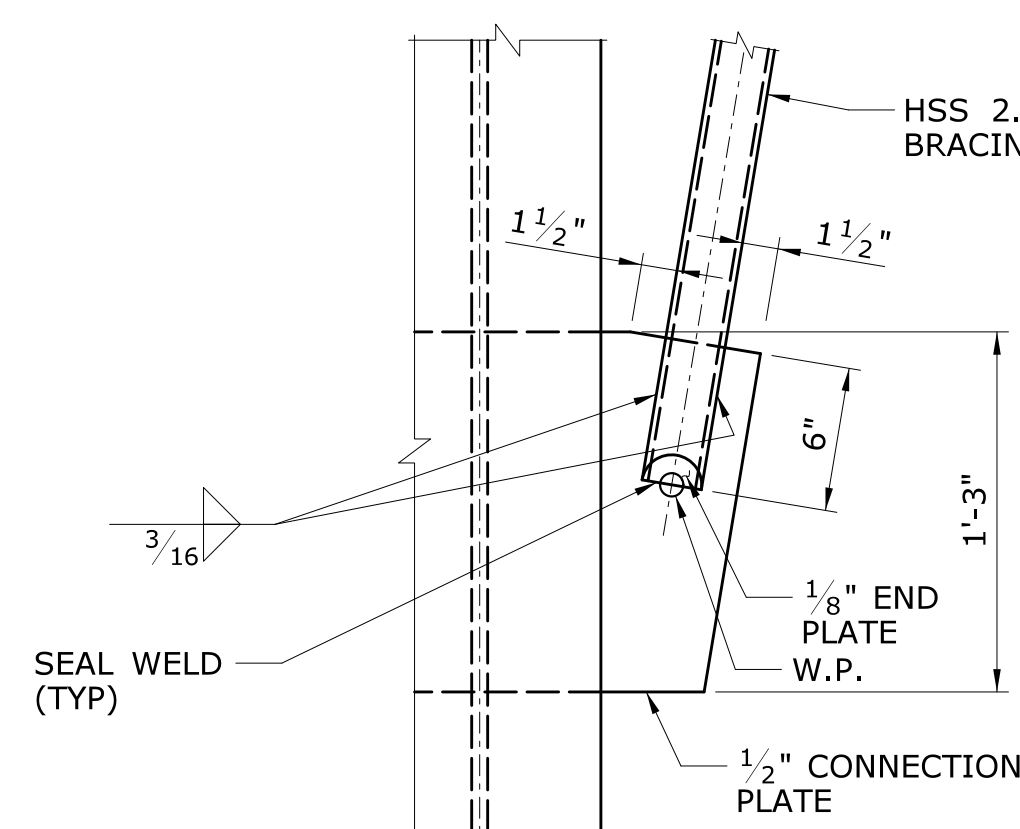
5
SECTION
SCALE: 1" = 1'-0"



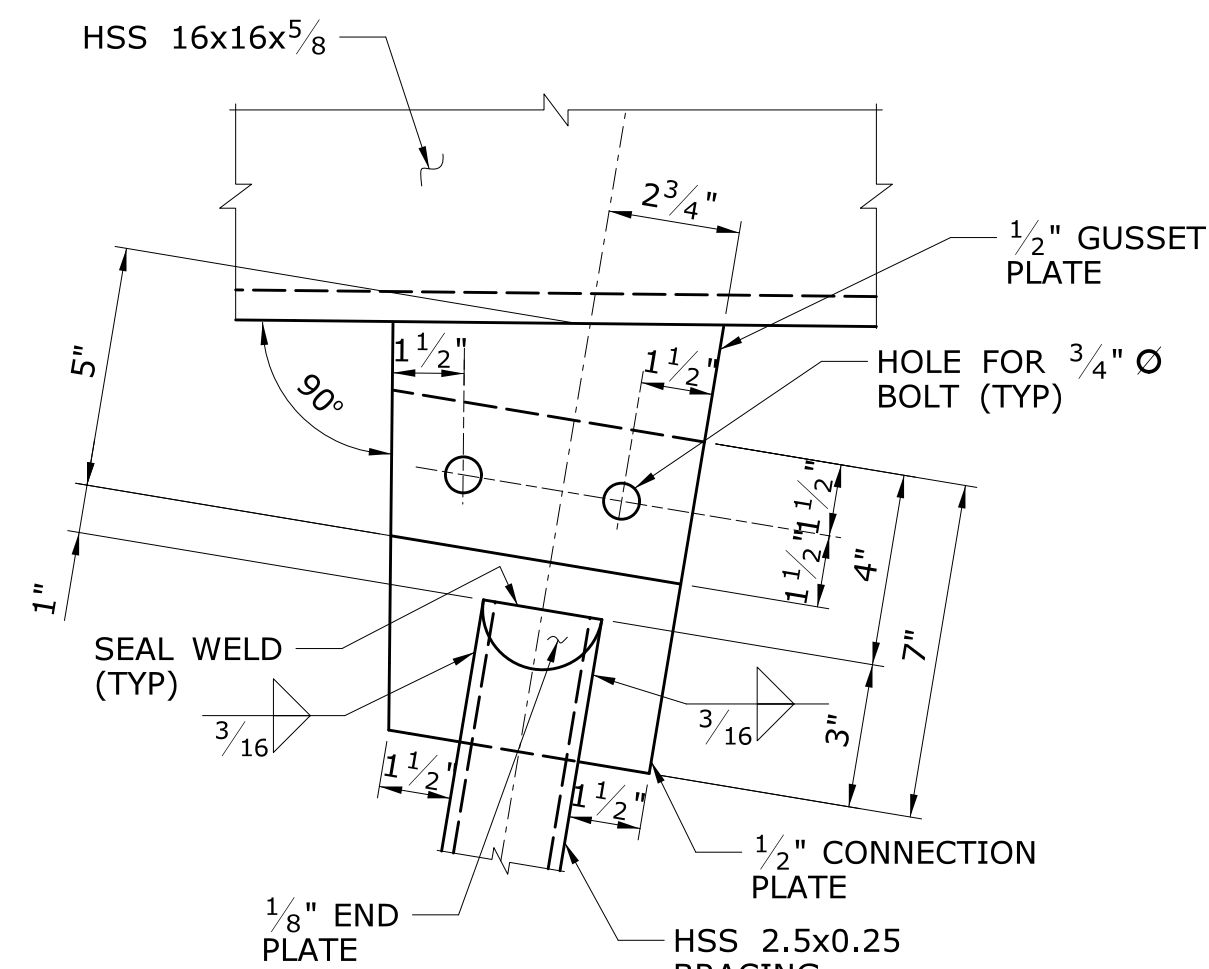
6
SECTION
SCALE: 1" = 1'-0"



4
FST-111
VIEW
SCALE: 1 1/2" = 1'-0"



3
DETAIL
SCALE: 1 1/2" = 1'-0"



4
DETAIL
SCALE: 3" = 1'-0"

| | | | |
|------|------|----------------------|-----------|
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
| - | - | - | - |
| - | - | - | - |
| - | - | - | - |
| - | - | - | - |
| - | - | - | - |
| - | - | - | - |
| - | - | - | - |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

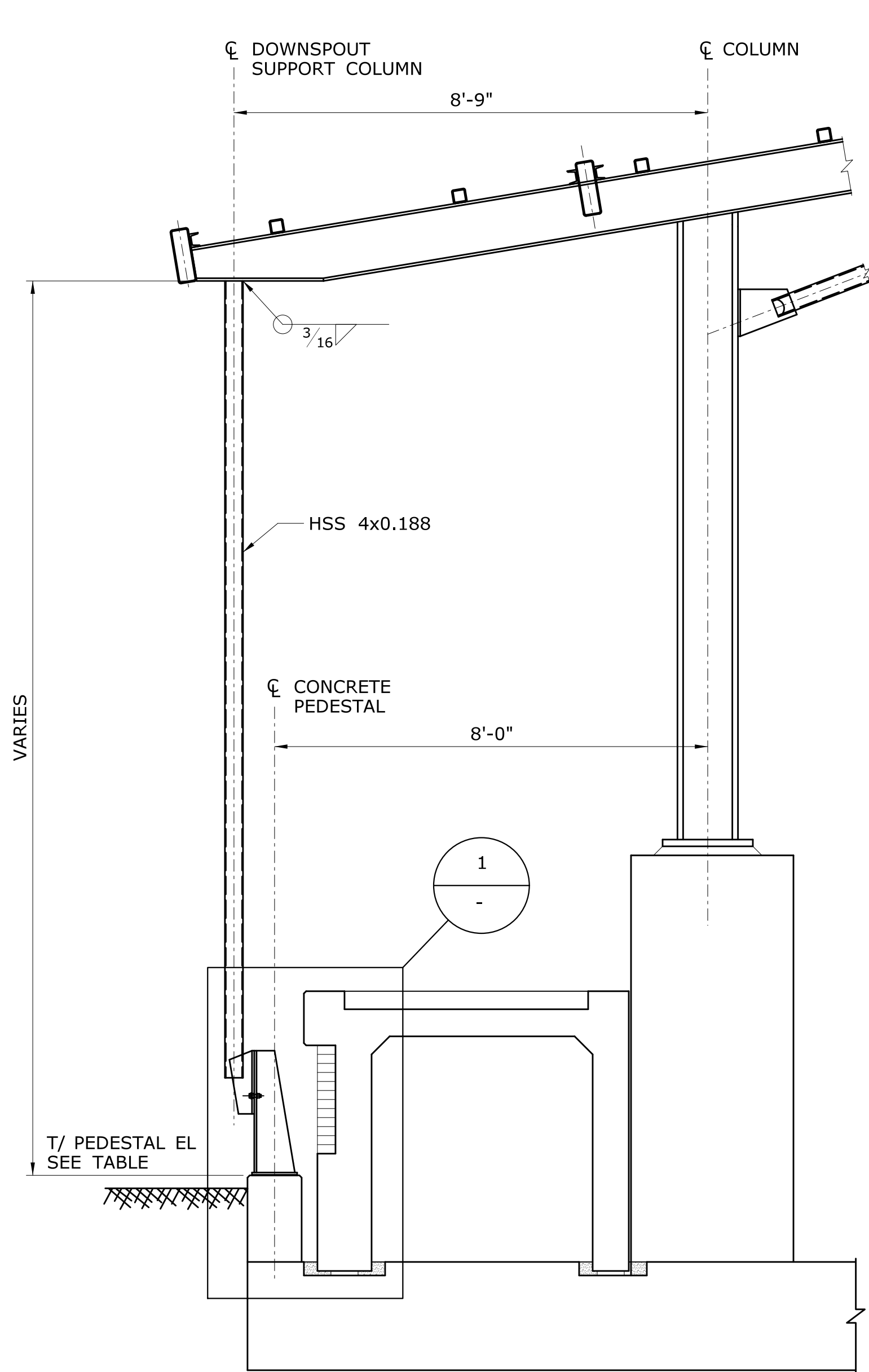
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_305.dgn

SIGNATURE/BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**CANOPY SECTIONS
& DETAILS 6**

PROJECT NO.
170-3155
DRAWING NO.
FST-305
SHEET NO.
01.12.047

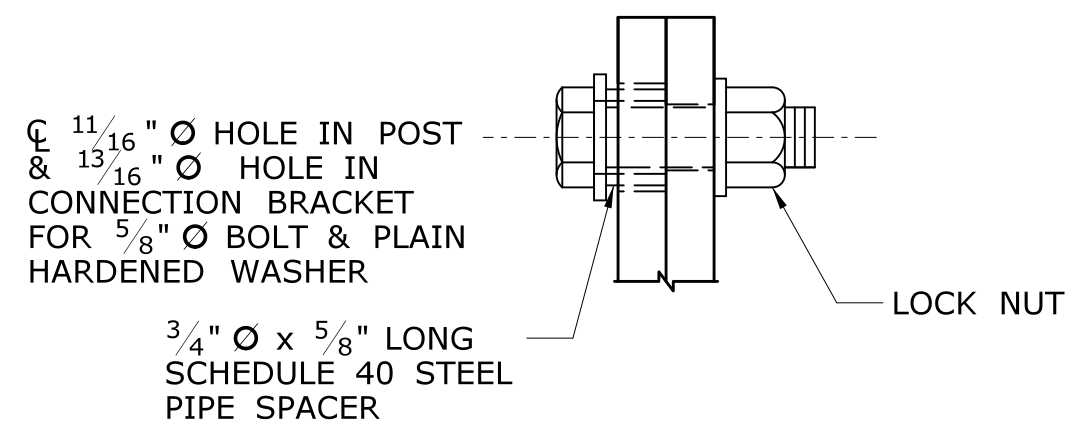


NOTE:
DOWNSPOUT SUPPORT COLUMN AT RAMP
LOCATION SHOWN. OTHER LOCATIONS
ARE SIMILAR.

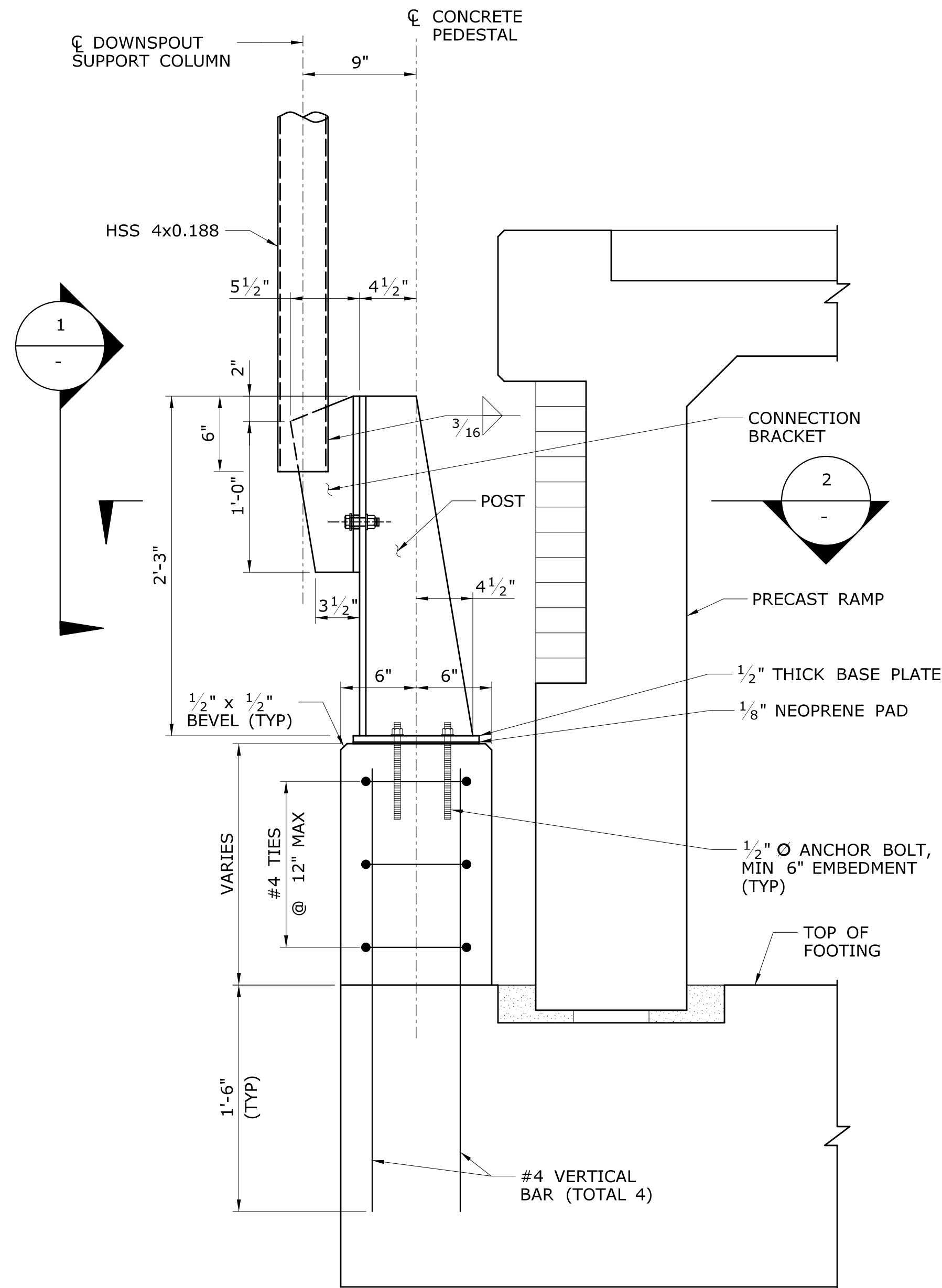
DOWNSPOUT SUPPORT COLUMN ELEVATION

SCALE: 1/2" = 1'-0"

| ELEVATION TABLE | | | |
|-----------------|----------------------------|-------------|----------------------------|
| COLUMN LINE | T/ PEDESTAL ELEVATION (FT) | COLUMN LINE | T/ PEDESTAL ELEVATION (FT) |
| 1E | 61.30 | 14W | 61.50 |
| 3E | 61.80 | 19W | 61.50 |
| 5E | 62.00 | 21W | 61.00 |
| 18E | 61.40 | 22W | 60.70 |
| 20E | 60.70 | 24W | 60.70 |

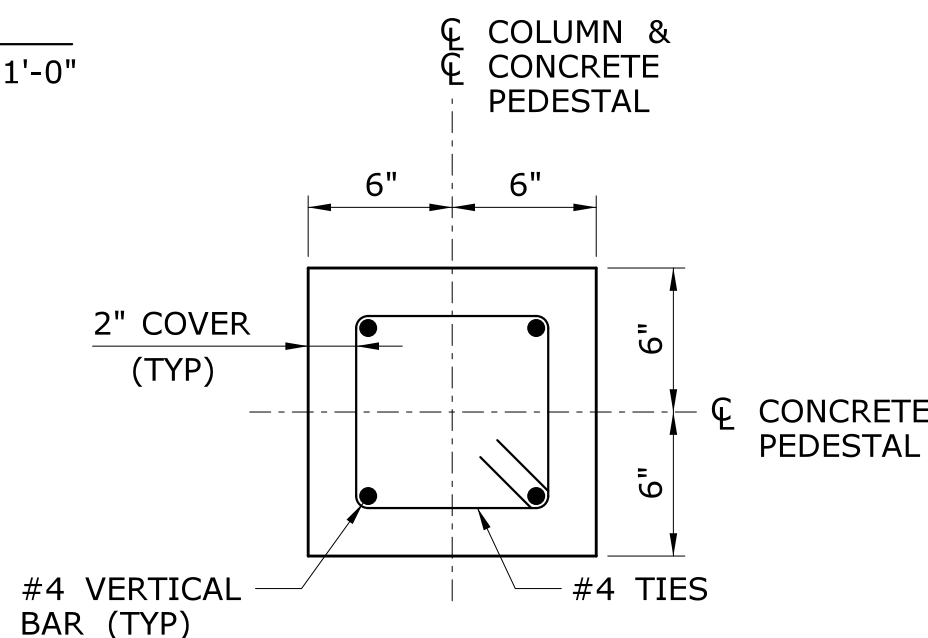


2
-
DETAIL
SCALE: 6" = 1'-0"

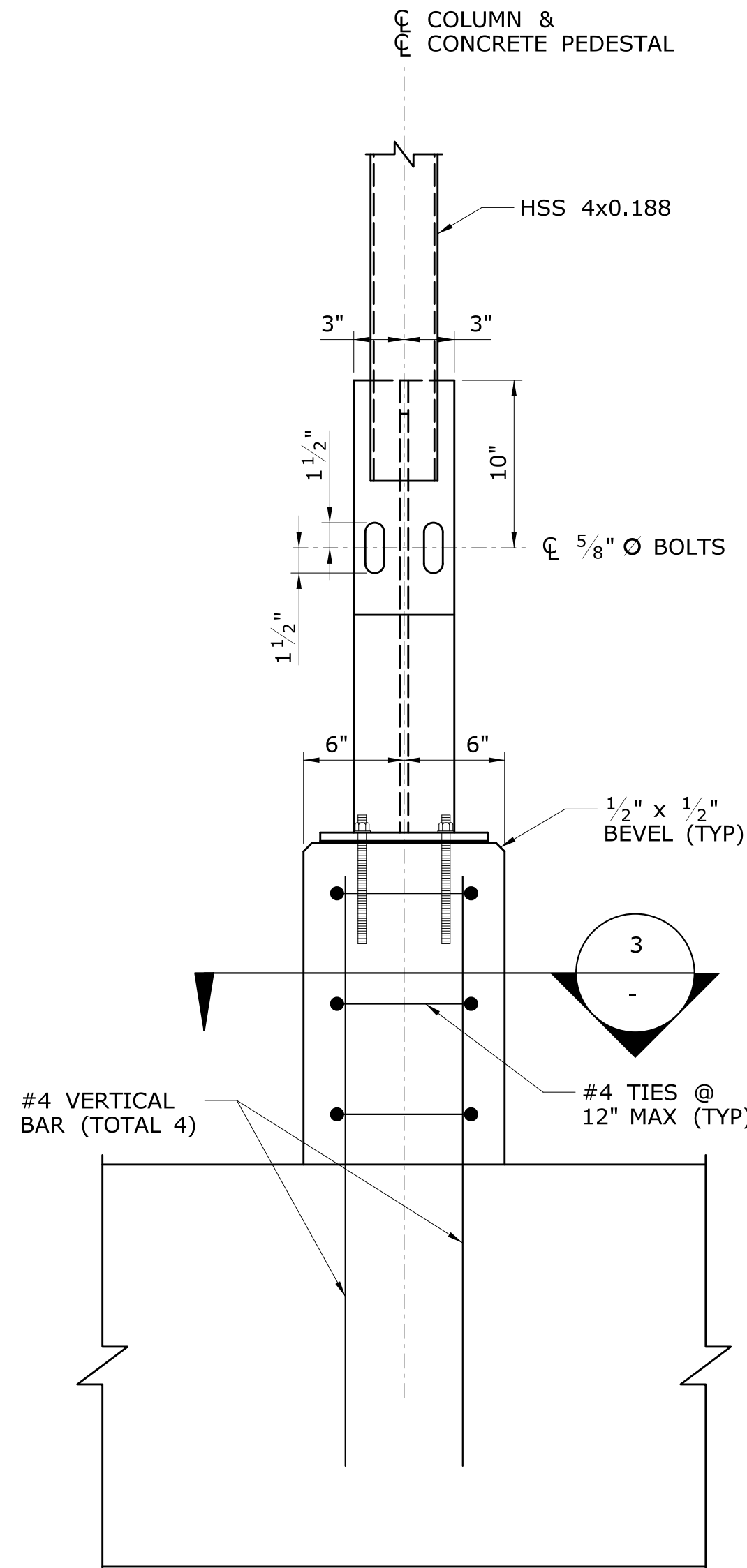


NOTE:
FOOTING REINFORCEMENT
NOT SHOWN FOR CLARITY.

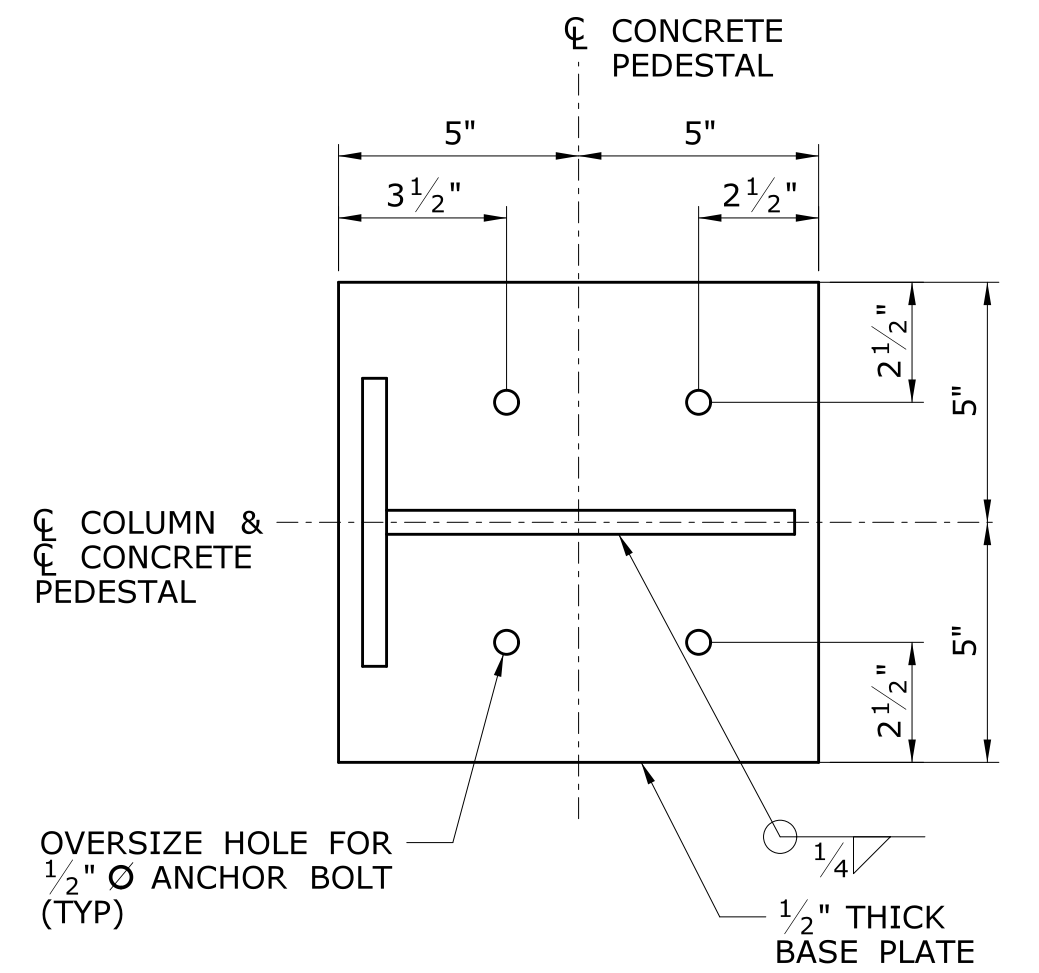
1
-
DETAIL
SCALE: 1 1/2" = 1'-0"



3
-
SECTION
SCALE: 1 1/2" = 1'-0"

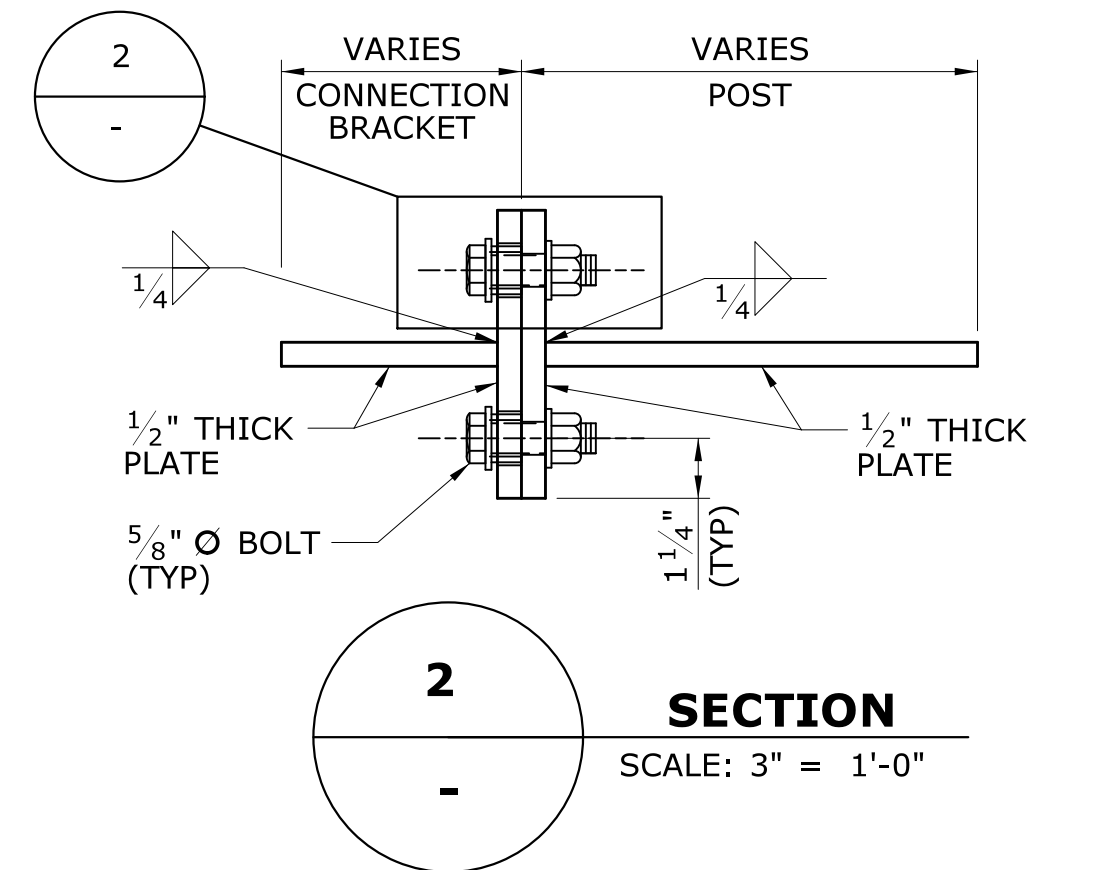


1
-
VIEW
SCALE: 1 1/2" = 1'-0"



BASE PLATE DETAIL

SCALE: 3" = 1'-0"



2
-
SECTION
SCALE: 3" = 1'-0"

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED
QUANTITIES OF WORK, SHOWN ON THESE
SHEETS IS BASED ON LIMITED
INVESTIGATIONS BY THE STATE AND IS
IN NO WAY WARRANTED TO INDICATE
THE CONDITIONS OF ACTUAL QUANTITIES
OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

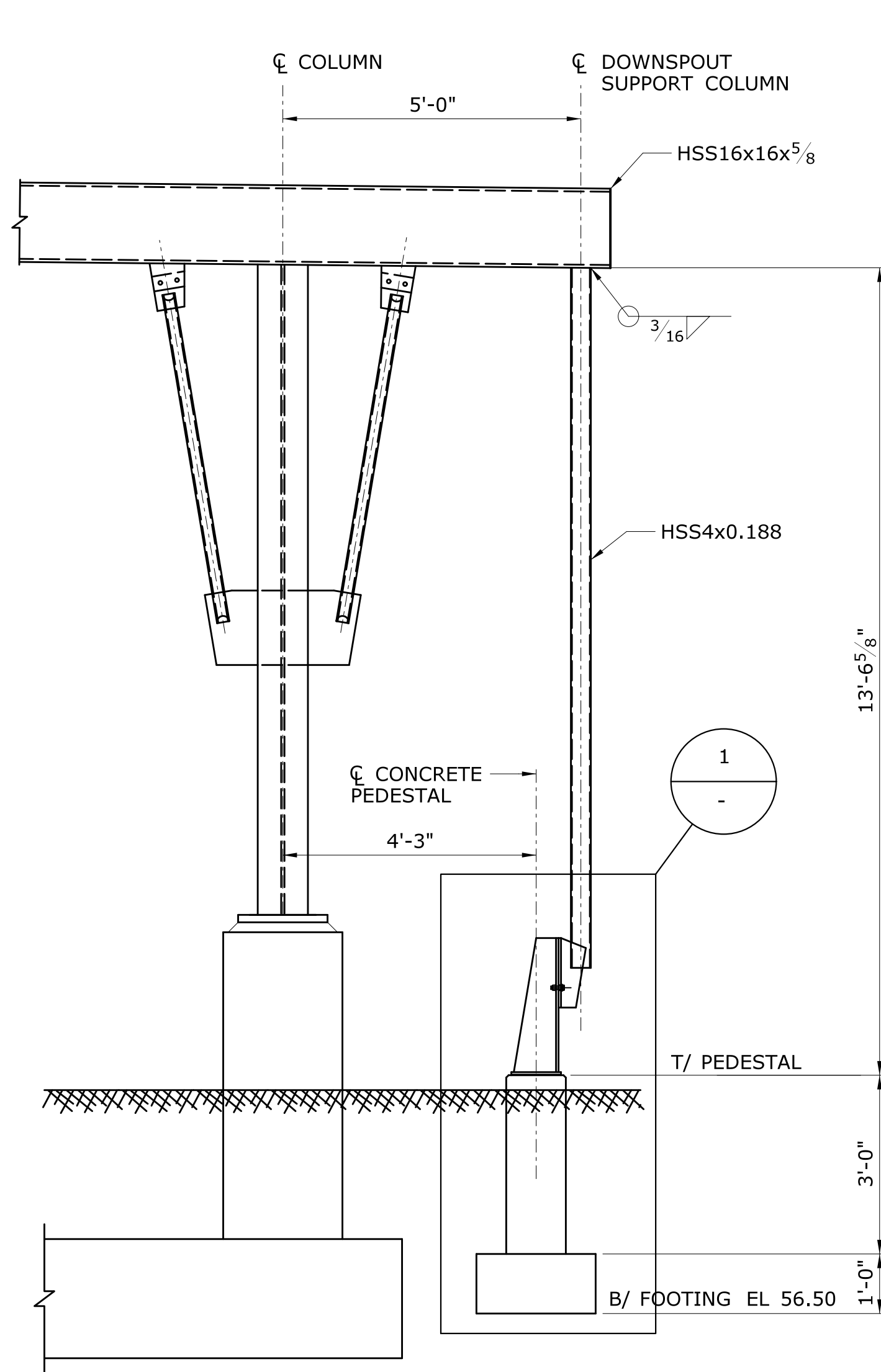
DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_306.dgn

SIGNATURE/
BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

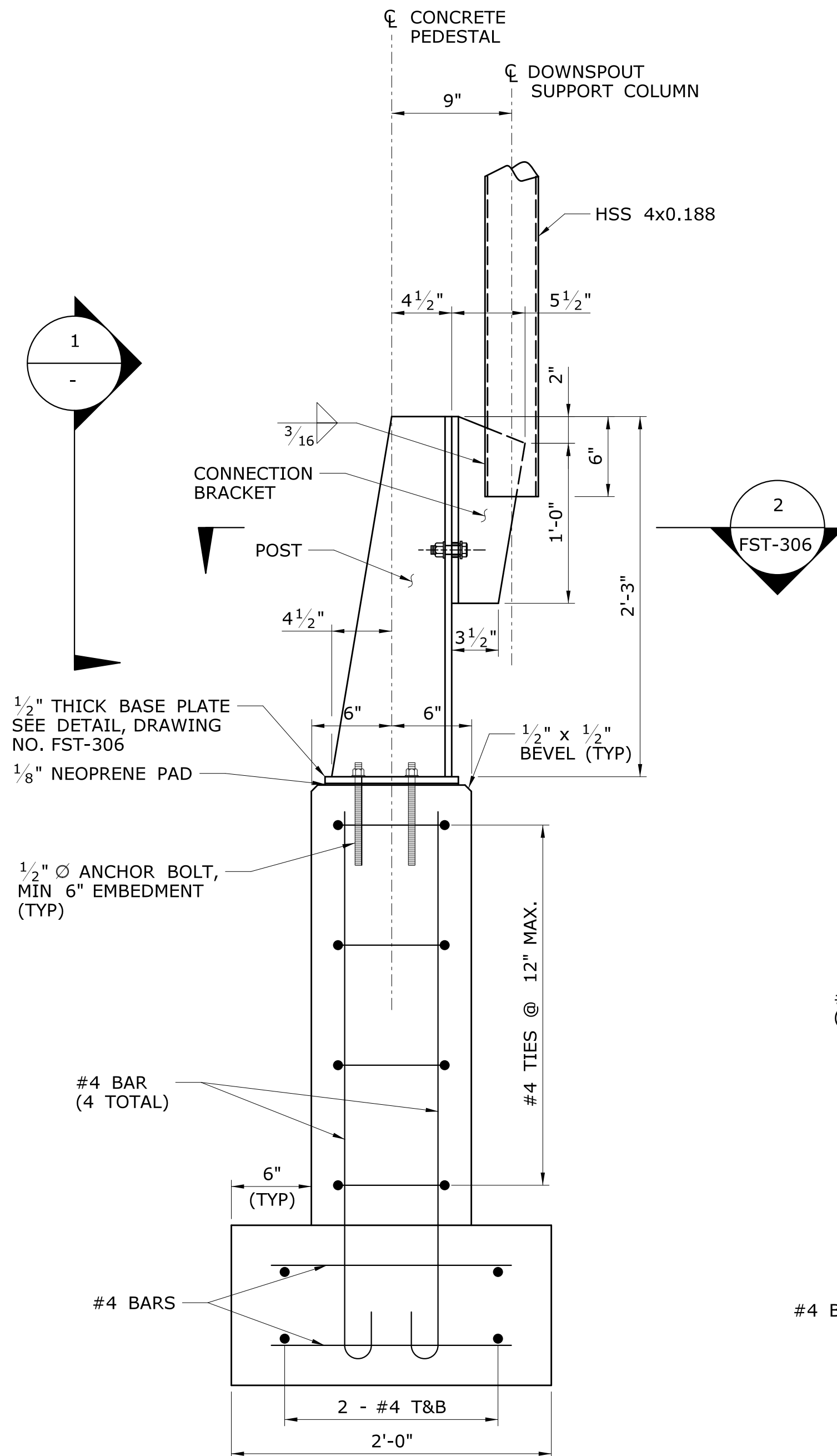
PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**CANOPY SECTIONS
& DETAILS 7**
PROJECT NO.
170-3155
DRAWING NO.
FST-306
SHEET NO.
01.12.048

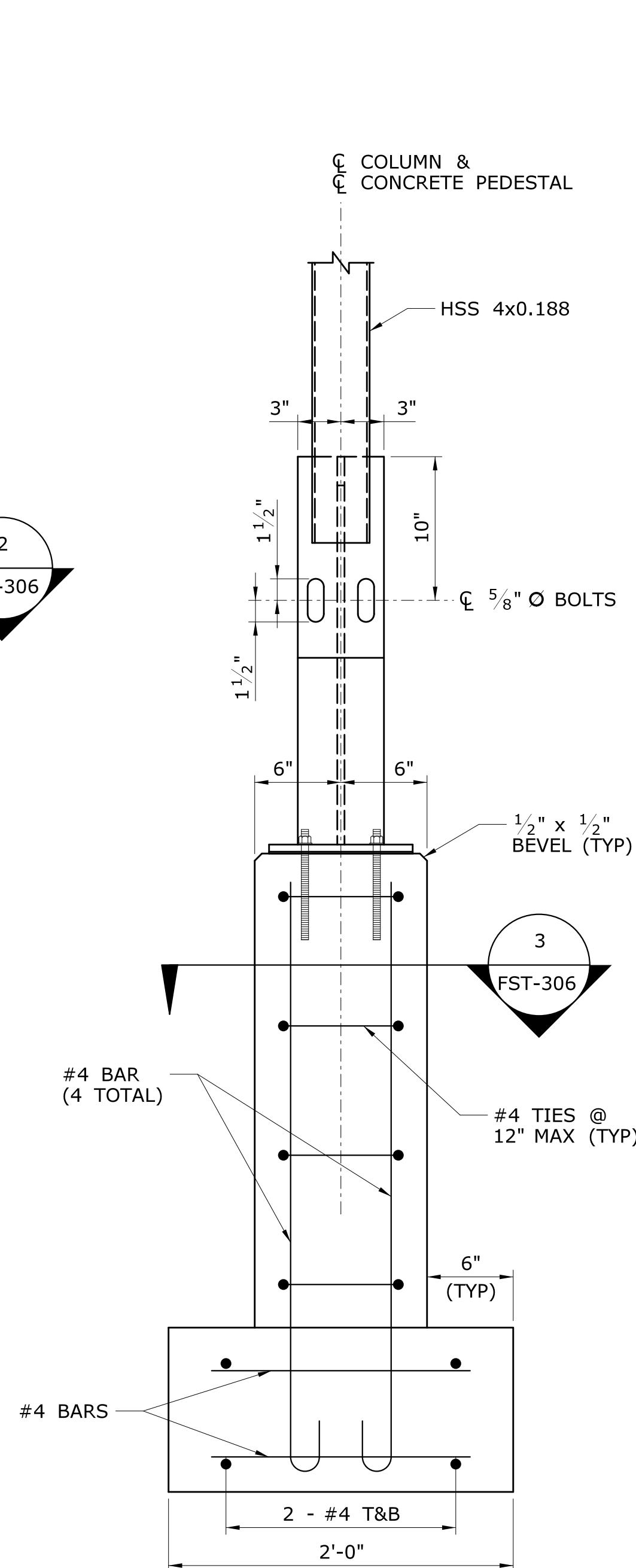


**DOWNSPOUT SUPPORT COLUMN ELEVATION
(WITH INDEPENDENT FOUNDATION)**

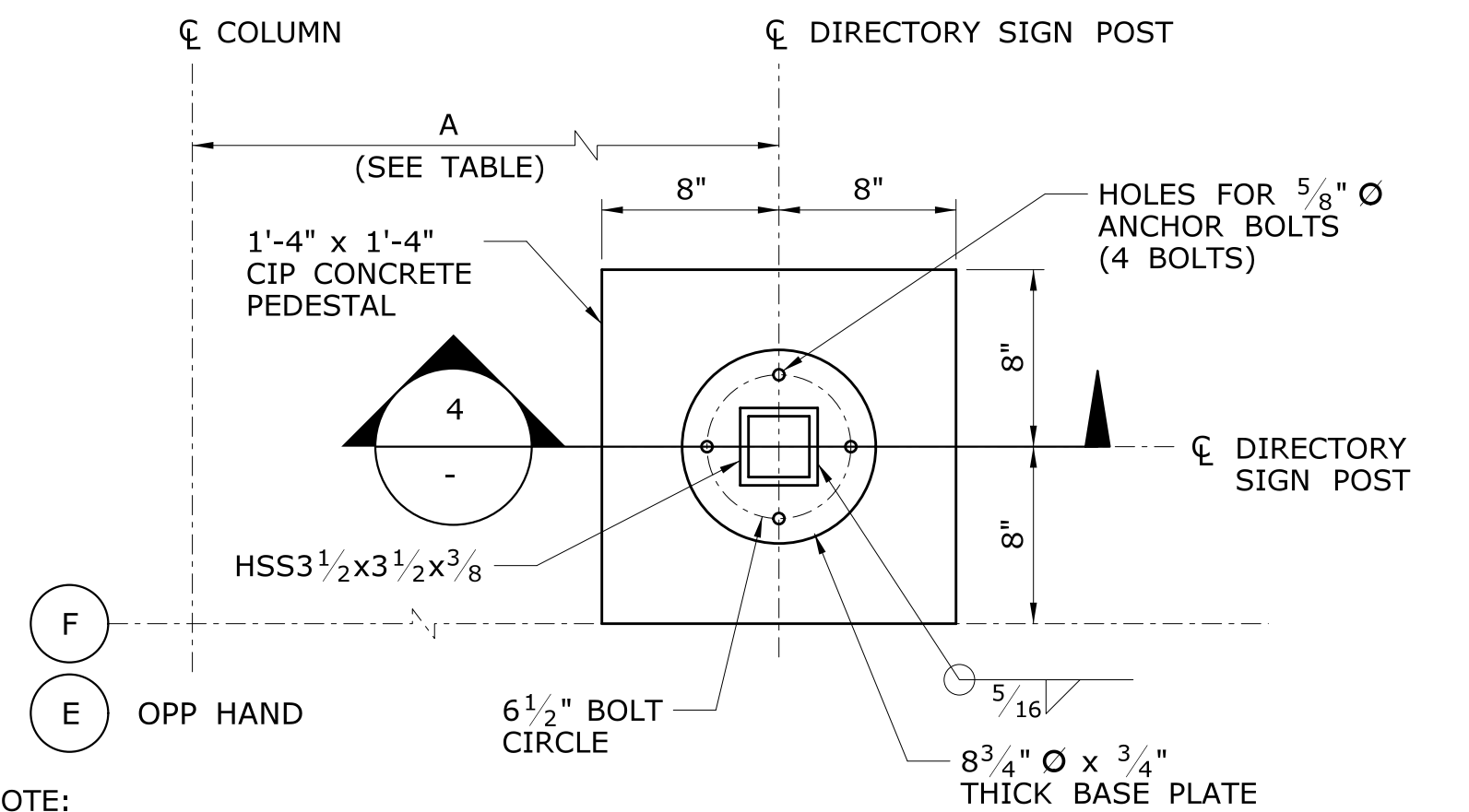
SCALE: 1/2" = 1'-0"



**1
-** **DETAIL**
SCALE: 1 1/2" = 1'-0"



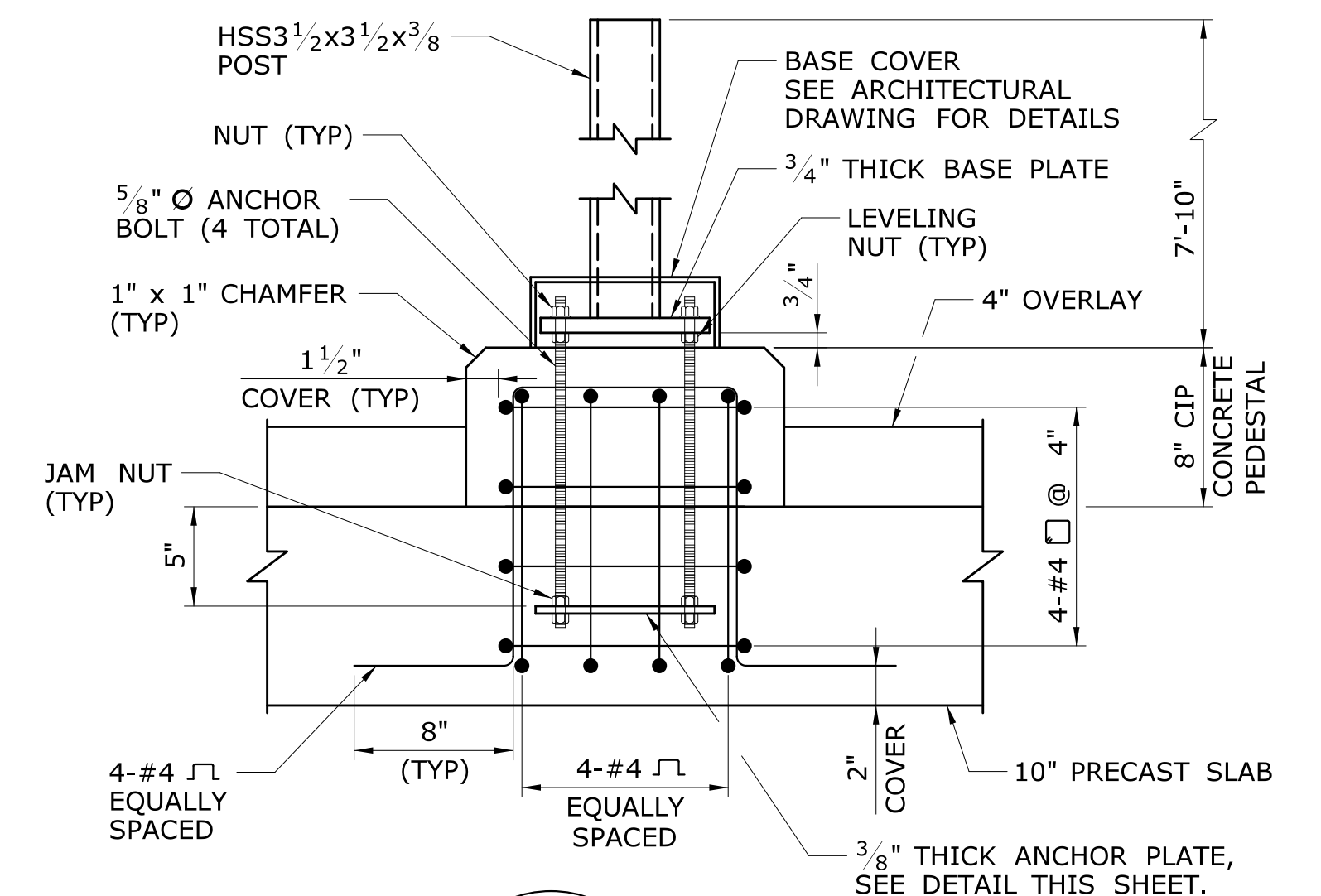
**1
-** **VIEW**
SCALE: 1 1/2" = 1'-0"



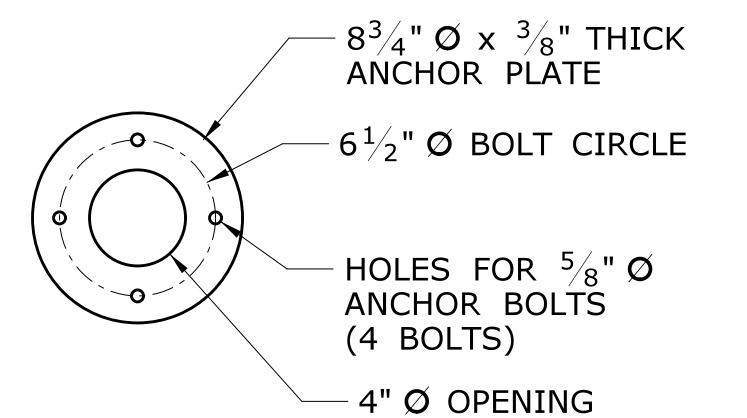
NOTE:
THE CONTRACTOR SHALL COORDINATE
FINAL LOCATIONS OF A11 DIRECTORY
SIGN PEDESTALS PRIOR TO FABRICATION
OF PRECAST PLATFORM PANELS.

**PLAN
DIRECTORY SIGN SUPPORT DETAIL**

SCALE: 1 1/2" = 1'-0"



**4
-** **SECTION**
SCALE: 1 1/2" = 1'-0"



ANCHOR PLATE DETAIL

SCALE: 1 1/2" = 1'-0"

| DIRECTORY SIGN PEDESTAL LOCATION TABLE | |
|--|--------------|
| COLUMN LINE | DISTANCE "A" |
| 1E | 10'-0" |
| 1E | 17'-8" |
| 2E | 6'-2" |
| 2E | 13'-10" |
| 11E | 6'-2" |
| 11E | 13'-10" |
| 12W | 6'-2" |
| 12W | 13'-10" |
| 21W | 6'-2" |
| 21W | 13'-10" |

| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

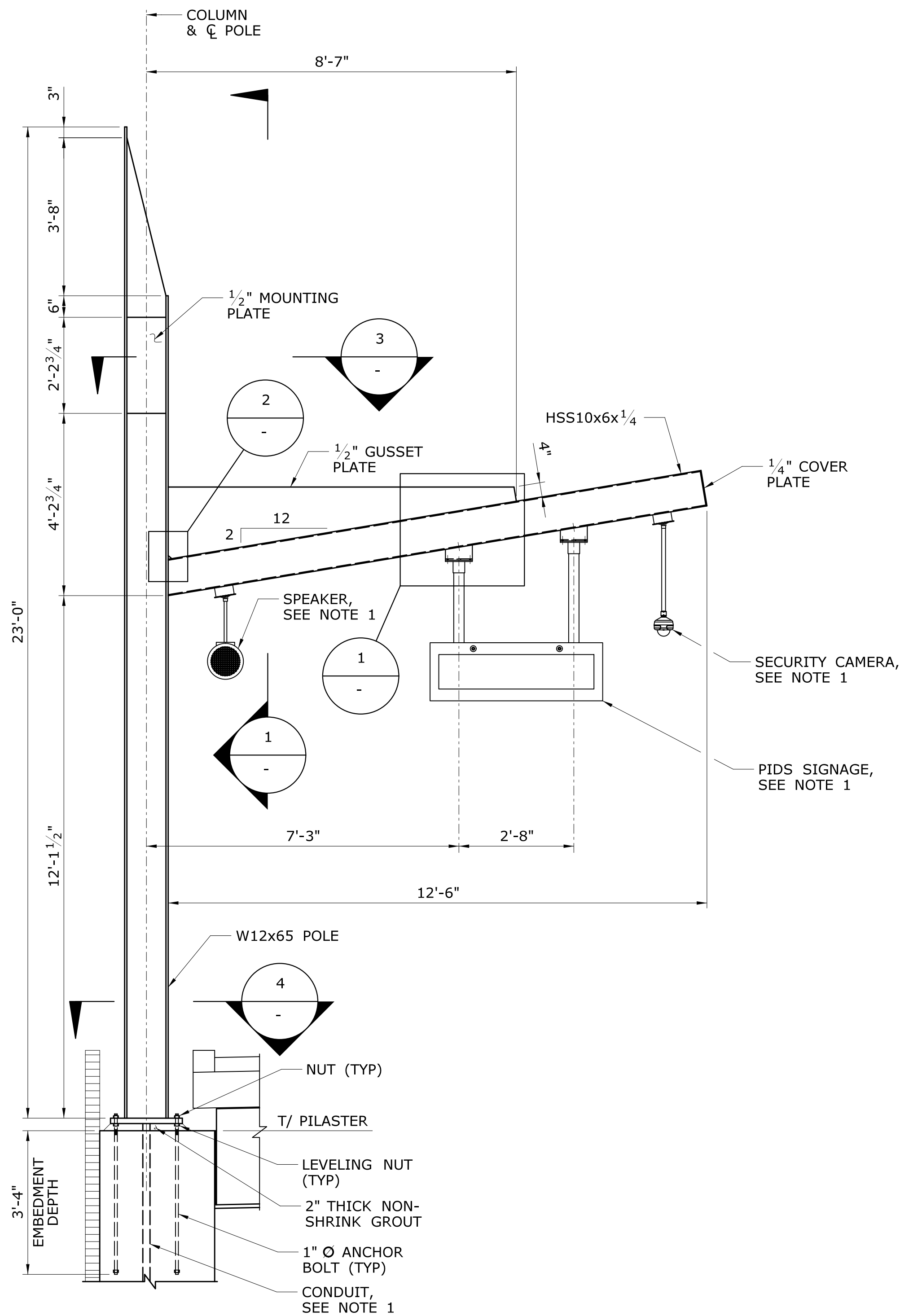
**STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION**
Filename: ...FA_CGR_CPS_0170-2296_007_04_FST_307.dgn

SIGNATURE/
BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

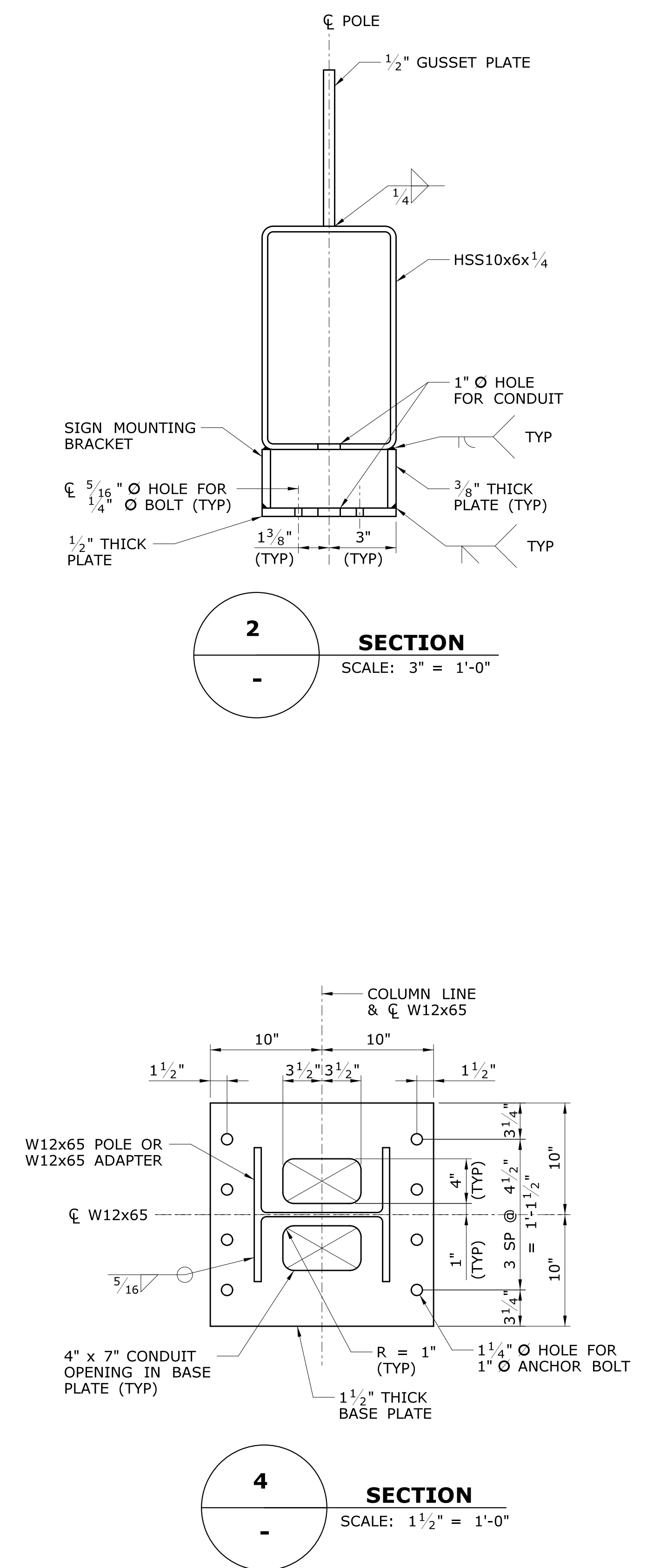
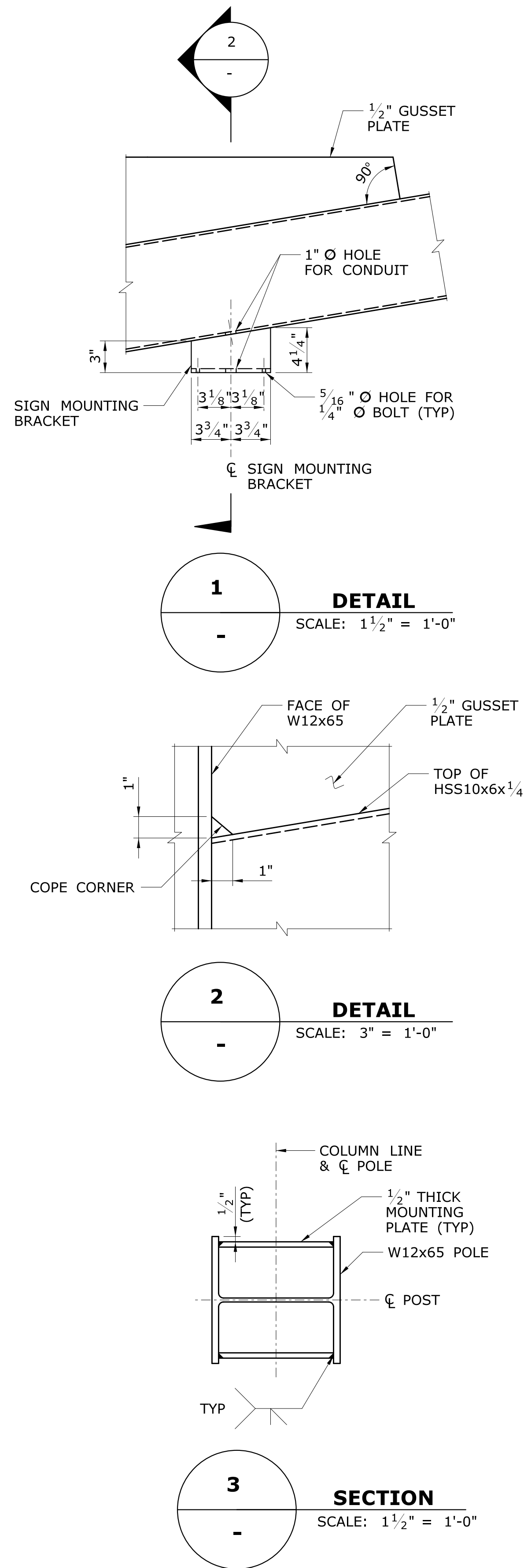
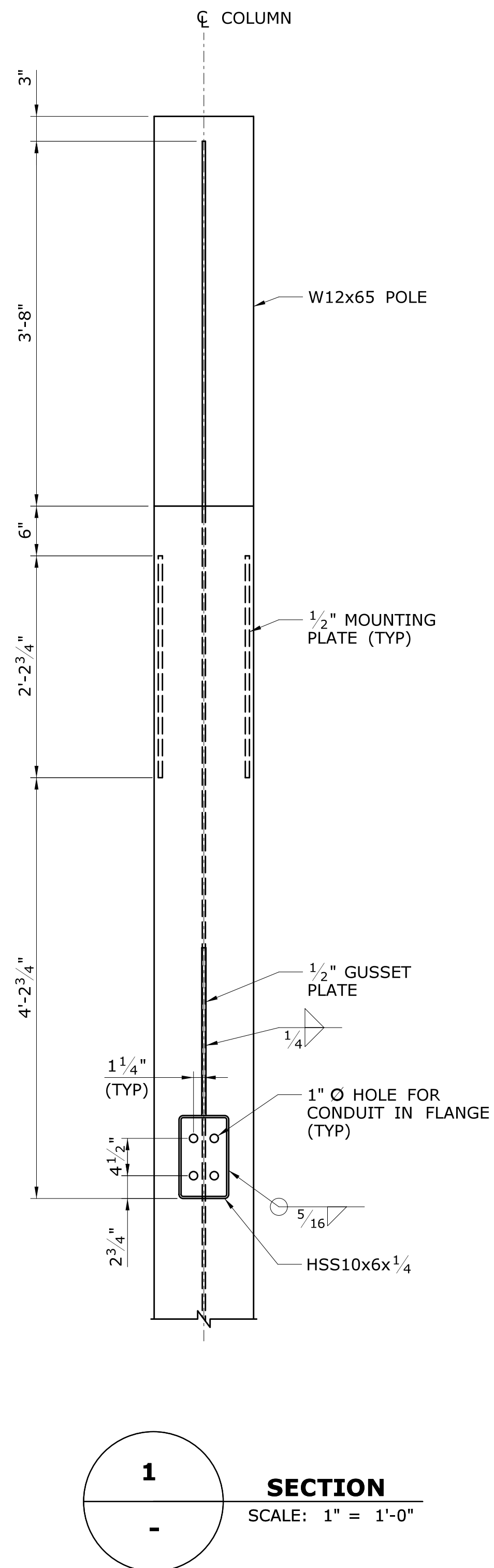
PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**CANOPY SECTIONS
& DETAILS 8**

PROJECT NO.
170-3155
DRAWING NO.
FST-307
SHEET NO.
01.12.049



PLATFORM STEEL POLE ELEVATION
SCALE: $\frac{1}{2}$ " = 1'-0"



| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
|------|------|----------------------|-----------|
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
C DONOHUE
CHECKED BY:
H BUI
SCALE AS NOTED

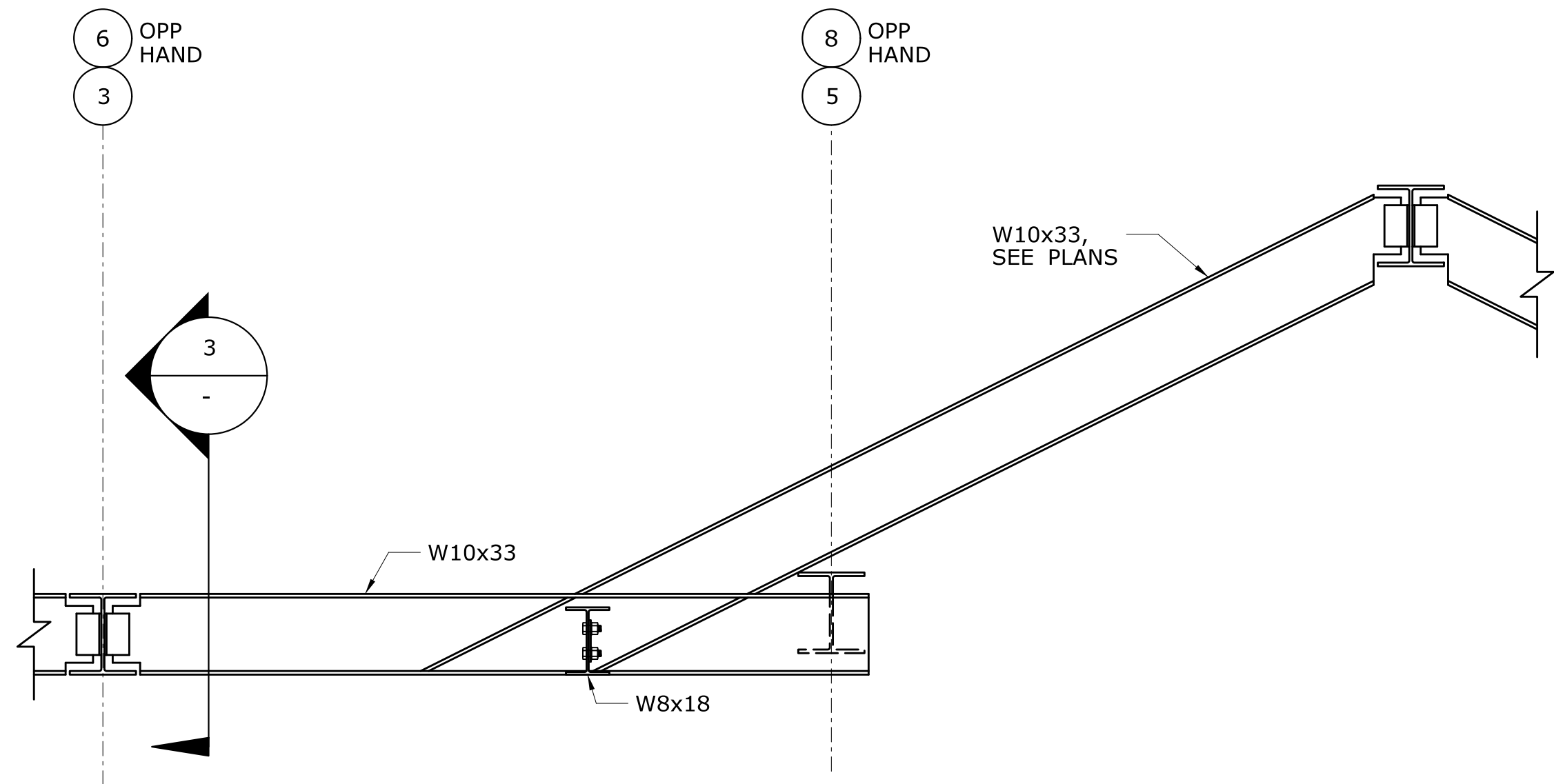
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_308.dgn

SIGNATURE/BLOCK:
TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

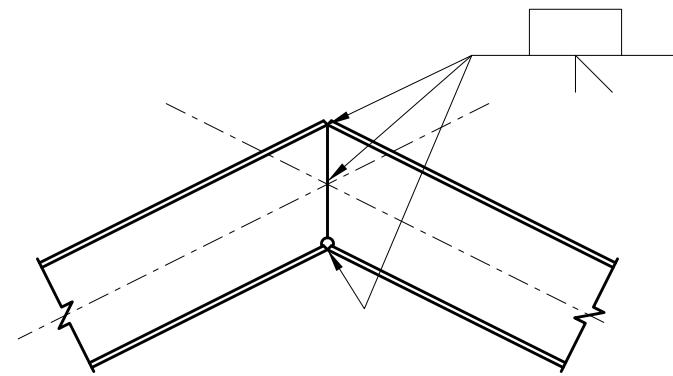
PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**PLATFORM MISC.
STEEL DETAILS 1**
PROJECT NO.
170-3155
DRAWING NO.
FST-308
SHEET NO.
01.12.050

[illegible]

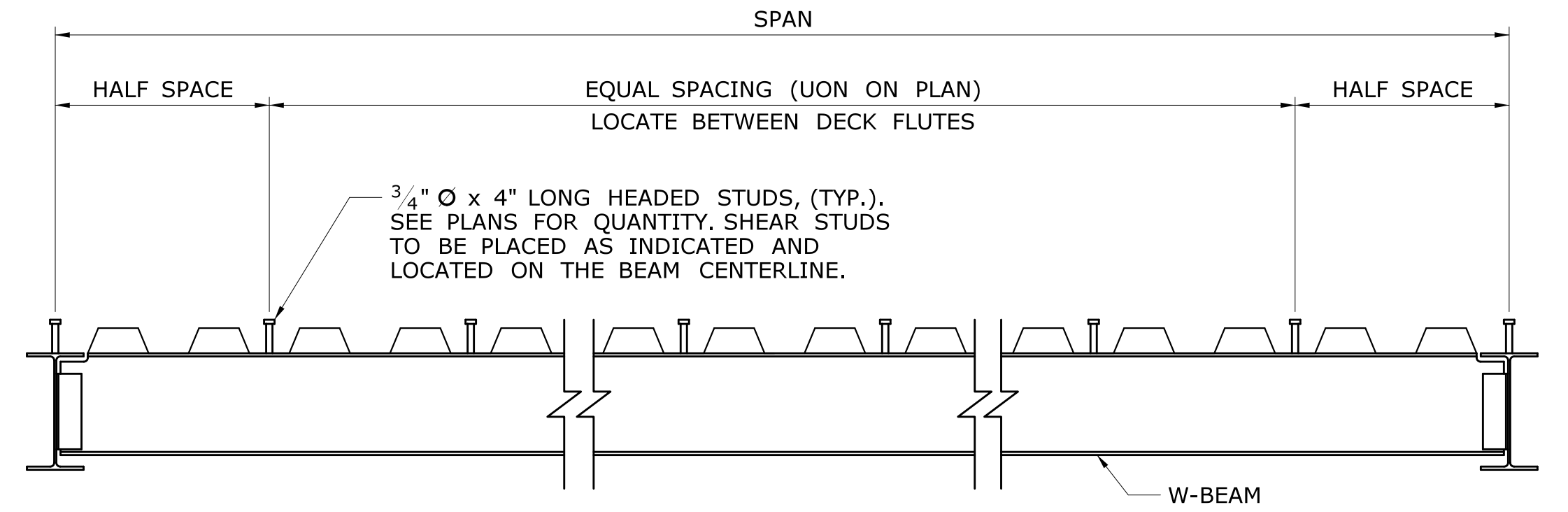


2
FST-114 SECTION
SCALE: $\frac{3}{4}" = 1'-0"$

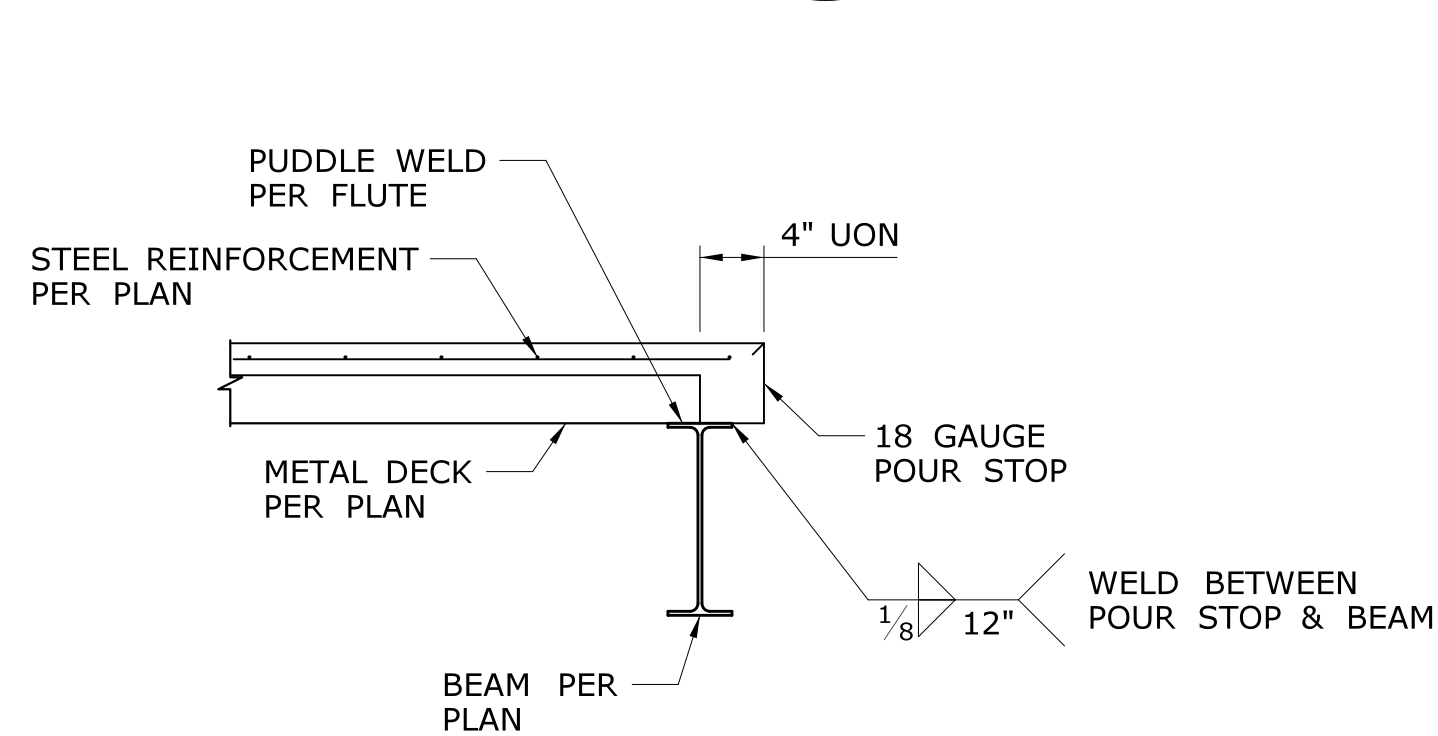


NOTE:
DETAIL SIMILAR FOR ALL
SLOPED ROOF MEMBERS.
W10X33 NOT SHOWN FOR CLARITY.

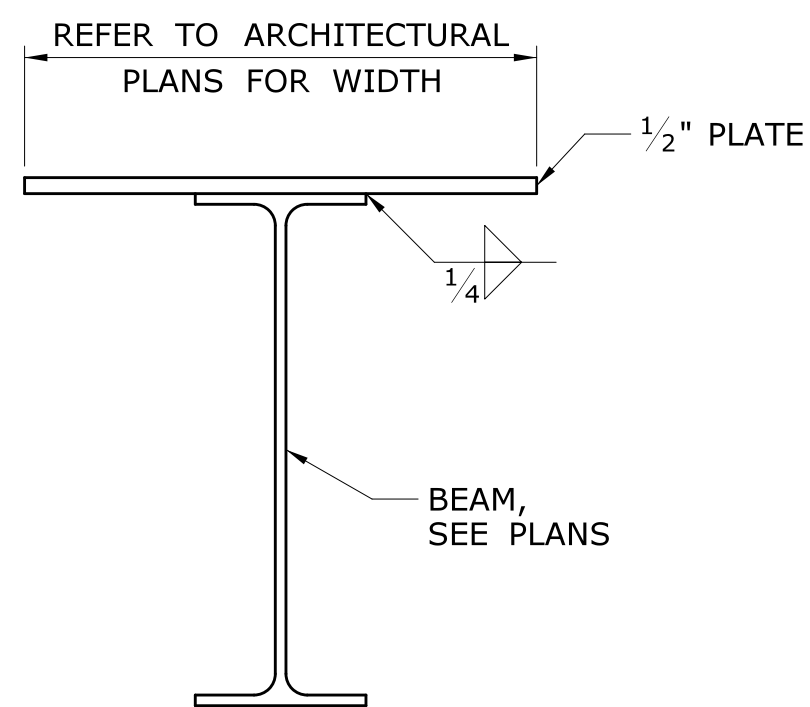
3
SECTION
SCALE: $\frac{3}{4}" = 1'-0"$



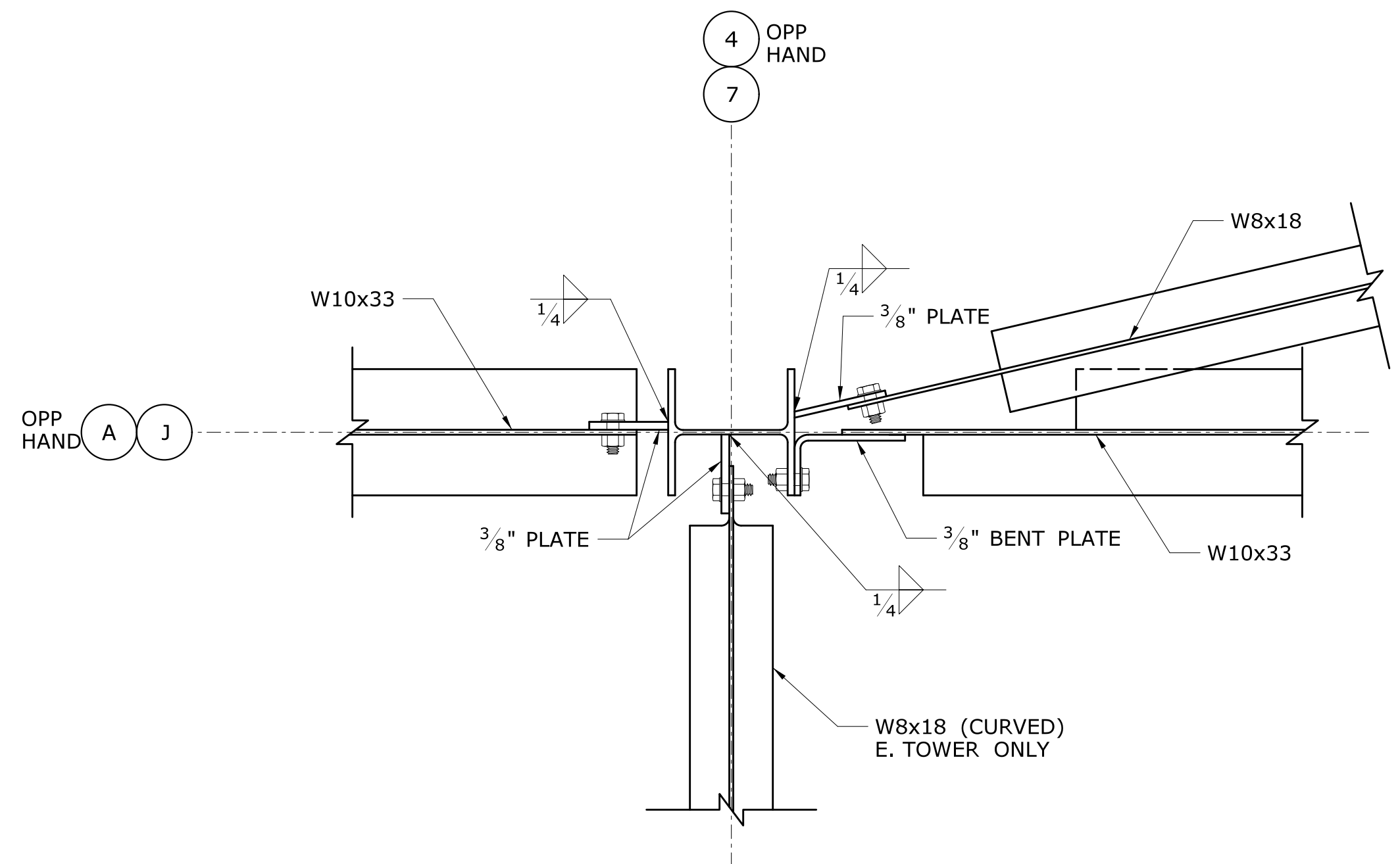
TYPICAL COMPOSITE BEAM DETAIL
SCALE: NTS



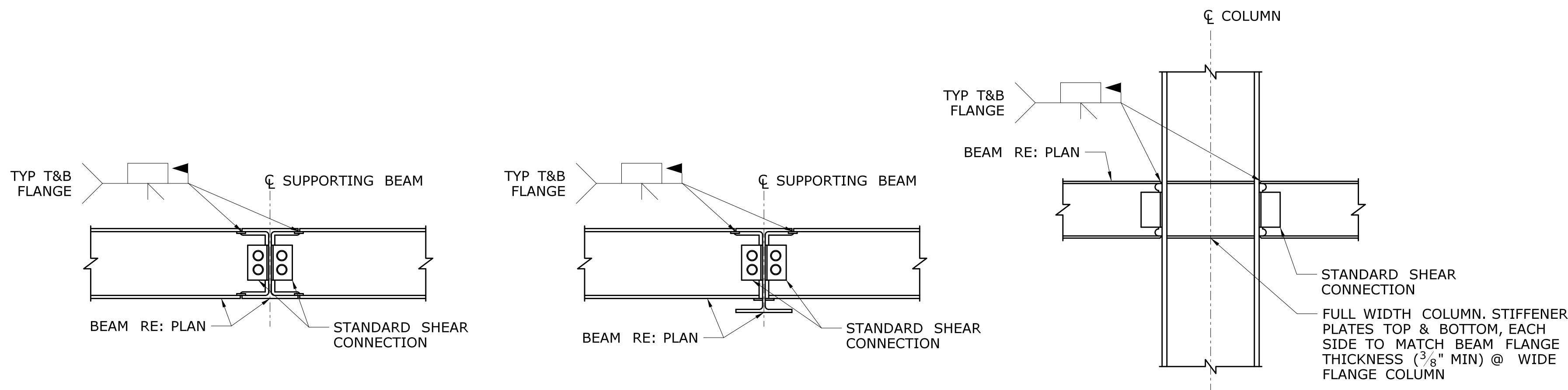
TYPICAL EDGE SLAB DETAIL
SCALE: 1" = 1'-0"



TYPICAL BEAM SUPPORTING MASONRY
SCALE: 1" = 1'-0"



4
FST-114 DETAIL
SCALE: $\frac{3}{4}" = 1'-0"$



TYPICAL MOMENT CONNECTIONS
MOMENT CONNECTION THRU BEAM (BEAMS UNEQUAL DEPTH)
(NOTED AS ON PLANS)
SCALE: NTS

| | | | |
|------|------|----------------------|-----------|
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

DESIGNER/DRAFTER:
J POPOLI
CHECKED BY:
H BUI
SCALE AS NOTED

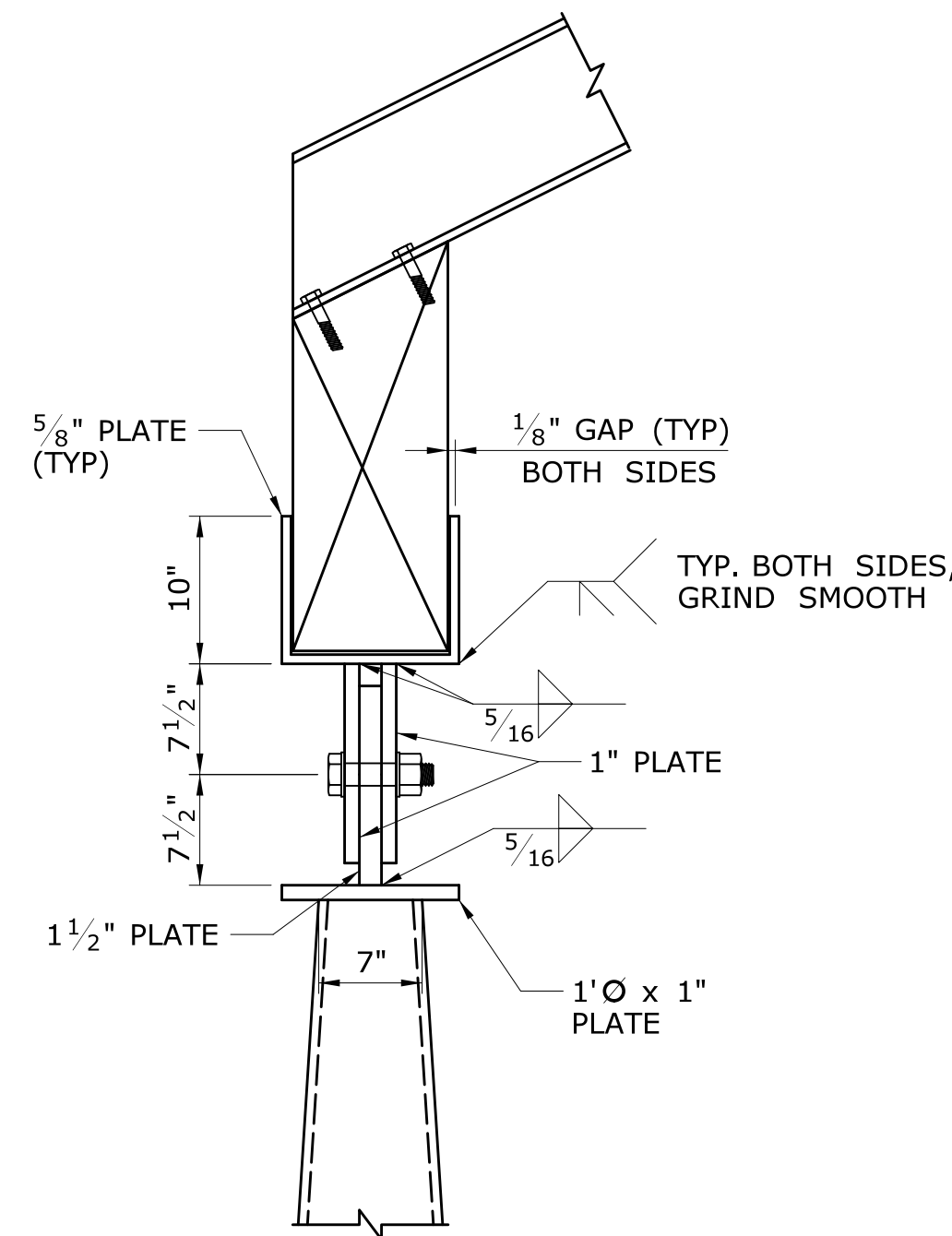
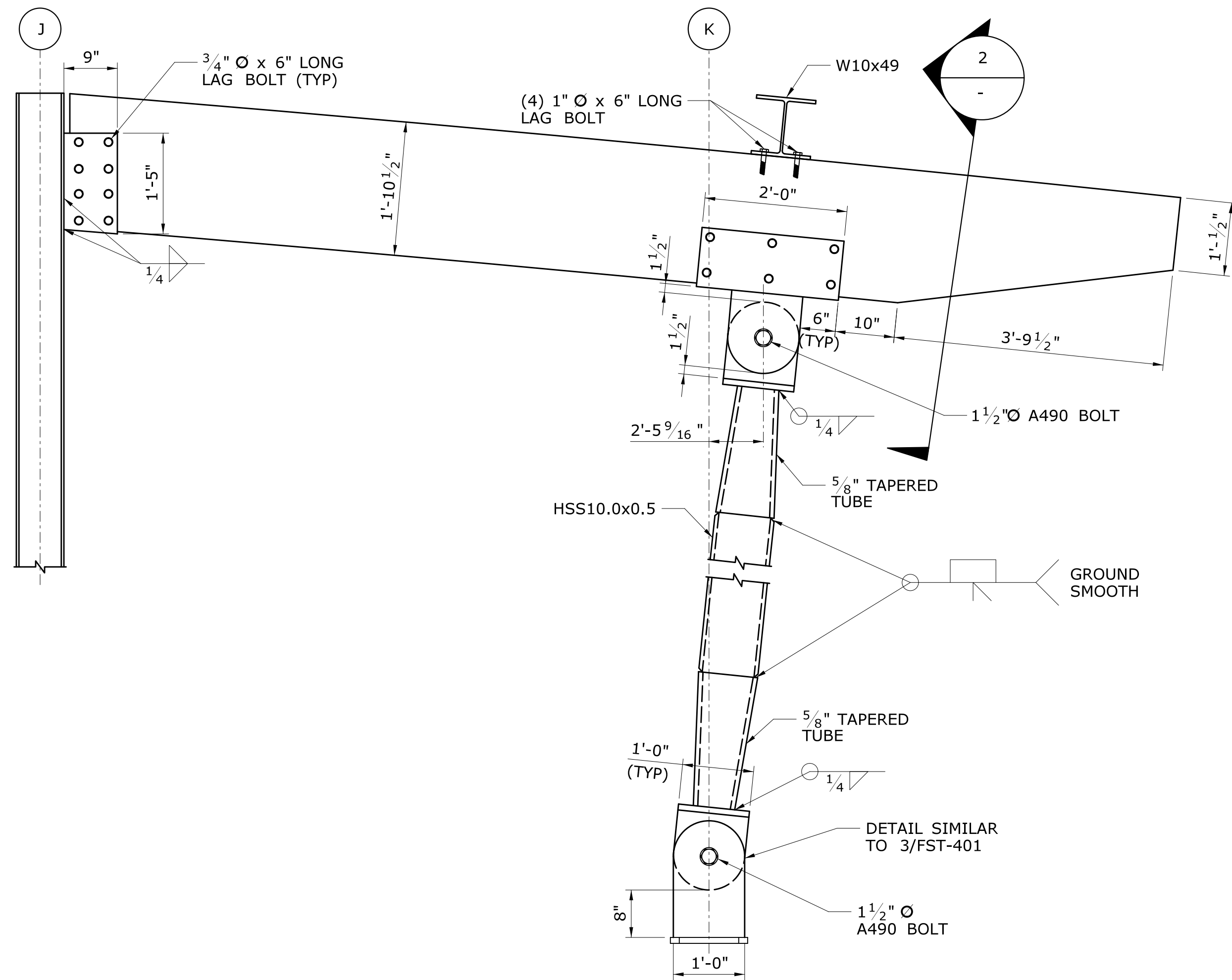
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_400.dgn

SIGNATURE/
BLOCK:
 TranSystems
530 PRESTON AVENUE
MERIDEN, CT 06450

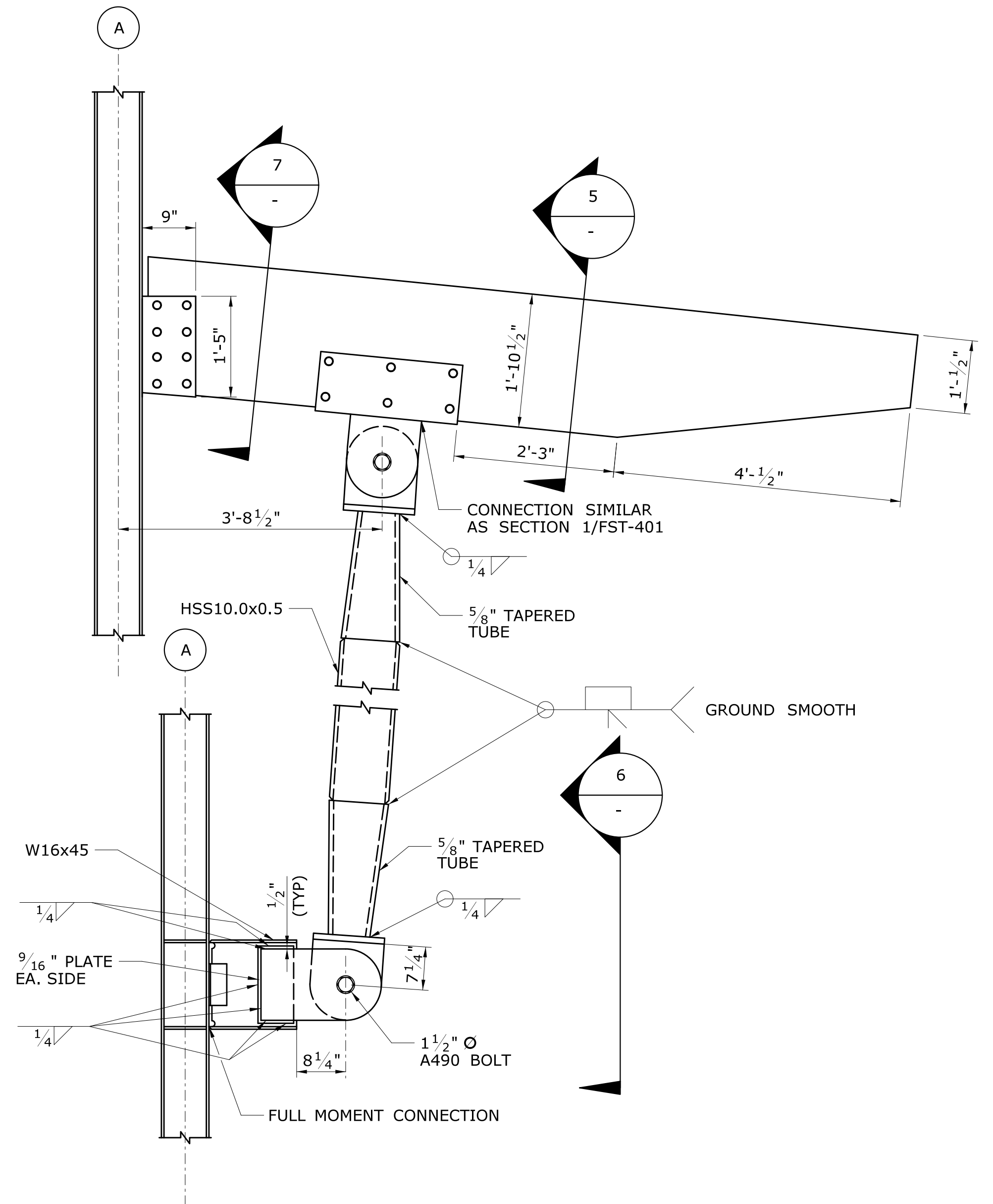
PROJECT TITLE:
**NEW HAVEN - HARTFORD
SPRINGFIELD
RAIL CORRIDOR**

TOWN:
BERLIN
DRAWING TITLE:
**TOWER SUPERSTRUCTURE
SECTIONS & DETAILS 1**

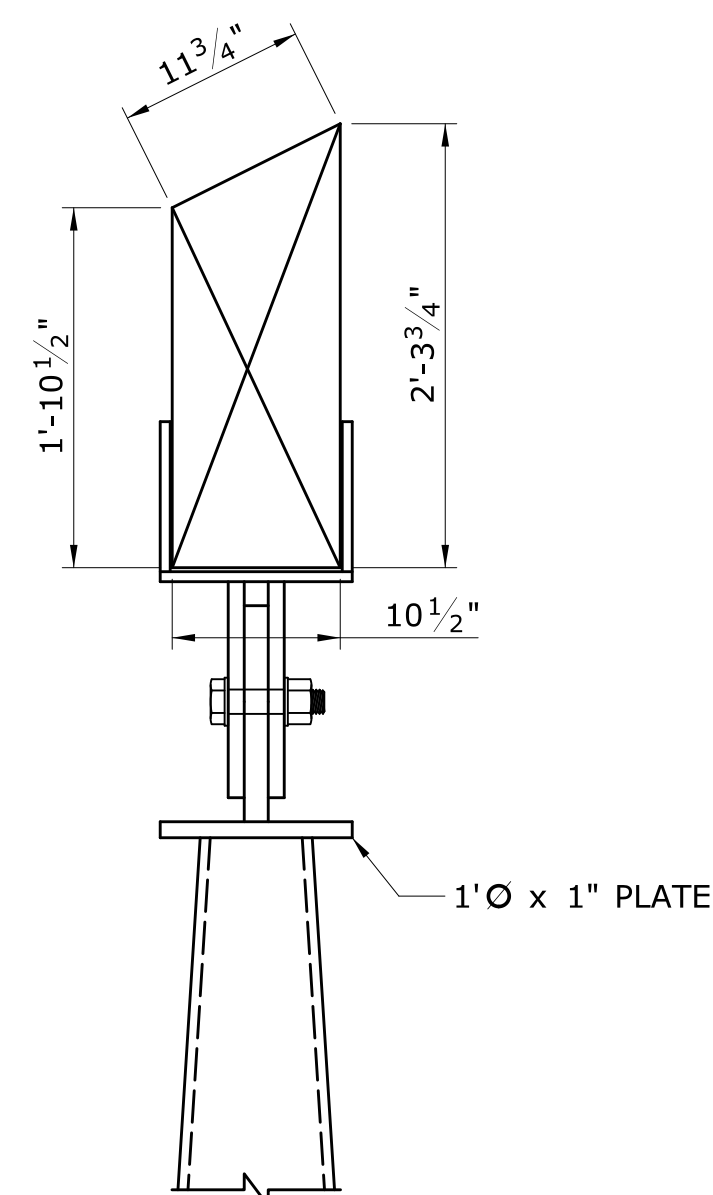
PROJECT NO.
170-3155
DRAWING NO.
FST-400
SHEET NO.
01.12.052



2
SECTION
SCALE: 1" = 1'-0"

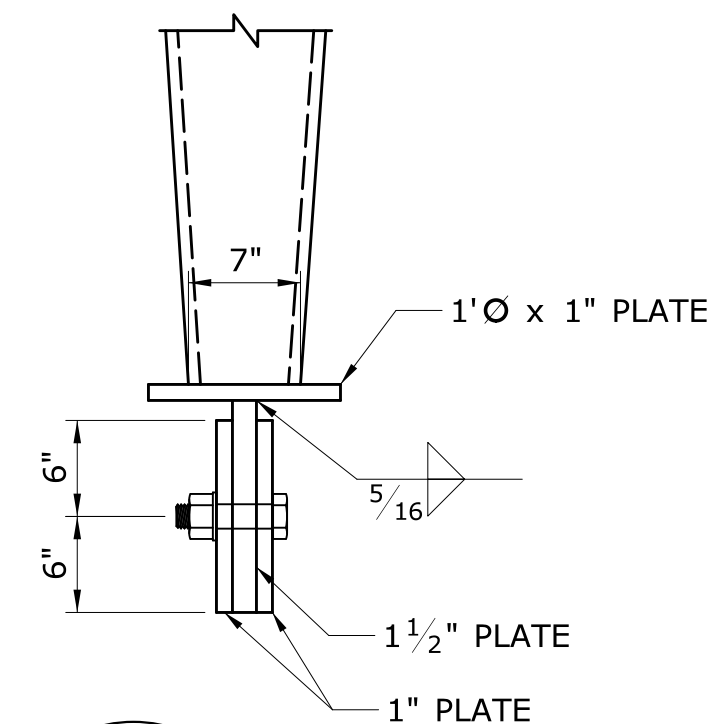


3
SECTION
FST-114
SCALE: 3/4" = 1'-0"

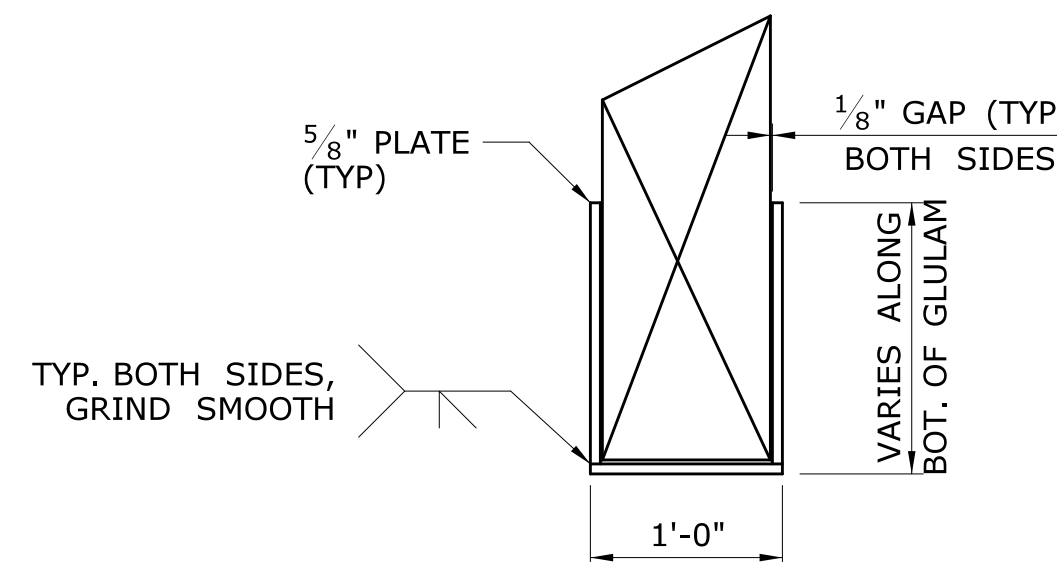


5
SECTION
SCALE: 1" = 1'-0"



1
SECTION
FST-114
SCALE: 3/4" = 1'-0"

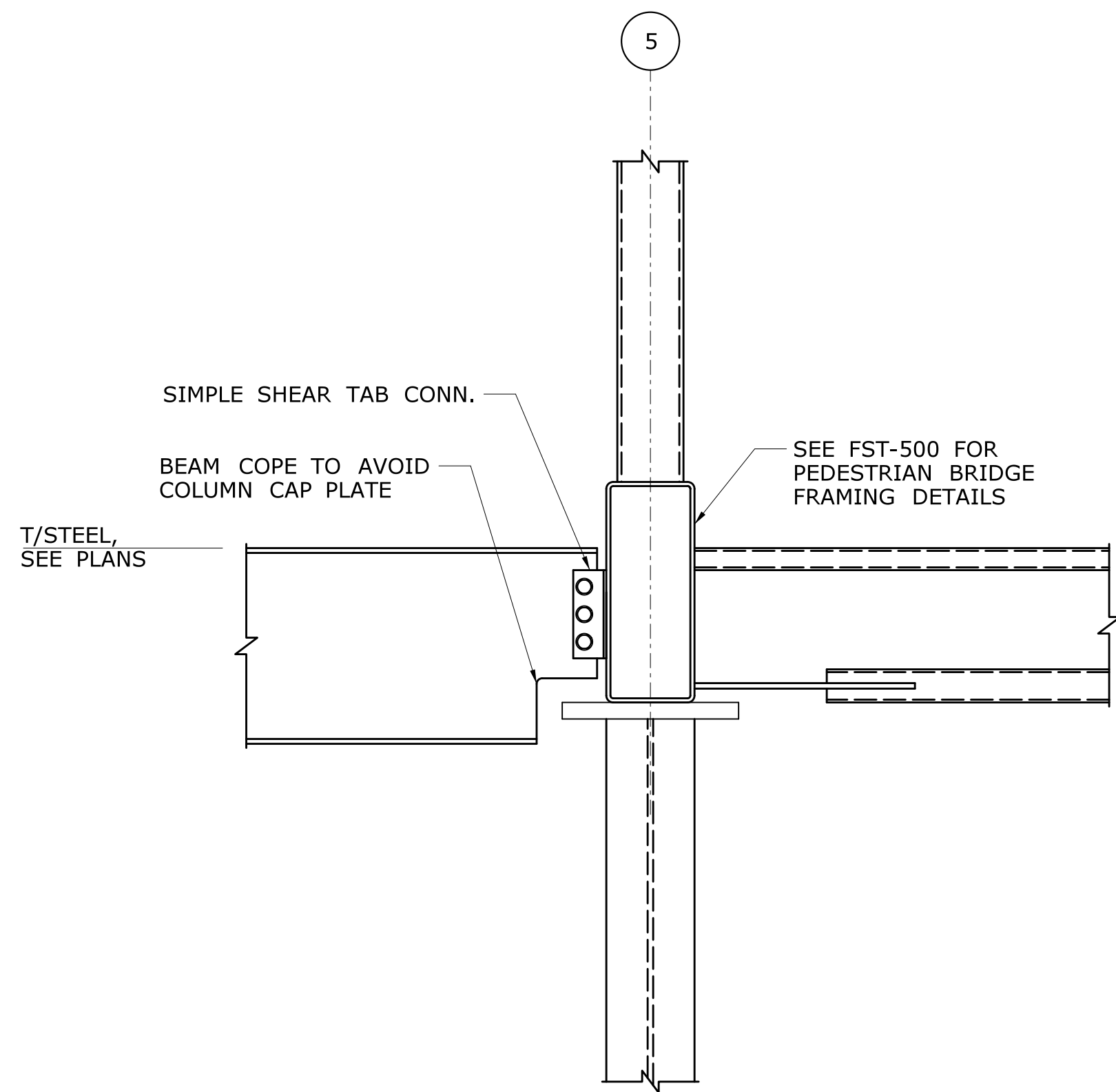


6
VIEW
SCALE: 1" = 1'-0"



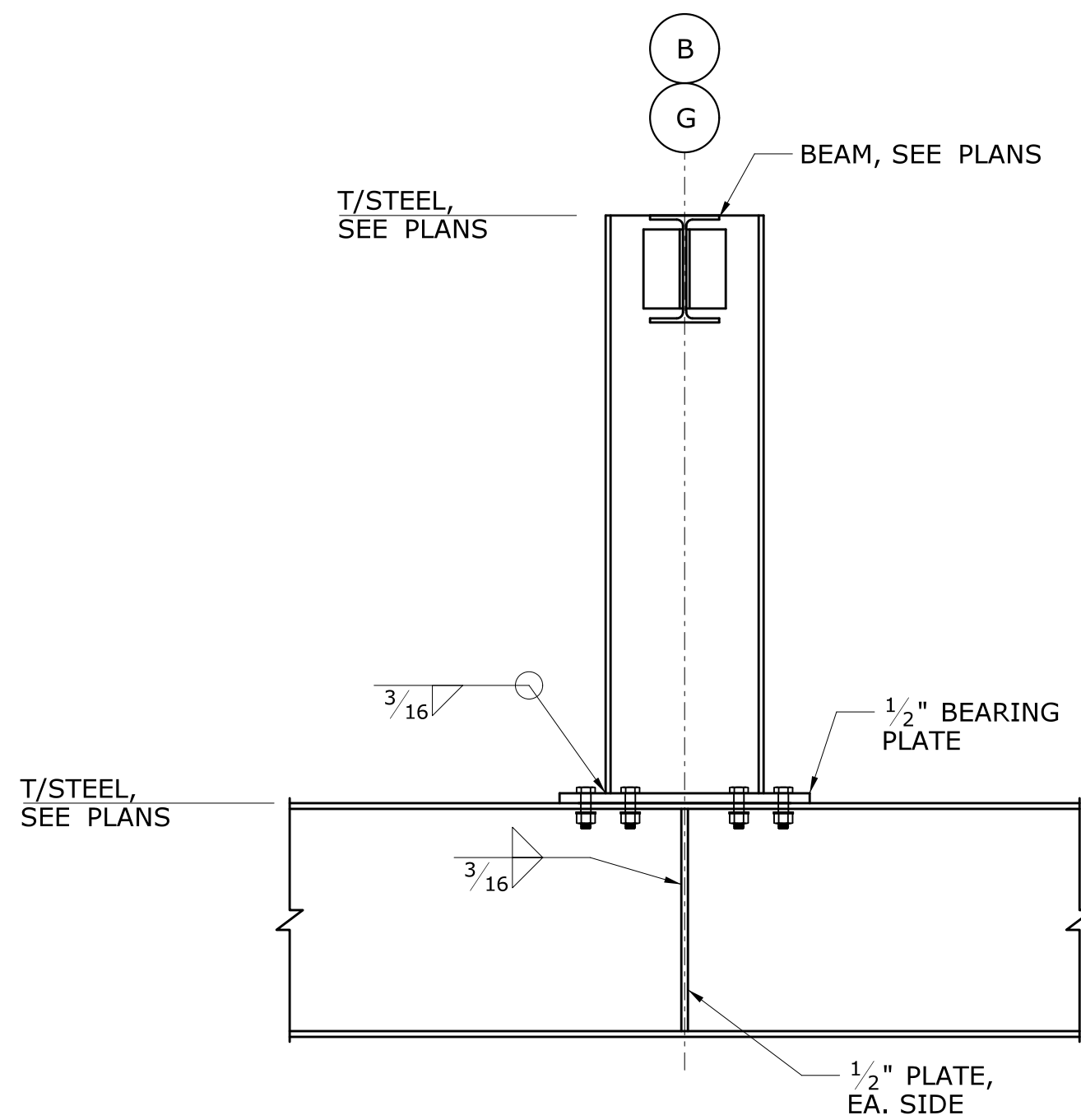
7
SECTION
SCALE: 1" = 1'-0"

| | | | | | | | | | | | | | | | | | |
|------|--|------|--|---|--|--------------------------------------|--|--|--|--|--|---|--|------------------------|--|--------------------------------|--|
| | | | | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | | DESIGNER/DRAFTER: J POPOLI | |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION | | SIGNATURE/ BLOCK:  530 PRESTON AVENUE MERIDEN, CT 06450 | | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | | TOWN: BERLIN | | PROJECT NO. 170-3155 | |
| | | | | | | CHECKED BY: H BUI | | | | DRAWING NO. FST-401 | | | | | | SHEET NO. 01.12.053 | |
| | | | | | | SCALE AS NOTED | | | | | | | | | | | |
| REV. | | DATE | | REVISION DESCRIPTION | | SHEET NO. | | Plotted Date: 2/26/2014 | | Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_401.dgn | | | | | | | |



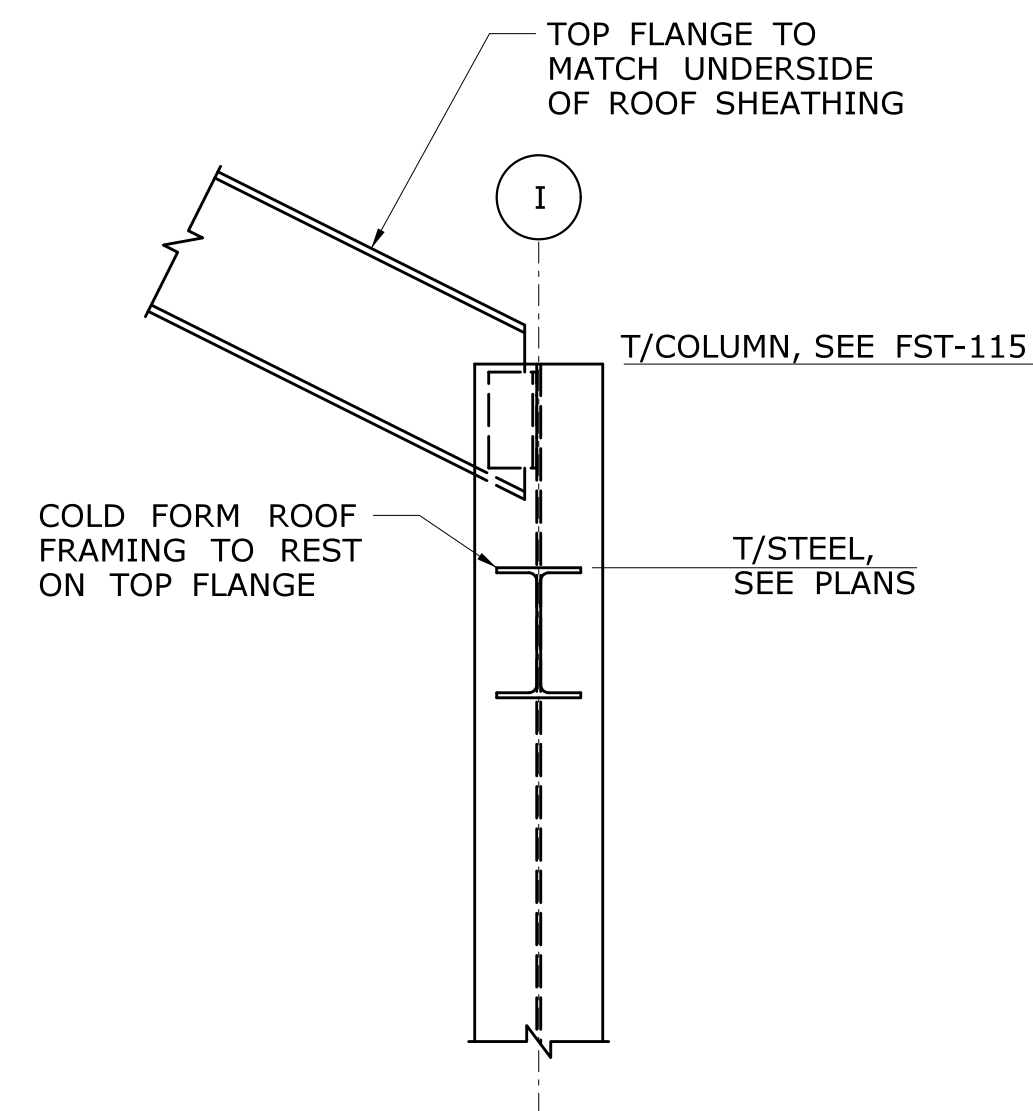
3
FST-113

DETAIL
SCALE: $\frac{3}{4}" = 1'-0"$



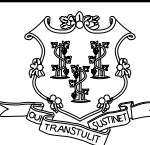
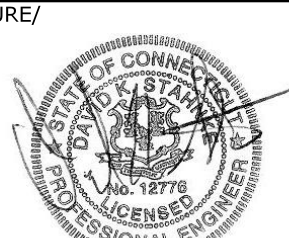
4
FST-113

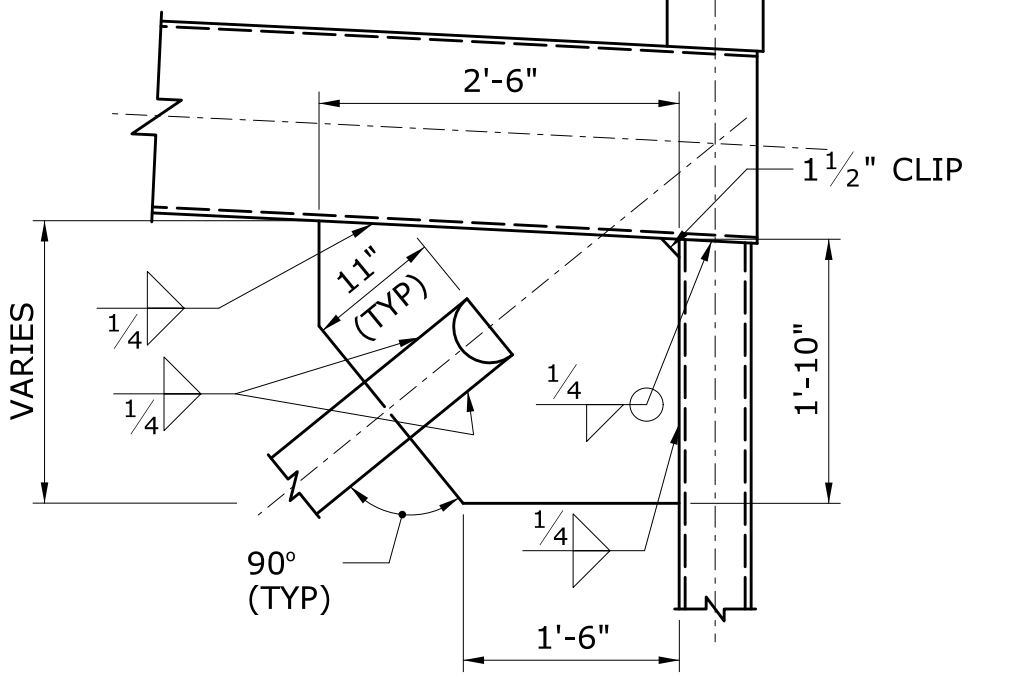
DETAIL
SCALE: $1" = 1'-0"$



4
FST-114

SECTION
SCALE: $1" = 1'-0"$

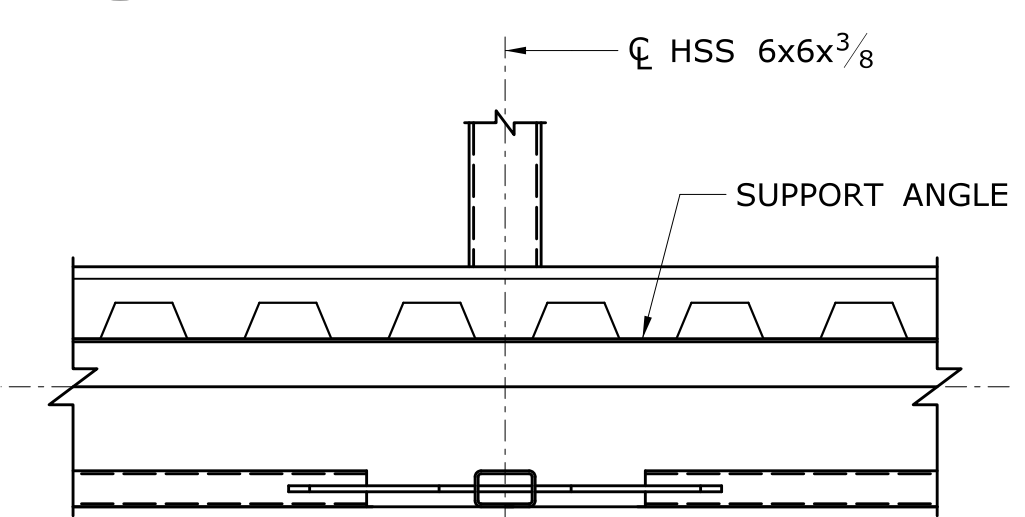
| | | | | | | | | | |
|------|------|----------------------|---|---|--|---|---|--|--|
| | | | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | DESIGNER/DRAFTER: J POPOLI CHECKED BY: H BUI SCALE AS NOTED |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_402.dgn | SIGNATURE/BLOCK:  530 PRESTON AVENUE MERIDEN, CT 06450 | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | TOWN: BERLIN DRAWING TITLE: TOWER SUPERSTRUCTURE SECTIONS & DETAILS 3 | PROJECT NO. 170-3155 DRAWING NO. FST-402 SHEET NO. 01.12.054 |
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. | Plotted Date: 2/26/2014 | | | | | |




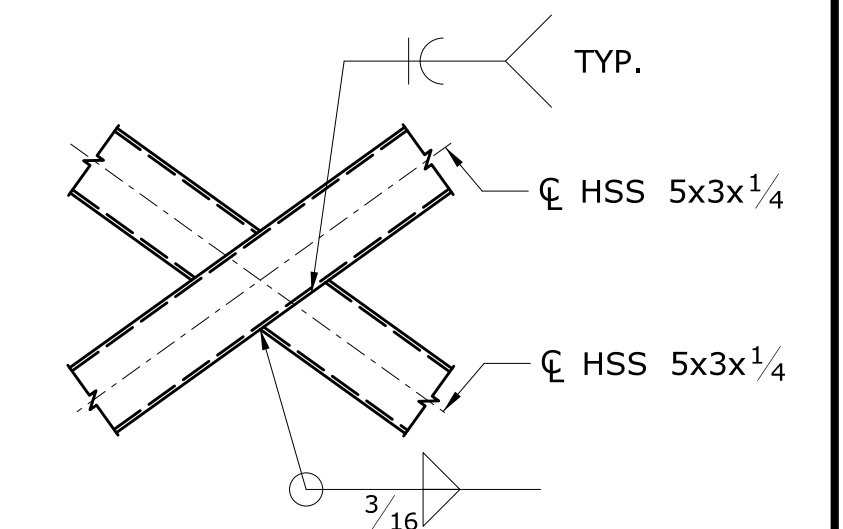
1

DETAIL

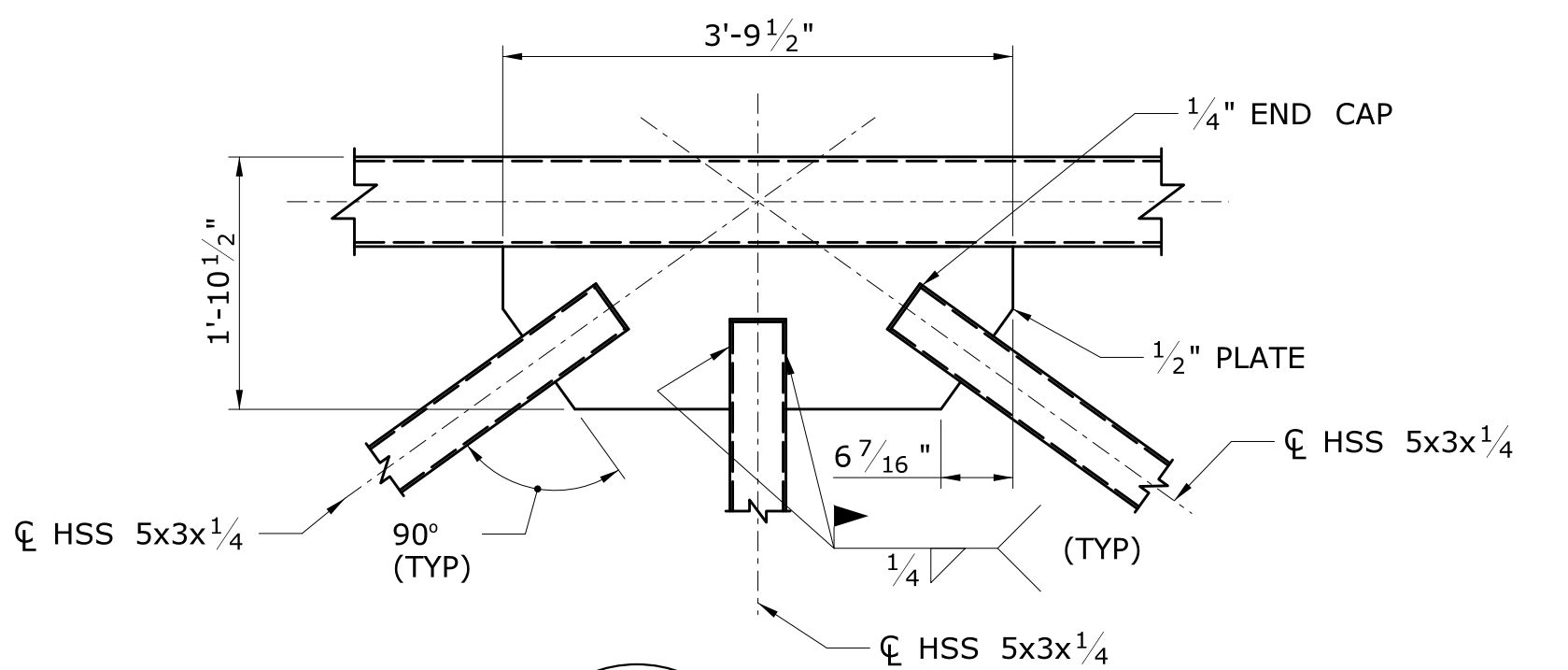
SCALE: $\frac{3}{4}" = 1'-0"$



SECTION
SCALE: $\frac{3}{4}" = 1'-0"$



DETAIL
SCALE: 1" = 1'-0"





9
-

DETAIL

SCALE: $\frac{3}{4}" = 1'-0"$

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/26/2014

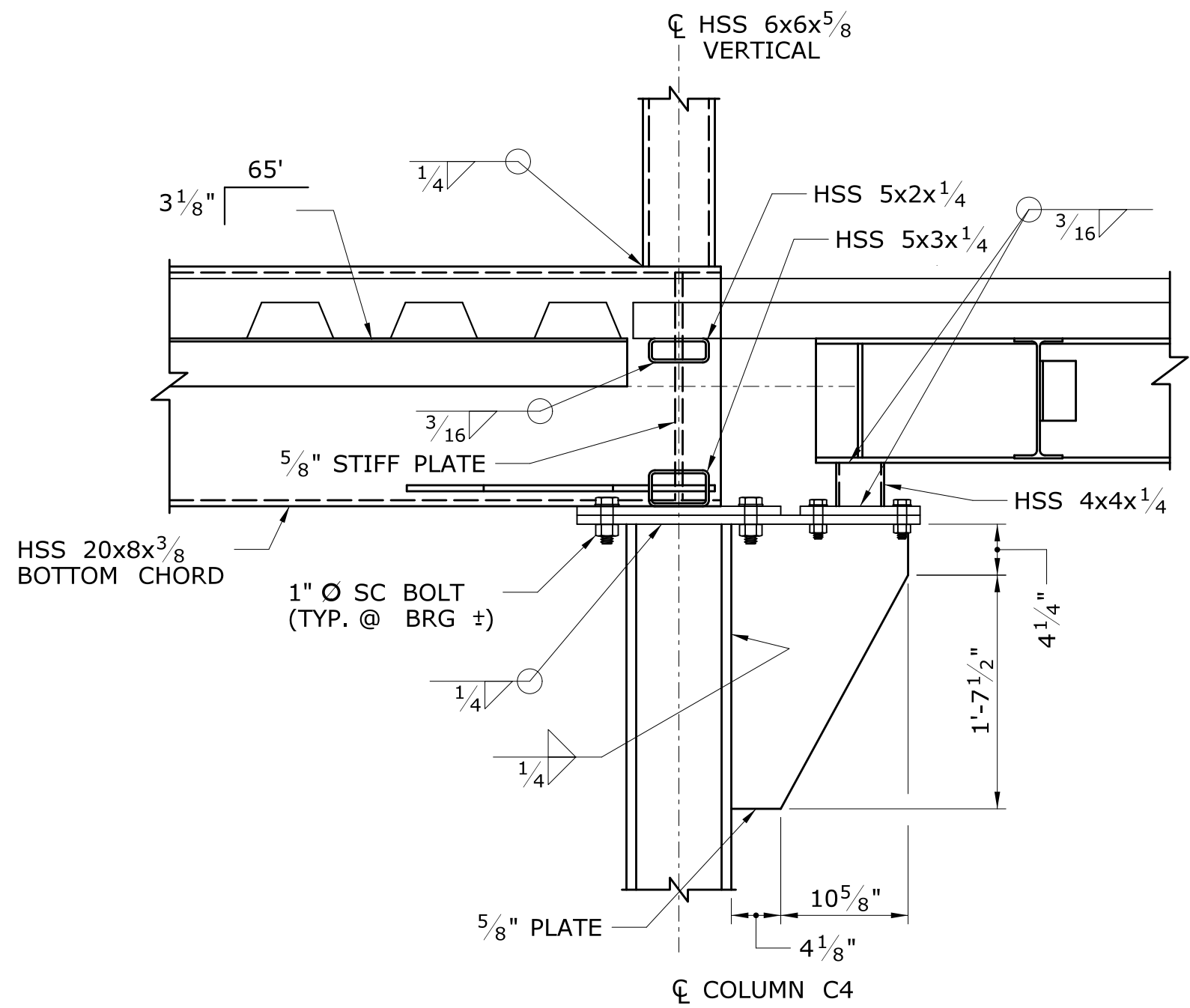

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION


Filename: ...\\FA-CGR-CPS-0170-2296-007-04-FST-500.dgn

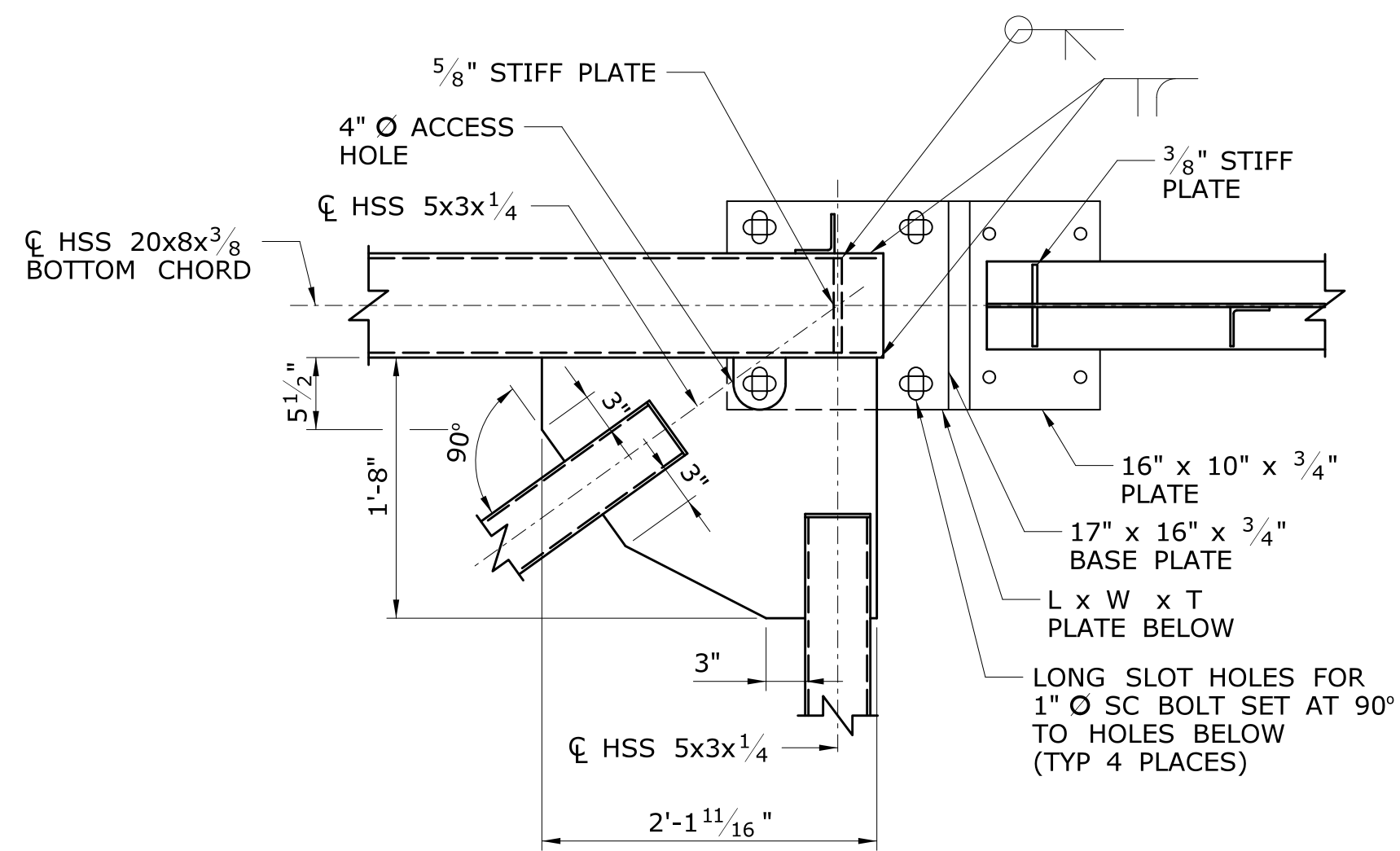
TranSystems
530 PRESTON AVENUE
MURFREESBORO, TN 37132

| | |
|----------------|--|
| TOWN: | BERLIN |
| DRAWING TITLE: | PEDESTRIAN BRIDGE FRAMING DETAILS 1 |

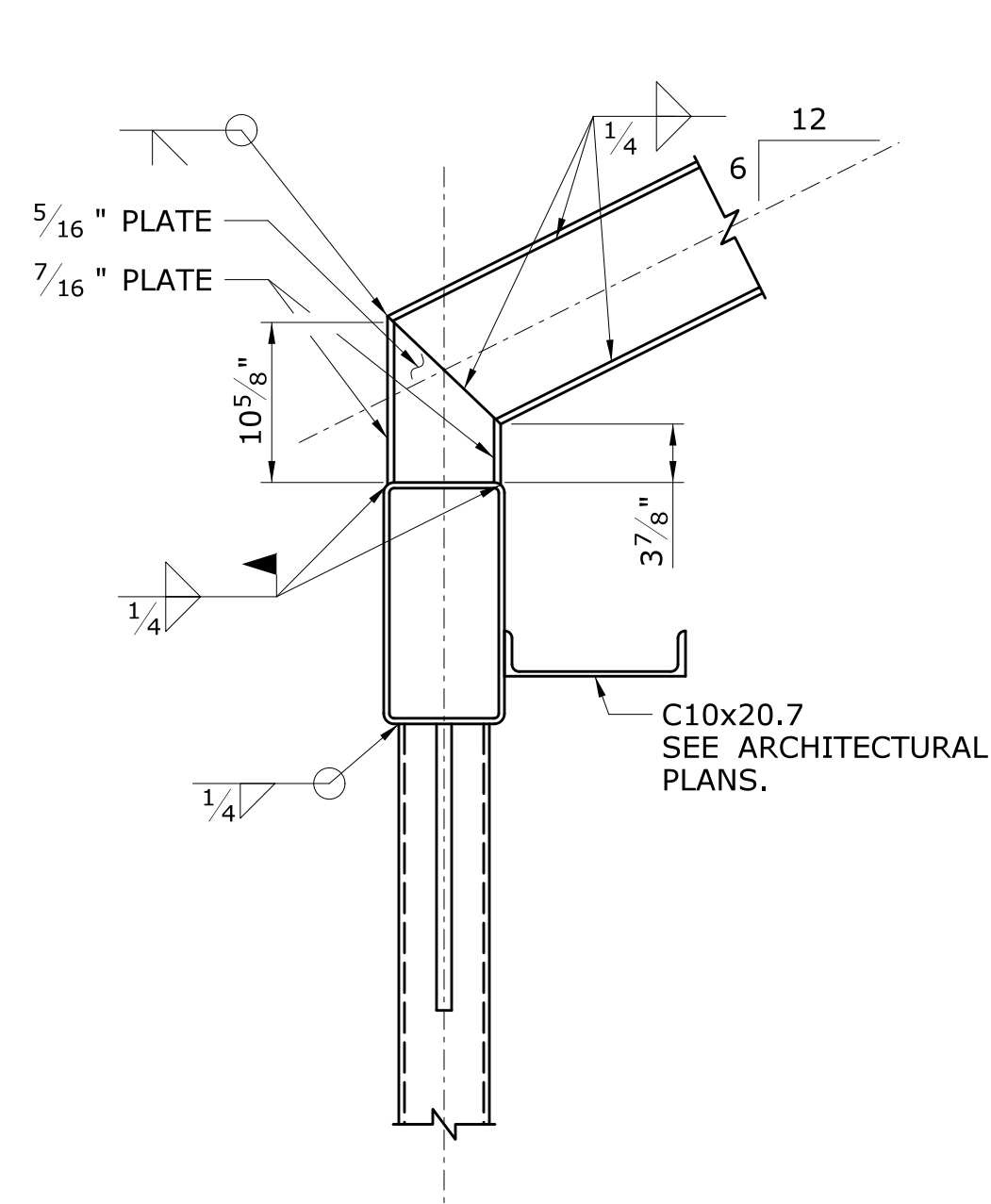
| | |
|-------------|------------------|
| PROJECT NO. | 170-3155 |
| DRAWING NO. | FST-500 |
| SHEET NO. | 01.12.055 |



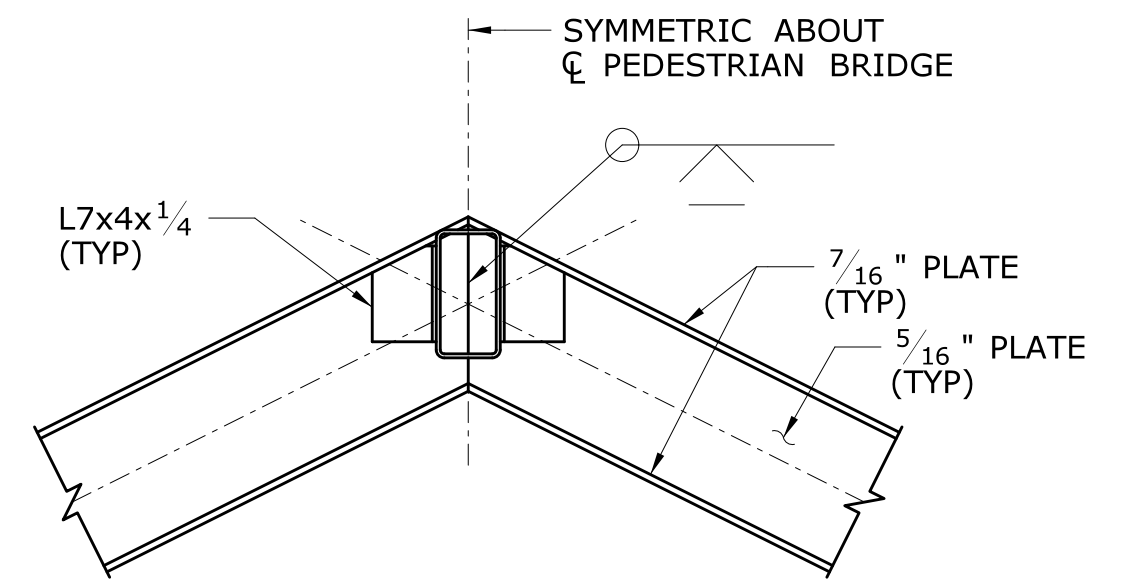
5
FST-500
SECTION
SCALE: 1" = 1'-0"



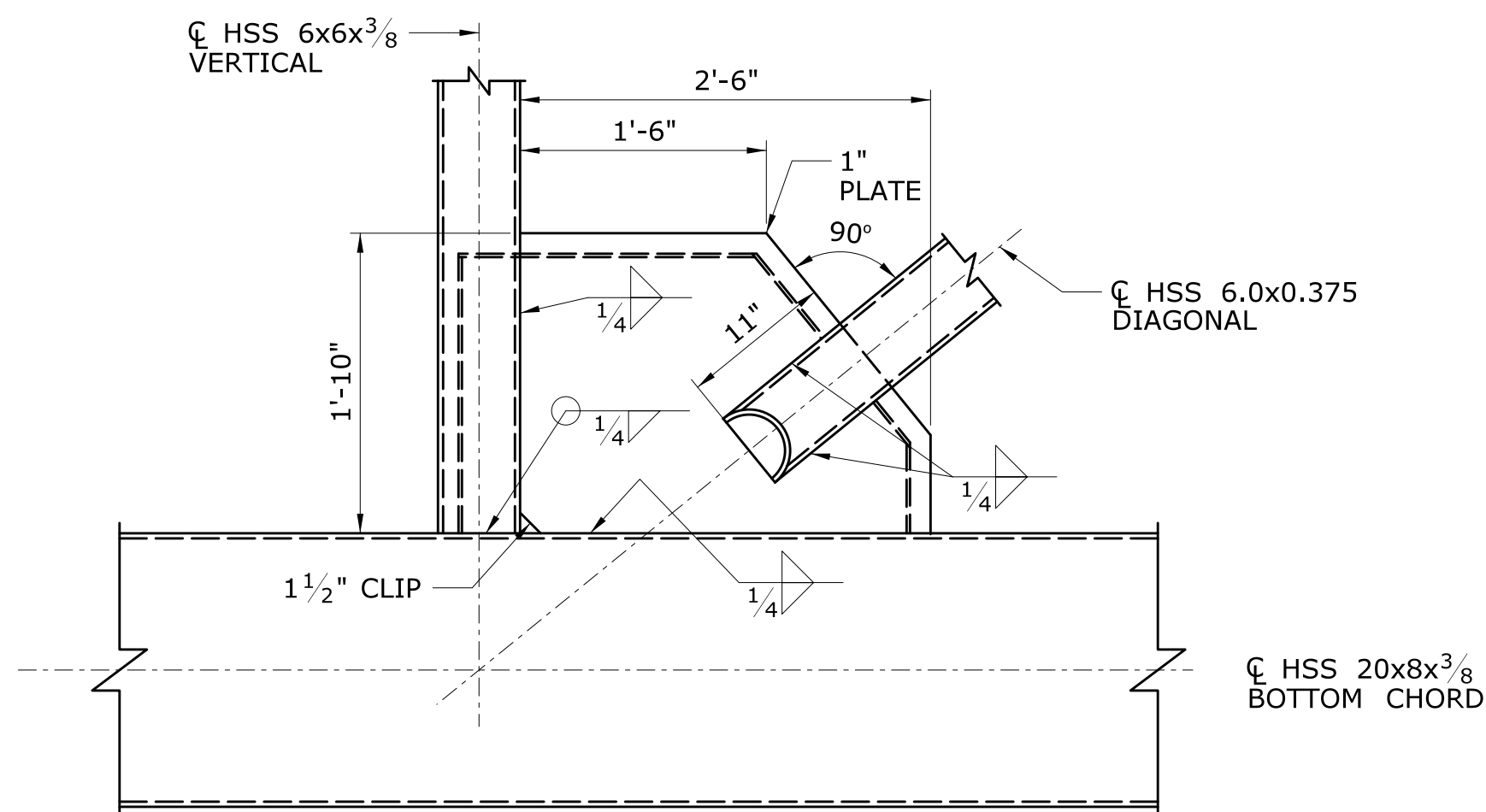
10
FST-500
DETAIL
SCALE: 1" = 1'-0"



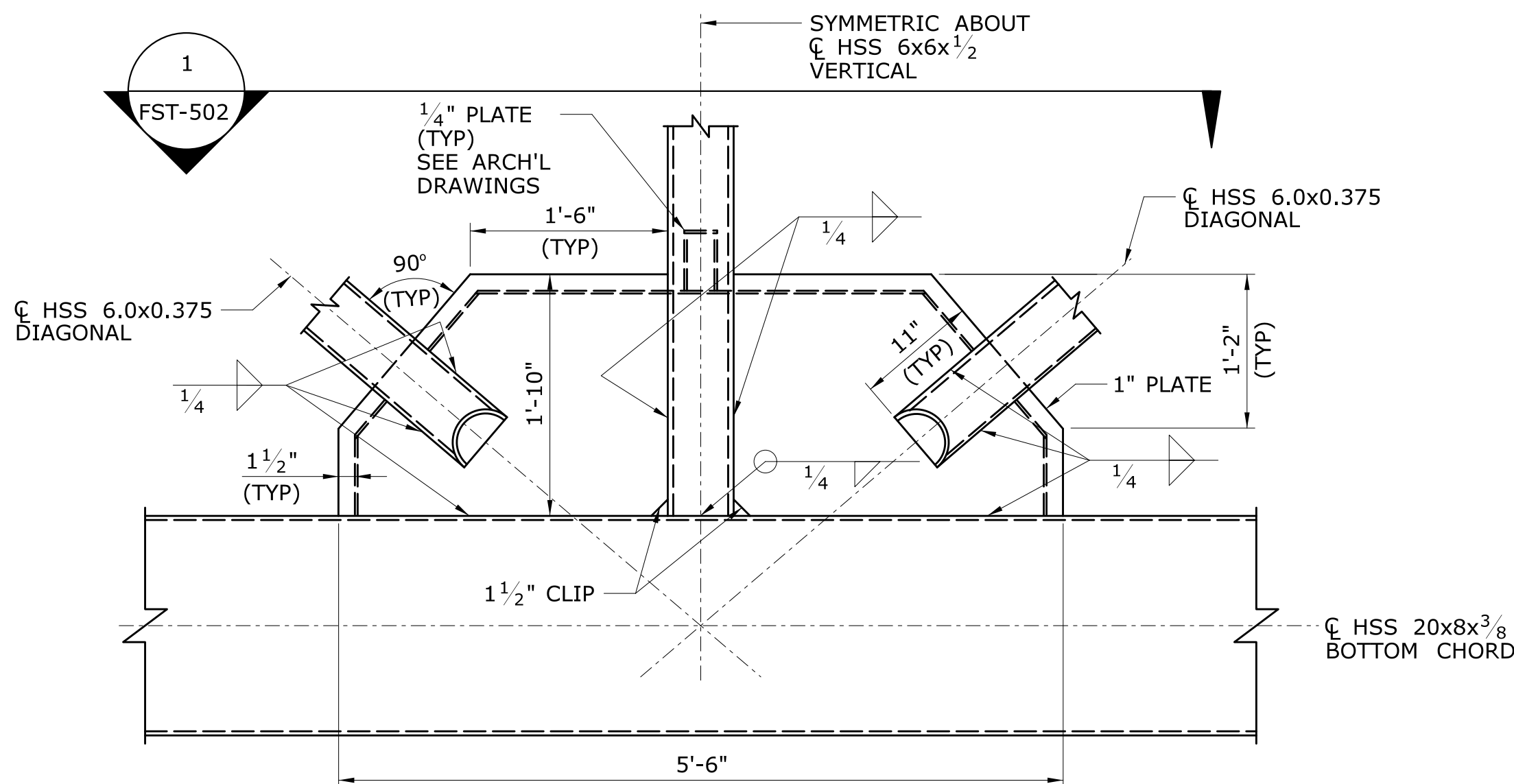
5
FST-500
DETAIL
SCALE: 1" = 1'-0"



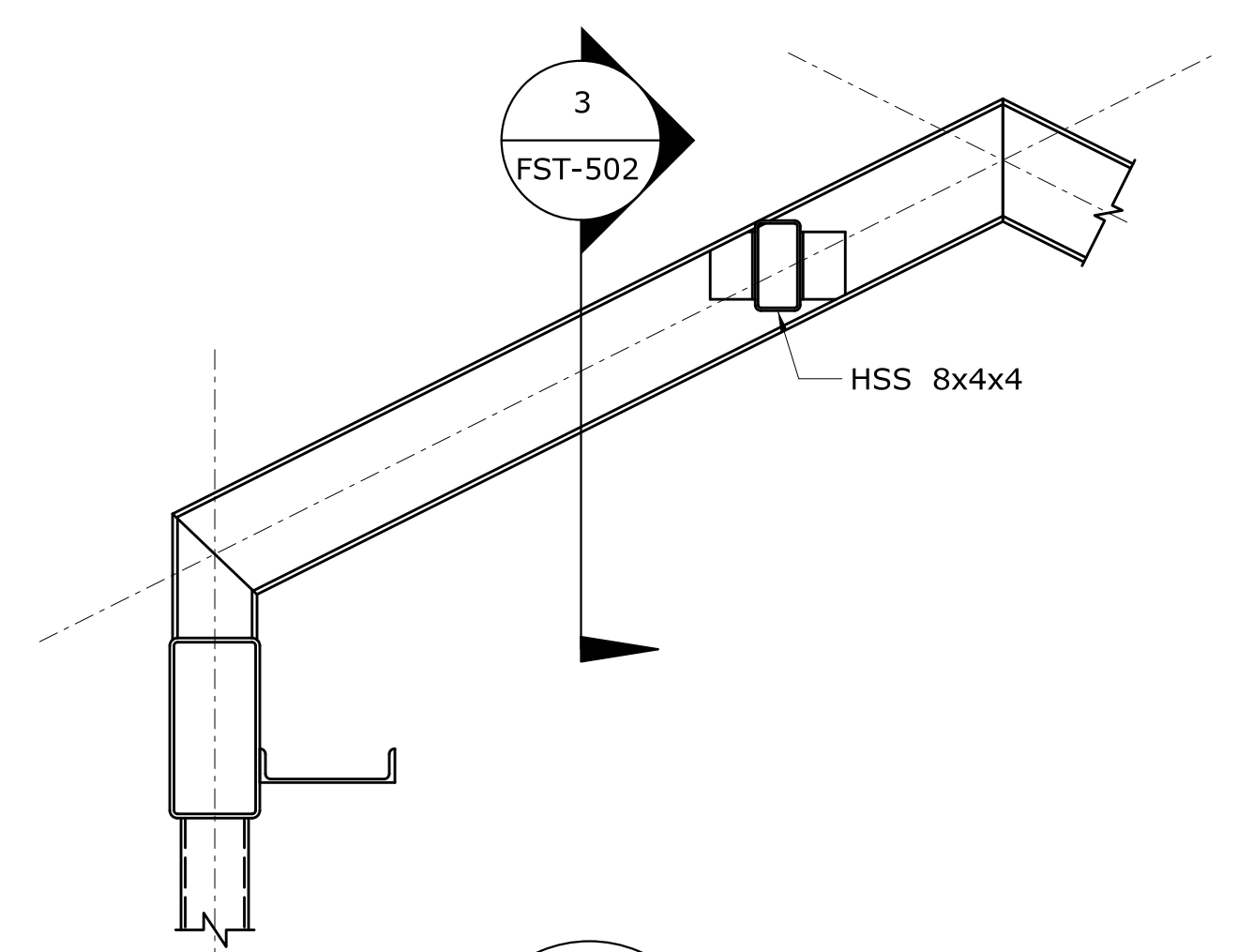
4
FST-500
SECTION
SCALE: 1" = 1'-0"





2
FST-500
DETAIL
SCALE: 1" = 1'-0"

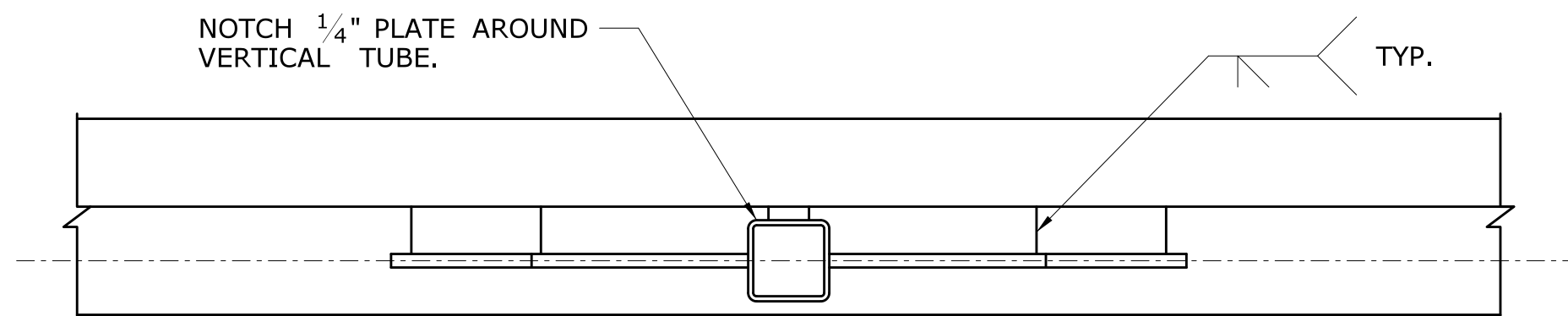


3
FST-500
DETAIL
SCALE: 1" = 1'-0"

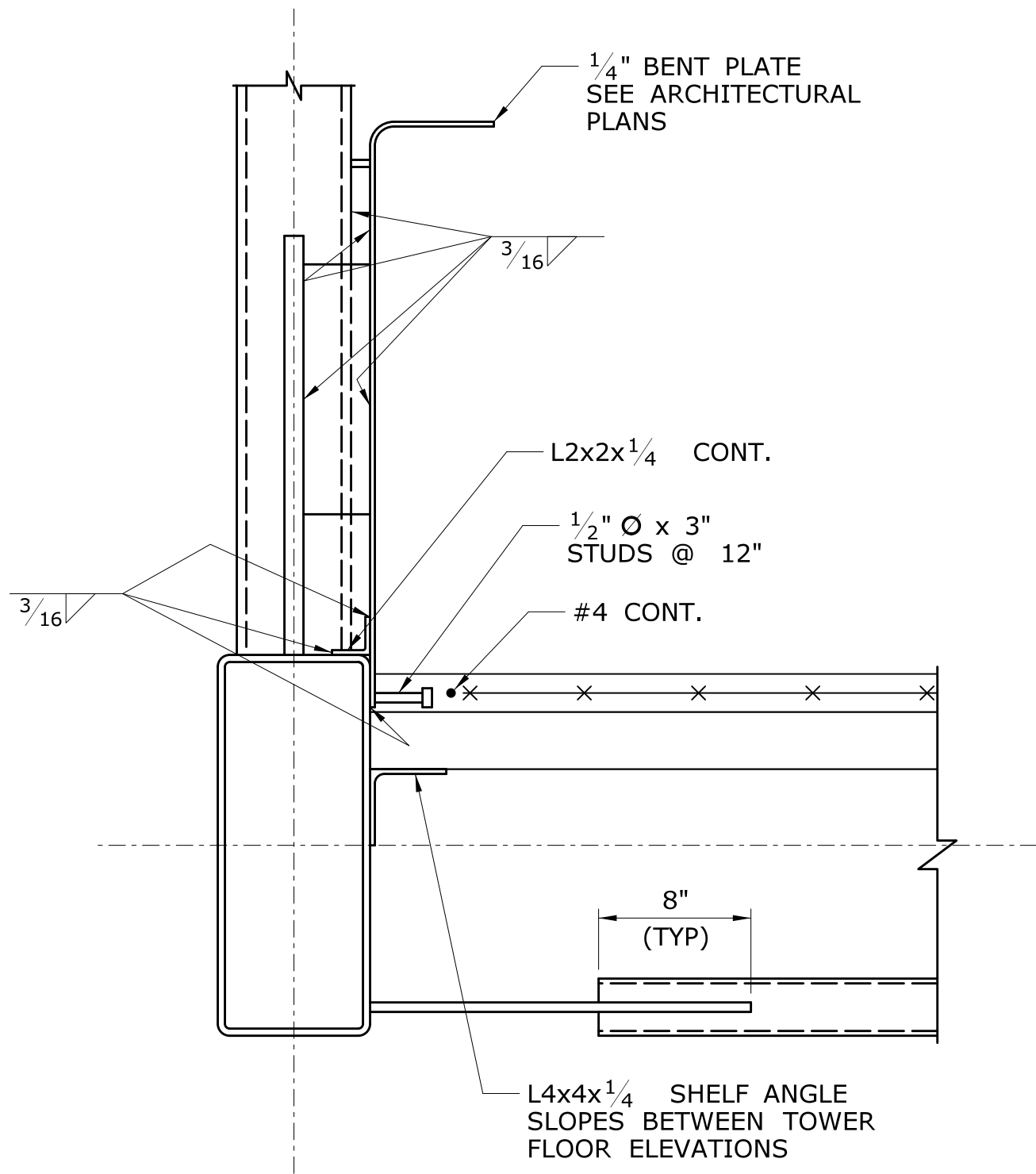


6
FST-500
DETAIL
SCALE: 1" = 1'-0"

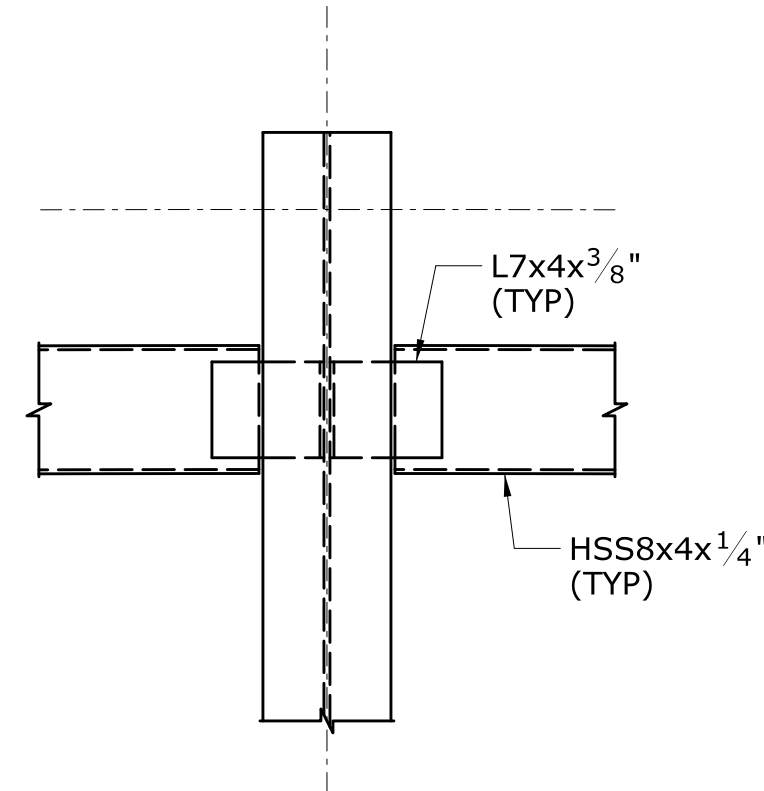
| | | | | | | | | | | | | | | | | | |
|------|--|------|--|---|--|--------------------------------------|--|---|--|--|--|---|--|----------------------------|--|--------------------------------|--|
| | | | | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | | DESIGNER/DRAFTER: J POPOLI | |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION | | SIGNATURE/ BLOCK:  530 PRESTON AVENUE MERIDEN, CT 06450 | | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | | TOWN: BERLIN | | PROJECT NO. 170-3155 | |
| | | | | | | CHECKED BY: H BUI | | | | DRAWING NO. FST-501 | | | | | | | |
| | | | | | | SCALE AS NOTED | | | | SHEET NO. 01.12.056 | | | | | | | |
| REV. | | DATE | | REVISION DESCRIPTION | | SHEET NO. | | Plotted Date: 2/26/2014 | | Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_501.dgn | | | | | | | |



1
FST-501
SECTION
SCALE: 1" = 1'-0"




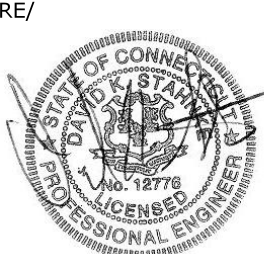

7
FST-500
DETAIL
SCALE: 1 1/2" = 1'-0"



3
FST-501
SECTION
SCALE: 1" = 1'-0"

NOTES:

1. FOR GENERAL NOTES AND STRUCTURAL NOTES, SEE DRAWING NOS. FST-001 TO FST-003.
2. COORDINATE ALL DIMENSIONS AND DETAILS WITH THE ARCHITECTURAL DRAWINGS.
3. FOR PLATFORM PLANS, SEE DRAWING NOS FST-103 TO FST-105.
4. FOR TOWER ROOF FRAMING PLANS, SEE DRAWING NOS. FST-112 TO FST-113.
5. FLOOR DECK IS TO BE INSTALLED OFF SITE AND TO RECEIVE METALIZED FINISH ALONE WITH THE BRIDGE STRUCTURE PRIOR TO DELIVERY. COORDINATE WITH ARCHITECTURAL PLANS AND SPECS FOR FINISHES.

| | | | | | | | | | | | |
|------|------|----------------------|-----------|---|--|---|--|---|---|--|--------------------------------|
| | | | | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | | DESIGNER/DRAFTER: J POPOLI CHECKED BY: H BUI SCALE AS NOTED |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_502.dgn | SIGNATURE/ BLOCK:  530 PRESTON AVENUE MERIDEN, CT 06450  | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | TOWN: BERLIN DRAWING TITLE: PEDESTRIAN BRIDGE FRAMING DETAILS 3 | PROJECT NO. 170-3155 |
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. | Plotted Date: 2/26/2014 | | | | | | | SHEET NO. 01.12.057 |

ERECTION SEQUENCE - OVERALL GENERAL NOTES:

1. THE SUGGESTED STEPS ILLUSTRATE A SEQUENCE OF STEEL ERECTION ATTEMPTING TO CONFORM TO RAILROAD REGULATIONS, TO MINIMIZE DISRUPTIONS TO THE RAIL SERVICE AND SITE CONSTRAINTS. THE SEQUENCE MAY BE ALTERED SO LONG AS THE OPERATION OF THE RAILROAD IS MAINTAINED IN CONFORMANCE WITH THE SPECIFICATIONS, AND SUBJECT TO APPROVAL BY THE DESIGN PROFESSIONALS, THE CONNECTICUT DEPARTMENT OF TRANSPORTATION (CONNDOT) AND AMTRAK.
2. EQUIPMENT SHOWN IS PRESENTED TO DEMONSTRATE REQUIRED CLEARANCES AND POSSIBLE LOCATIONS OF EQUIPMENT WITH RELATIONSHIP TO CONSTRUCTION ACTIVITIES. IN NO WAY SHOULD IT BE CONSTRUED TO BEING A FINAL AND ONLY WAY FOR CONSTRUCTION. DIFFERENT AND/OR SMALLER PIECES OF EQUIPMENT CAN BE UTILIZED AT THE CONTRACTOR'S DETERMINATION AS LONG AS AMTRAK AND CONNDOT REQUIREMENTS, ALONG WITH ALL APPLICABLE CODES AND REGULATIONS, ARE SATISFIED.
3. THE WORK AND STAGING INFORMATION LISTED IS NOT INTENDED TO COVER ALL DETAILS OF THE CONSTRUCTION. THE CONTRACTOR MUST PREPARE A DETAILED CONSTRUCTION SEQUENCE PLAN(S) AND SCHEDULE(S) FOR REVIEW AND WITH WRITTEN APPROVAL PRIOR TO BEGINNING ANY WORK.
4. GIVEN THE NATURE OF THIS CONSTRUCTION WITH WORK PARALLELING THE RAIL LINES, THE CONTRACTOR IS TO HAVE ON SITE AT ALL TIMES EQUIPMENT CAPABLE OF REMOVING FAILED MACHINES OR DEBRIS FROM THE RAILS IF A BREAKDOWN WERE TO OCCUR.
5. CONTRACTOR IS TO HAVE ALL EROSION AND SEDIMENTATION CONTROLS AND PROTECTIONS IN PLACE PRIOR TO BEGINNING ANY WORK.
6. FOR EXPECTED TRACK RESTRICTIONS, SEE "NOTICE TO CONTRACTOR - WORK ON RAILROAD PROPERTY " INCLUDED IN THE PROJECT SPECIFICATIONS.
7. ALL WORK THAT FALLS WITHIN 15 FEET OF THE RAIL CENTERLINE WILL REQUIRE A TRACK CLOSURE.
8. REFER TO AMTRAK SPECIFICATION 01142A FOR CRANE/HOISTING OPERATIONS OVER THE RAILROAD RIGHT-OF-WAY.
9. CRANE SHOWN IS FOR PLANNING PURPOSES ONLY, CONTRACTOR SHALL SUBMIT CRANE PLACEMENT AND ERECTION PLANS TO THE ENGINEER AND AMTRAK RAILROAD FOR REVIEW. THE PLANS SHALL BE PREPARED BY A PROFESSIONAL ENGINEER LICENSED IN CONNECTICUT AND SHALL DEPICT THE ENTIRE LIFTING AND SETTING PROCEDURE INCLUDING:
 - A. CRANE LOCATION(S)
 - B. RIGGING
 - C. SEQUENCE
 - D. CRANE CHARTS
 - E. STAGING LOCATION OF BRIDGE AT PICK POINT
 - F. TAGLINE HANDLING
10. A MULTIPLE-TRACK NIGHT TIME CLOSURE IS REQUIRED DURING LIFTING AND PLACING OPERATIONS. CONTRACTOR TO COORDINATE WITH AMTRAK THE AVAILABLE DAY AND TIME.
11. PRIOR TO THE PREPARATION OF CRANE PLACEMENT AND ERECTION PLANS, THE CONTRACTOR SHALL FIELD VERIFY COMMUNICATION WIRES AND OTHER CONDITIONS SHOWN ON PLANS.
12. CRANE TO BE MOUNTED LEVEL ON EXTENDED OUTRIGGERS IN FULL CONTACT WITH GROUND SURFACE. COMPACTED CRUSHED STONE OR SAND MAY BE USED TO FILL ANY LOW AREAS FOR LEVELNESS.
13. ROAD PLATES UNDER CRANE OUTRIGGERS TO BE GRADE A36 STEEL OR BETTER.
14. CRANE POSITIONS MAY BE ADJUSTED TO SUITE FIELD CONDITIONS AS LONG AS MAXIMUM RADIUS VS. LOAD IS NOT EXCEEDED.
15. MAXIMUM WIND SPEED AT TIME OF CRANE OPERATION SHALL NOT EXCEED 10 MPH.
16. A TABLE OR CHART PREPARED BY THE CRANE MANUFACTURER TO DESCRIBE THE MAXIMUM LIFT AT ALL CONDITIONS OF LOADING SHALL BE POSTED IN CRANE CAB IN CLEAR VIEW OF OPERATOR.
17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE WEIGHT OF THE LIFT AND FOR INSURING THE STABILITY OF EACH MEMBER DURING ALL PHASES ERECTION.
18. ANCHOR BOLTS FOR TOWER STRUCTURE AND BRIDGE STRUCTURE TO BE INSTALLED AND FASTENED BEFORE THE CRANE SUPPORT CABLES ARE RELEASED.
19. ALL RIGGING SLINGS AND COMPONENTS TO HAVE A FACTOR OF SAFETY OF 5.
20. CRANE CAPACITY AT THE MAXIMUM OPERATING RADIUS SHALL BE 150% GREATER THAN THE LOAD.
21. ALL PERSONNEL SHALL BE AMTRAK SAFETY TRAINED PER THE CONTRACT SPECIFICATIONS.
22. AFTER THE PEDESTRIAN BRIDGE IS SET, THE CONTRACTOR SHALL COORDINATE THE INSTALLATIONS OF BRIDGE FINISHES (GLAZING, ROOFING, TOUCH-UP PAINTING) WITH AMTRAK TO OBTAIN THE NECESSARY TRACK CLOSURES.
23. TEMPORARY FALL PROTECTION SHOULD BE INSTALLED ON THE BRIDGE AS REQUIRED UNTIL THE FINAL BRIDGE FINISHES ARE COMPLETE.
24. COORDINATE THE NECESSARY GROUNDING OF CRANE EQUIPMENT WITH THE ENGINEER AND AMTRAK R.R.

ERECTION SEQUENCE - SUGGESTED DESIGN PROCEDURE:

1. CONSTRUCT NEW PLATFORM FOUNDATION, ELEVATED PLATFORM STRUCTURE AND CANOPIES.
2. CONSTRUCT PLATFORM IN ACCORDANCE TO STAGING PLANS.
3. UTILIZE SMALLER EQUIPMENT TO ACCESS FROM THE REAR OF THE CONSTRUCTION ACTIVITY TO MINIMIZE DISRUPTION TO RAIL SERVICE.

STAIR AND ELEVATOR TOWER CONSTRUCTION

1. CONSTRUCT ELEVATOR/STAIR TOWER IN ACCORDANCE TO STAGING PLANS. TOWER STRUCTURE WILL COME ON SITE SHOP ASSEMBLED IN SECTIONS TO GREATEST EXTENT POSSIBLE. ERECTION OF TOWER SECTIONS WILL REQUIRE A TRACK CLOSURE WHICH REQUIRES THE CONTRACTOR TO COORDINATE WITH AMTRAK.
2. AFTER TOWER SECTIONS ARE SET, INSTALL INTERMEDIATE FRAMING AND STAIR STRINGERS. WHERE POSSIBLE USE SMALL CRANE OR LIFT TO REDUCE IMPACT ON RAIL SERVICE AND TRACK CLOSURES. IF DESIRED, SET TOP MEMBERS FIRST AND UTILIZE RIGGING AND HOISTS OFF MEMBERS TO SET LOWER FRAME SECTIONS. PROTECT SECTIONS FROM DAMAGE.
3. CONTRACTOR TO VERIFY IN FIELD THE DISTANCES BETWEEN THE PEDESTRIAN BRIDGE BEARING PAD LOCATIONS PRIOR TO BEARING NEW BRIDGE ONTO TOWER STEEL FRAME STRUCTURES.

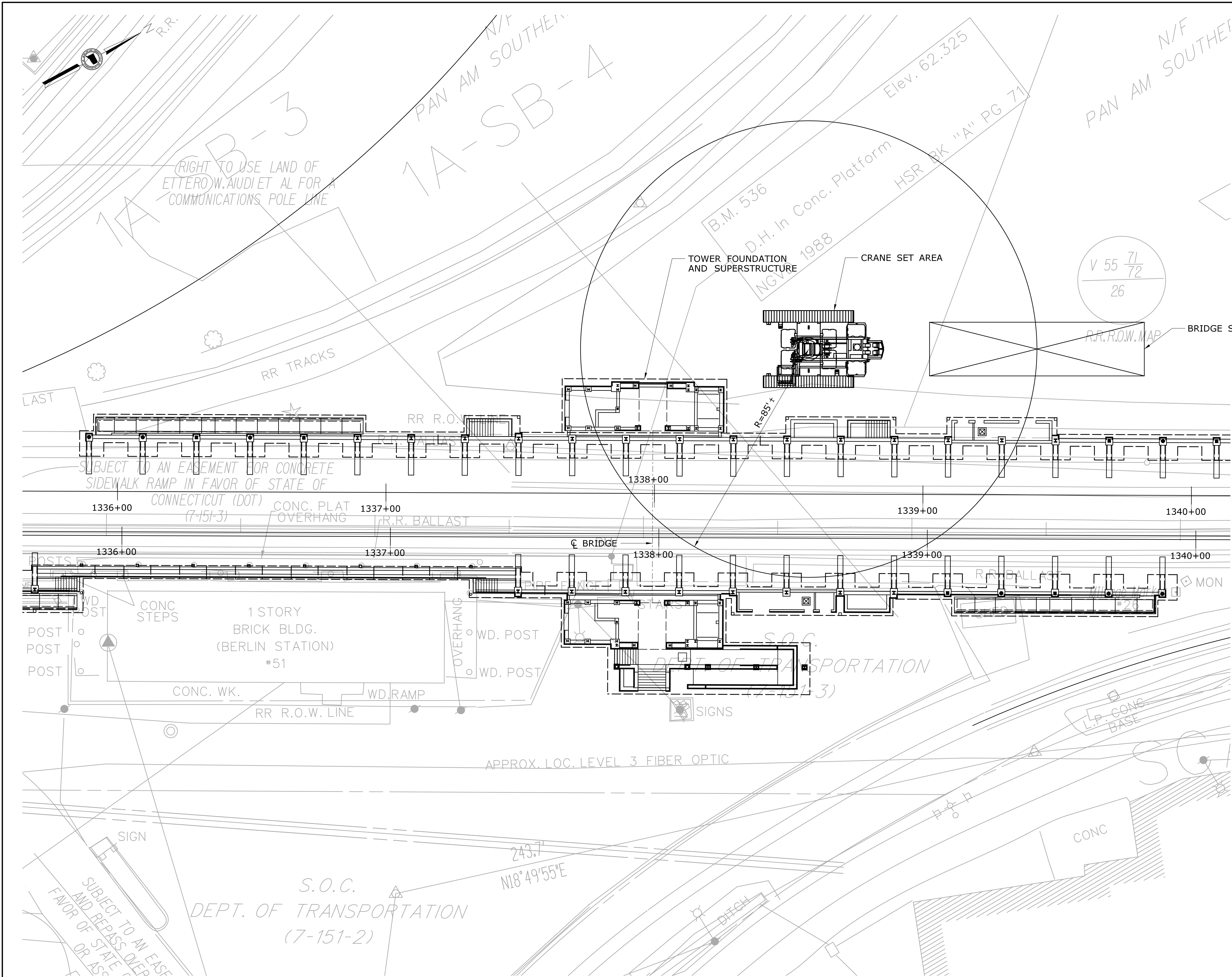
PEDESTRIAN BRIDGE OVERPASS CONSTRUCTION

1. ERECTION OF PEDESTRIAN BRIDGE WILL REQUIRE TRACK CLOSURE(S). CONTRACTOR TO COORDINATE WITH RAIL AUTHORITIES. CONTRACTOR NEEDS TO BE PREPARED TO RAISE, SET AND SECURE THE BRIDGE SECTION IN A LIMITED TIME FRAME, LIKELY 12AM-4AM ON A WEEKEND.
2. THE BRIDGE HAS BEEN DESIGNED FOR EACH SIDE OF THE TRUSS TO BE SHOP FABRICATED IN ONE (1) SECTION AND THEN DELIVERED TO SITE. FLOOR CROSS BRACING AND ROOF RAFTER FRAMING SHALL BE FIELD INSTALLED AS SHOWN ON PLANS. THE CONTRACTOR IS ALLOWED TO SUBMIT ALTERNATE BOLTED CONNECTIONS TO THE ENGINEER FOR APPROVAL. ASSEMBLE BRIDGE ON GROUND (IN AREA SHOWN ON PLAN) TO PREPARE FOR HOISTING, RAISING AND SETTING IN PLACE.
3. BRIDGE SECTION IS TO BE RAISED WITH FLOOR SLAB METAL DECK INSTALLED, PREPPED AND READY FOR CONCRETE PLACEMENT AFTER ERECTION.
4. BRIDGE SECTION IS TO HAVE A TEMPORARY PLYWOOD COVERING INSTALLED ON THE OUTSIDE WALLS. PLYWOOD IS TO ENCLOSE THE BRIDGE AREA SO THAT WORK CAN PROCEED ON THE INTERIOR FINISHES AND WINDOW SYSTEM AFTER THE BRIDGE SECTION HAS BEEN SET AND SECURED, SO THAT ADDITIONAL RAIL CLOSURES CAN BE AVOIDED. CARE MUST BE TAKEN AS TO NOT DAMAGE THE BRIDGE MEMBERS OR FINISH WITH THE TEMPORARY PLYWOOD COVERING.
5. PEDESTRIAN BRIDGE ROOF FRAMING, STEEL ROOF DECK AND ELECTRICAL CONDUITS ARE TO BE DELIVERED TO SITE AND INSTALLED IN STAGING AREA PRIOR TO LIFTING IT IN PLACE.
6. ERECTION OF PEDESTRIAN BRIDGE TO BE PERFORMED WITH ALL TRACKS OUT OF SERVICE (TO BE COORDINATED WITH AMTRAK).
7. CRANE TO BE SET UP AND PREPARED IN POSITION. HOOK ONTO BRIDGE STRUCTURE USING FOUR-LEG BRIDLE. NOTE, LIFTING SEQUENCE TO BE PROGRAMMED IN ADVANCE ON THE ON-BOARD COMPUTER. CONTRACTOR SHALL POSITION PERSONNEL TO CONTROL THE POSITION, SWING AND ROTATION OF THE BRIDGE THROUGHOUT THE ENTIRE LIFTING PROCEDURE.
8. RAISE THE PEDESTRIAN BRIDGE SO THAT THE UNDERSIDE OF THE BRIDGE STRUCTURE CLEARS THE TOP OF THE STAIR/TOWER FRAMING.
9. LOWER THE STRUCTURE ONTO THE END BEARINGS AND SECURE USING ANCHOR BOLTS BEFORE RELEASING THE LOAD.
10. ALL BEARING POINTS MUST BE MADE SECURE PRIOR TO RELEASE OF TRACK OUTAGE.

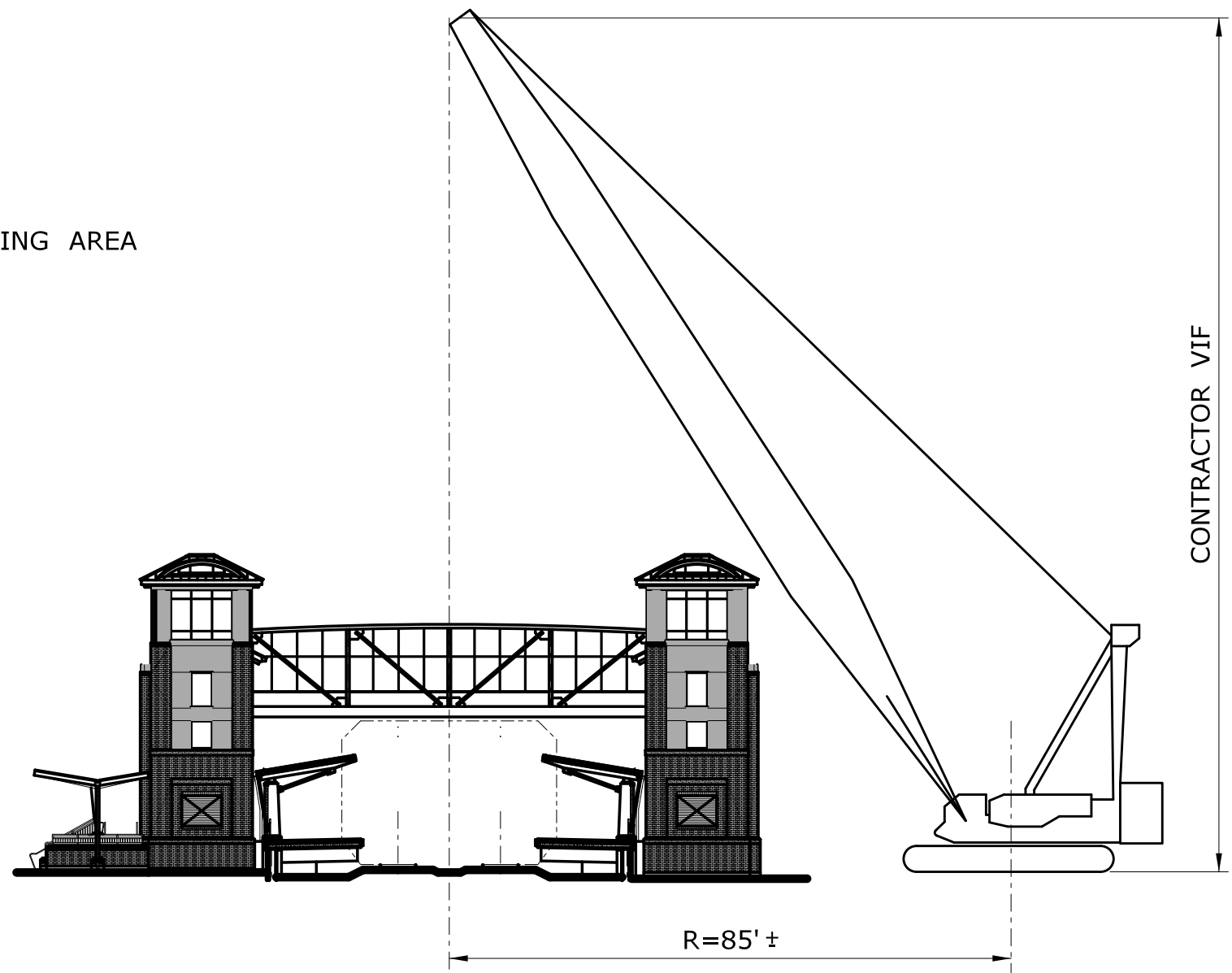
WRAP UP AND COMPLETE CONSTRUCTION

1. COMPLETE THE INSTALLATION AND RELATED ELECTRICAL WORK FOR THE ELEVATORS, SITE LIGHTING, PLATFORM LIGHTING, AND CANOPY LIGHTING.
2. COMPLETE THE GLAZING AND ALL RELATED FINISH WORK FOR THE PEDESTRIAN BRIDGE AND ELEVATOR SYSTEMS.
3. COMPLETE THE PUNCH LIST WORK.
4. REMOVE THE TEMPORARY CONSTRUCTION FACILITIES. CLEAN UP THE SITE AND PREPARE FOR OWNER ACCEPTANCE AND PUBLIC USE OF THE NEW TRAIN FACILITY.

[illegible]





- NOTES:**
1. PEDESTRIAN BRIDGE ERECTION IS PERFORMED DURING STAGE 2 CONSTRUCTION, SEE STAGING PLANS FOR OVERALL CONSTRUCTION STAGING.
 2. FOR ALL ERECTION SEQUENCE NOTES, SEE DRAWING NO. FST-600.



BRIDGE ERECTION ELEVATION
N.T.S.

OVERALL PLAN
SCALE: 1" = 40'

| | | | | | | | | | | |
|------|------|----------------------|---|-------------------------|--|---|--|---|--|--|
| | | | THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. | | DESIGNER/DRAFTER: C DONOHUE CHECKED BY: H BUI SCALE IN FEET 0 SCALE AS NOTED 80 SCALE 1"=40' |  STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION Filename: ...\\FA_CGR_CPS_0170-2296_007_04_FST_601.dgn |  530 PRESTON AVENUE MERIDEN, CT 06450 | PROJECT TITLE: NEW HAVEN - HARTFORD SPRINGFIELD RAIL CORRIDOR | TOWN: BERLIN DRAWING TITLE: ERECTION SEQUENCE 2 | PROJECT NO. 170-3155 DRAWING NO. FST-601 SHEET NO. 01.12.059 |
| REV. | DATE | REVISION DESCRIPTION | SHEET NO. | Plotted Date: 2/26/2014 | | | | | | |